



TOWN OF WELDON

PROJECT ACCELERATION PLAN

November 2023

ACKNOWLEDGMENTS

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TABLE OF CONTENTS

01	INTRODUCTION06 Overview Process + Schedule Vision + Goals Study Area Profile Active Transportation Benefits	04	NETWORK RECOMMENDATIONS68 Overview Design Guidance Bicycle + Pedestrian Network Prioritization Priority Projects
02	EXISTING CONDITIONS26 Overview Existing Facilities Previous Planning Efforts Policy Review Opportunities + Constraints	05	POLICY + PROGRAM RECOMMENDATIONS110 Overview Recommended Policies Recommended Programs
03	COMMUNITY ENGAGEMENT.....54 Overview Steering Committee Meetings Public Meeting Community Survey	06	IMPLEMENTATION120 Overview Key Partners in Implementation Action Plan Maintenance Guidance Project Development Process
	APPENDICES.....136 Appendix A: Funding Resources Appendix B: Design Resources Appendix C: Additional Community Engagement Appendix D: Cost Estimates		

EXECUTIVE SUMMARY

BICYCLING + WALKING IN WELDON

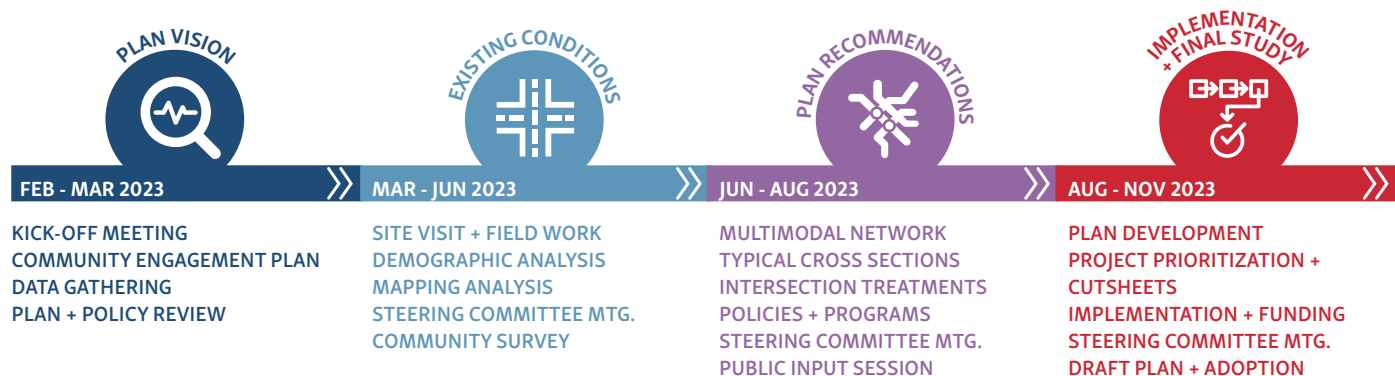
Weldon, a vibrant town of 1,668 residents, is located on the Roanoke River in Halifax County, North Carolina. It offers both the charm and quality of life of a small community with easy access to Interstate 95. Currently, a limited sidewalk network serves the downtown area and Weldon’s Historic District. Existing sidewalks within the Town offer mobility to residents and visitors of central Weldon. In addition, the Roanoke Canal Trail is a regional attraction within Weldon that links communities along the Roanoke River. The Roanoke Canal Trail connects Weldon to neighboring Roanoke Rapids from its eastern terminus at River Falls Park. Through the development of the Project Acceleration Plan, the Town of Weldon is well-positioned to improve its multimodal network and support walking and bicycling for recreational purposes, as well as viable modes of transportation for both visitors and residents alike.

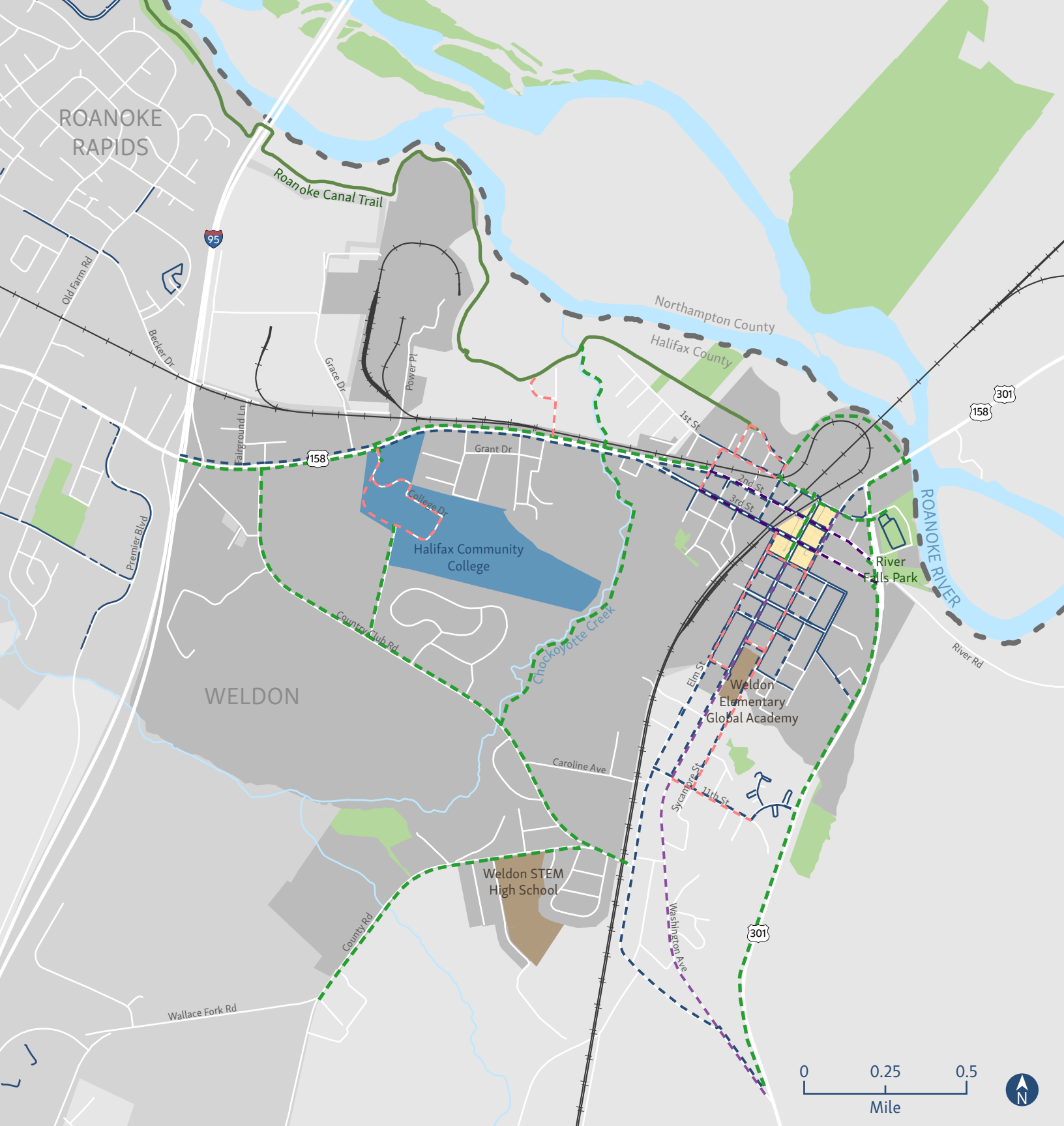
The Project Acceleration Plan is a community-led initiative that proposes an implementable multimodal network based on input from residents, key area stakeholders, Town staff, and local leaders. This plan will also serve as a guide for future development and the ongoing maintenance of multimodal facilities, aiming to increase opportunities for active transportation and outdoor enjoyment. The proposed multimodal network will connect local neighborhoods, businesses, schools, parks, and other popular destinations within and around the Town of Weldon.

KEY RECOMMENDATIONS

The key recommendations in this Plan are categorized into linear bicycle and pedestrian facilities, as well as intersection treatments and crossing improvements. Several of the proposed projects address critical gaps in the existing multimodal network between key destinations identified by both the Steering Committee and the public, notably, connections to the adjacent community of Roanoke Rapids, the Roanoke River, Halifax Community College, and the existing Roanoke Canal Trail. In addition to network recommendations, this plan also recommends programs and policies to enhance the overall multimodal network.

This plan highlights the Town’s eight priority projects within the recommended network to streamline future implementation efforts. These priority projects have been carefully selected based on a comprehensive set of eight criteria, including connections to schools, parks, recreation centers, activity centers, residential areas, and existing bicycle or pedestrian facilities. They also take into consideration valuable input from the community, addressing the need to improve connections across significant barriers or areas with high rates of bicycle or pedestrian crashes.





TOWN OF WELDON

PROJECT ACCELERATION PLAN

Proposed Bicycle and Pedestrian Network

LEGEND ▶

- - - Proposed Shared Use Path
- Existing Shared Use Path
- - - Proposed Sidewalk
- Existing Sidewalk
- - - Proposed Protected Bike Lane
- Proposed Bike Lane
- - - Proposed Shared Lane Markings
- + + + Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary



01

Introduction



OVERVIEW

The *Town of Weldon Project Acceleration Plan* assesses current conditions to recommend a system of multimodal travel connecting bicyclists and pedestrians to key destinations in and around the Town. Additionally, it recommends programs and policies, provides an overview of plan implementation, and garners support for adoption of the Plan by the Town's governing body. This Plan was funded through the North Carolina Department of Transportation (NCDOT) Integrated Mobility Division (IMD) Bicycle and Pedestrian Planning Grant Initiative.

Project Acceleration Plans

What is it?

A project acceleration plan is an abbreviated plan that focuses on priority bicycle, pedestrian, and multimodal project identification and project implementation.

How is it helpful?

Project acceleration plans focus on efficiently identifying multimodal projects and implementation steps to foster a more walkable, bikeable and transit-friendly environment in communities that might otherwise have difficulty resourcing or funding a multimodal network plan.

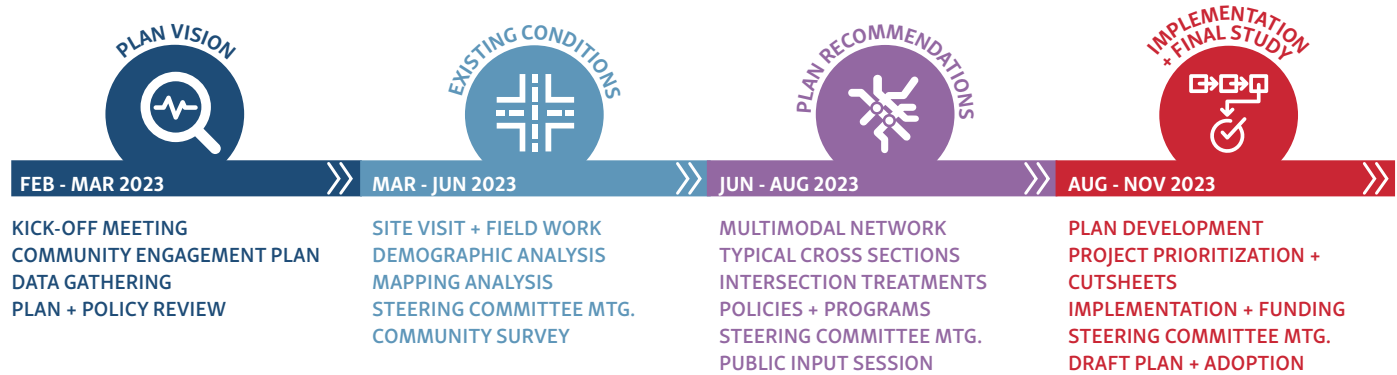
Who is it for?

A project acceleration plan is meant for municipalities with a population of 9,999 or less. In 2020, the Town of Weldon was home to 1,668 people.

PROCESS + SCHEDULE

The recommended bicycle and pedestrian network for the *Town of Weldon Project Acceleration Plan* was developed and evaluated using an approach with considerations of the built, natural, social, and economic environments. These environments are explored further in **Chapter 2: Existing Conditions**.

Initiated in February 2023 and concluded in November 2023, the *Town of Weldon Project Acceleration Plan* progressed through four distinct phases: Plan Vision, Existing Conditions, Plan Recommendations, and Implementation + Final Study. The graphic below highlights key components of each phase. Throughout this comprehensive process, community engagement efforts were actively pursued. This involved meetings with a Steering Committee and the general public, coupled with a community survey, ensuring a collaborative and inclusive approach to plan development.



Washington Ave is the "Main Street" of Downtown Weldon

VISION + GOALS

The Steering Committee for the *Town of Weldon Project Acceleration Plan* formed the vision and goals for the plan through a visioning exercise held during the first Steering Committee Meeting. The goals of the Plan focus on themes of safety, accessibility and connectivity, equity, economic impact, and project feasibility, and served as a guide throughout the planning process. Community members also echoed these goals during outreach events. The goals in this plan reflect the proposed outcomes and recommendations of the plan. The community vision for the plan is provided below and the goals are described to the right.

COMMUNITY VISION

“The Town of Weldon will promote a safe, accessible, and an interconnected bicycle and pedestrian network, prioritizing the well-being of its residents while generating economic impact and fostering community connectivity. By focusing on project feasibility, the Town will ensure the successful implementation of multimodal projects that benefit all, supporting a healthy community where active transportation thrives.”



Signage and branding help build a sense of place and community pride

COMMUNITY GOALS



Safety:

Address community safety needs in the development of Weldon’s bicycle and pedestrian network and propose safety improvements at critical intersections and access points.



Accessibility + Connectivity:

Ensure that Weldon’s bicycle and pedestrian network expands transportation and recreational access for residents and visitors and connects to key destinations within Town limits and neighboring jurisdictions.



Equity:

Improve bicycle and pedestrian access to social and economic opportunities in Weldon via an active transportation network suitable for users of all ages and abilities.



Economic Impact:

Maximize the economic opportunities of multimodal projects to leverage as catalysts for local economic and employment growth, stimulate local businesses, and attract visitors to the Town of Weldon.



Project Feasibility:

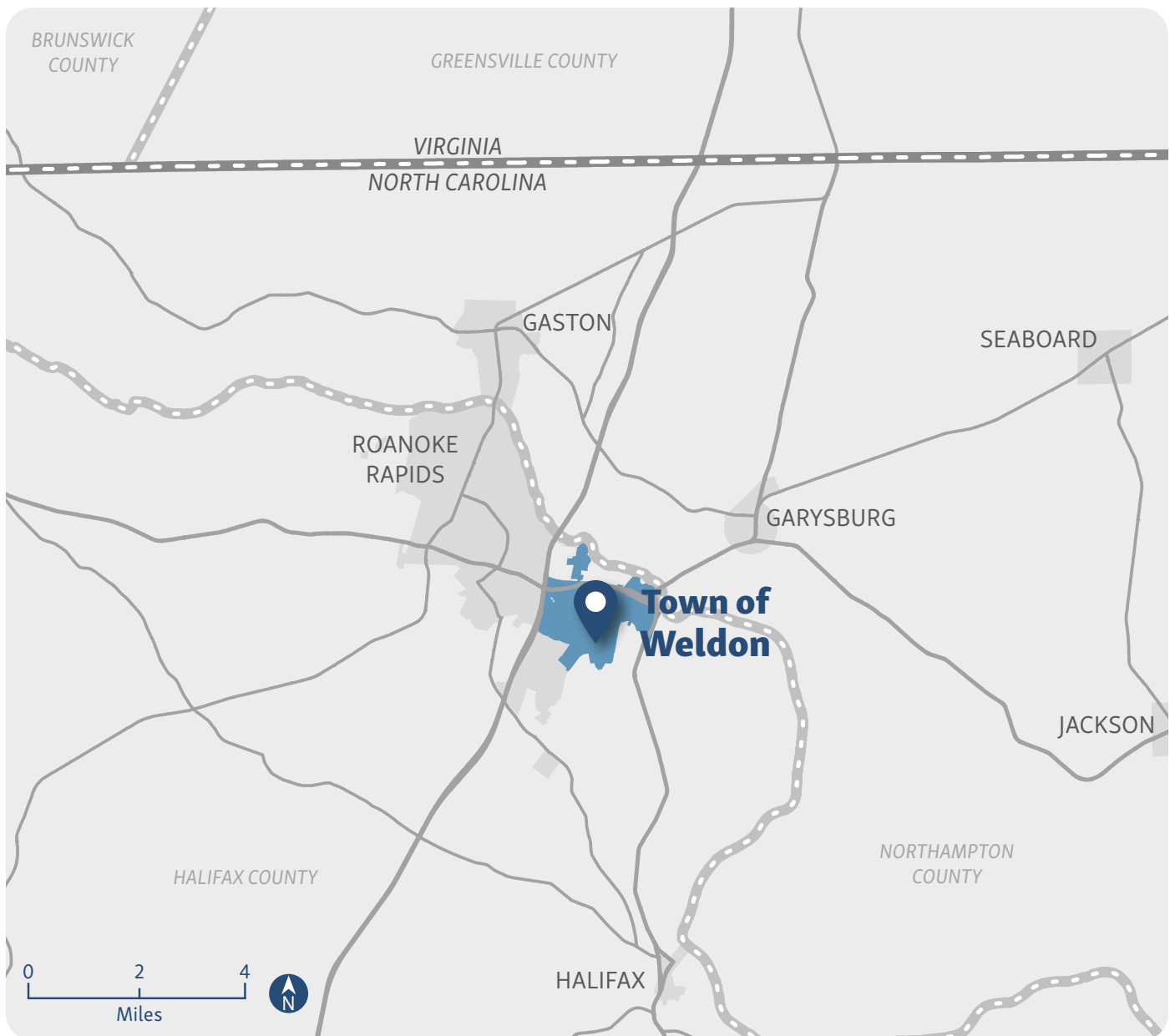
Prioritize the development of multimodal projects that are permissible, solve right-of-way challenges, meet community needs, and generate public support to be implemented and maintained with local, regional, and state resources.

STUDY AREA PROFILE

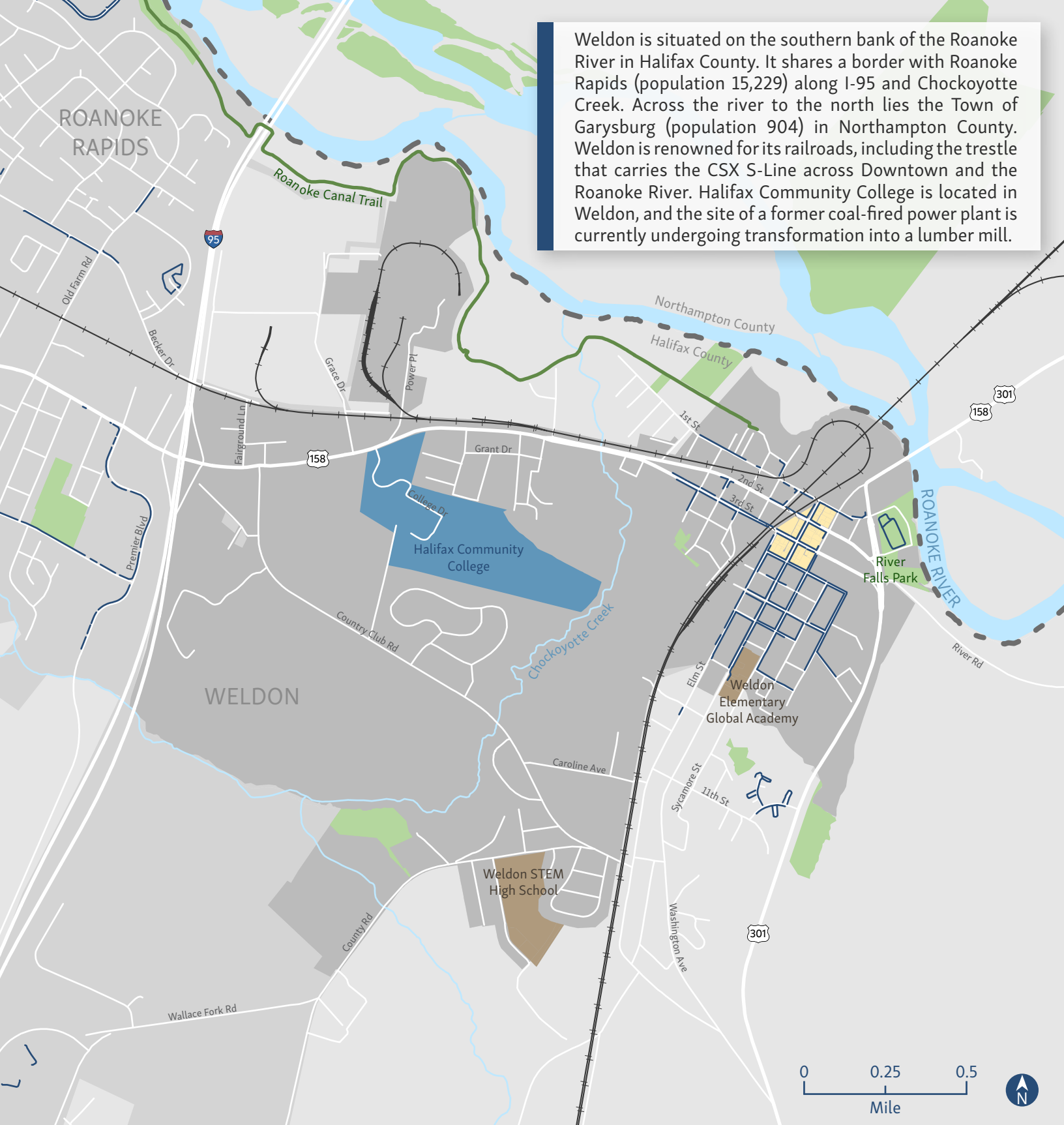
The Town of Weldon is a community of 1,668 residents near the Virginia state line in Halifax County, North Carolina. The adjacent Roanoke River has played an important role in its development. Founded in 1745, Weldon was the farthest navigable point on the river until the opening of the Roanoke Canal in 1823, which boosted the economy by opening the river to trade with Virginia. The Canal and its towpath now function as a 7.8-mile trail between Weldon and the City of Roanoke Rapids.

Another defining moment in Weldon’s history was the completion of the Wilmington & Weldon Railroad in 1841, which at the time was the longest railroad in the world. By the mid-1800s, Weldon was served by five railroads and had become a major transportation hub. However, the Civil War left some of these tracks in ruins, and railroad consolidation in the following decades further impacted Weldon. A new Weldon Union Station was opened in 1912 by the Seaboard Air Line and Atlantic Coast Line. The two companies later merged in 1967 and discontinued passenger service the following year. The tracks remain in use for freight and Amtrak traffic, and Union Station now functions as a library.

Weldon is presently known as the “Rockfish Capital of the World” due to the great abundance of striped bass in the Roanoke River, which draws many people to the area every spring. The Town is directly served by Interstate 95 and US-158 and is located approximately halfway between Raleigh and Richmond.



Weldon is situated on the southern bank of the Roanoke River in Halifax County. It shares a border with Roanoke Rapids (population 15,229) along I-95 and Chockoyotte Creek. Across the river to the north lies the Town of Garysburg (population 904) in Northampton County. Weldon is renowned for its railroads, including the trestle that carries the CSX S-Line across Downtown and the Roanoke River. Halifax Community College is located in Weldon, and the site of a former coal-fired power plant is currently undergoing transformation into a lumber mill.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Study Area

- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

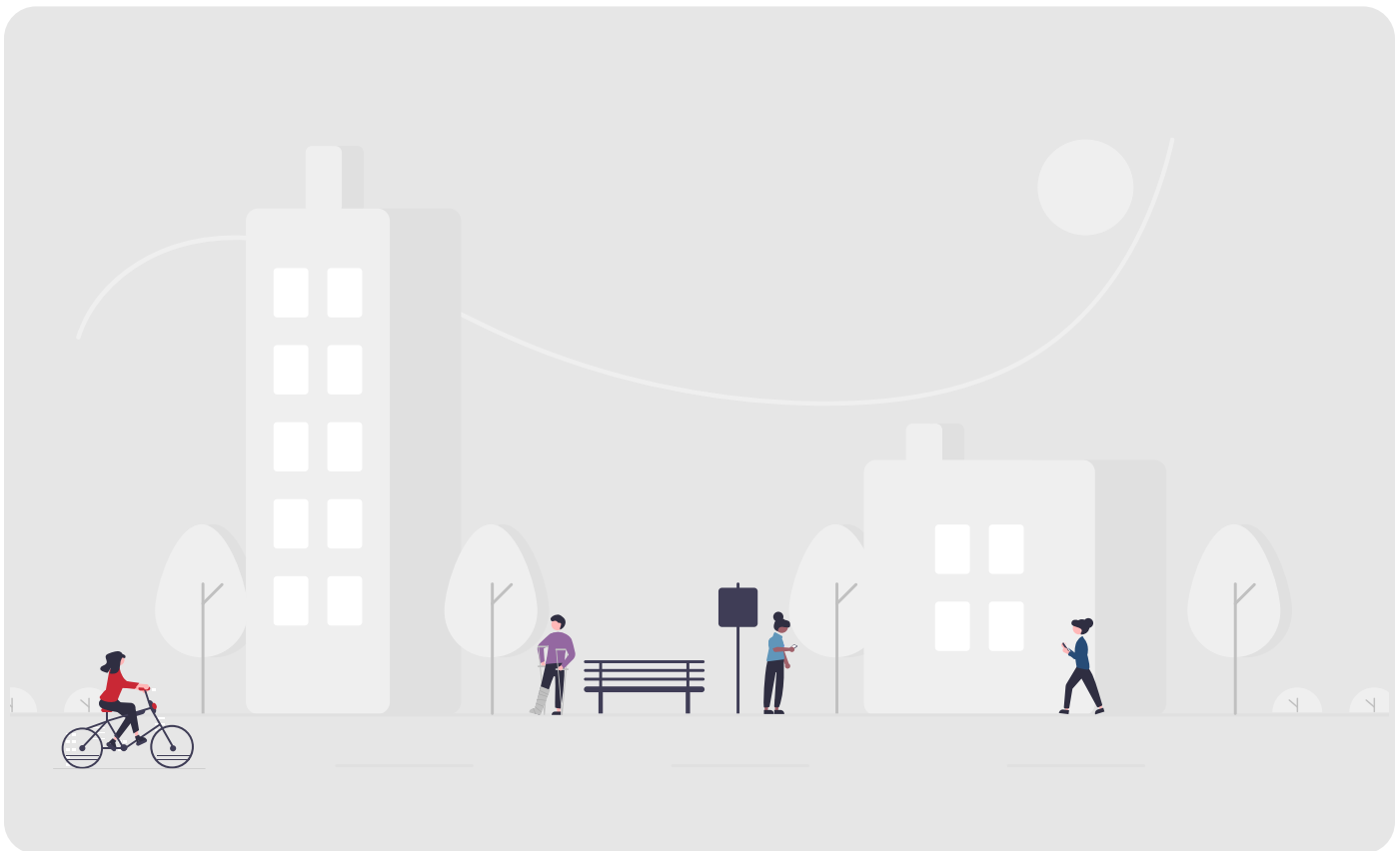
LEGEND ▶

COMMUNITY DEMOGRAPHICS

Analyzing the Town of Weldon's demographic trends are essential to planning the community's bicycle and pedestrian network. This analysis helps to inform the public engagement approach and to ensure proposed recommendations meet the diverse needs of people residing in Weldon. Demographic data was obtained from the United States Census Bureau. More specifically, the 2016-2020 American Community Survey 5-year estimates were used to inform the demographic analysis in this section of the plan.

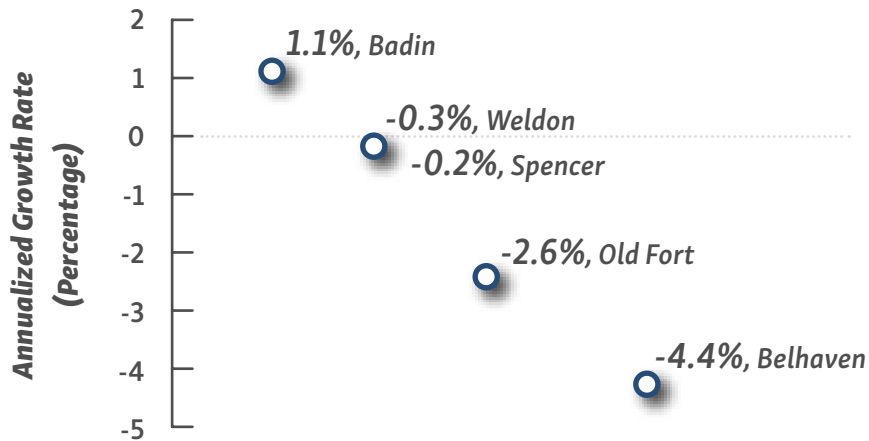
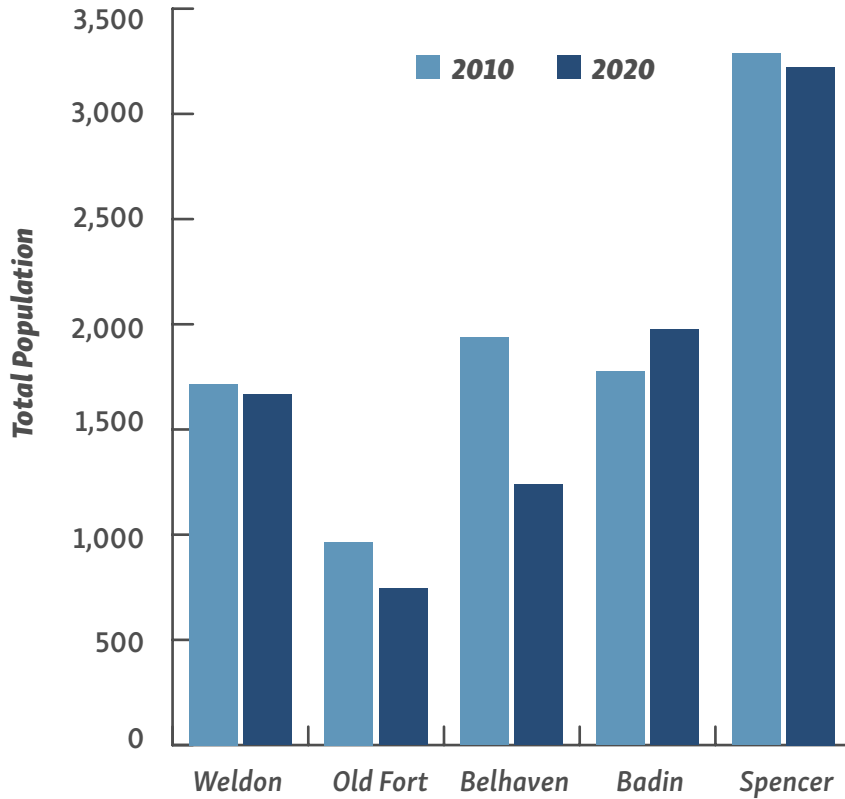
This section includes an analysis of the following factors:

- Population
- Race + ethnicity
- Age
- Education
- Households below the poverty level
- Housing + income
- Commute
- Vehicle availability
- Disabilities
- Employment density



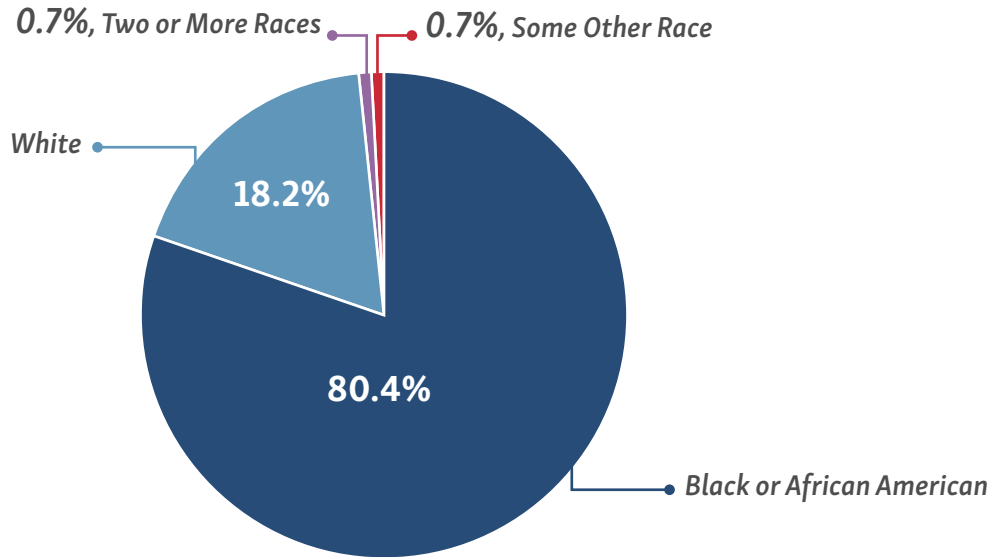
POPULATION

In 2020, the Town of Weldon had a population of 1,668. The Town experienced a decline in its growth rate (-0.3%) from previous data reporting years. By comparison, the populations of peer communities of Old Fort, Belhaven, Badin, and Spencer are 743, 1,239, 1,976, and 3,227 respectively. The Town of Weldon has an annualized population growth rate of -0.3 percent, which is similar to the Town of Spencer at -0.2 percent. The population comparisons for the peer communities are presented below.



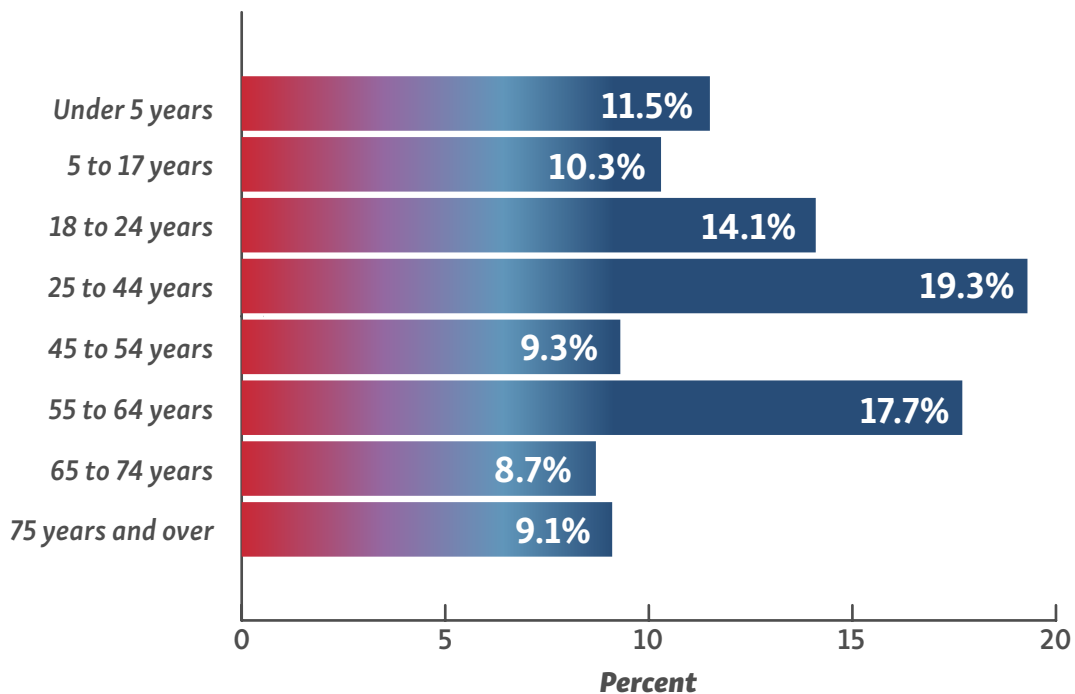
RACE + ETHNICITY

In North Carolina, approximately 67.6 percent of residents identify as “White alone,” and 21.4 percent of residents identify as “Black or African American.” By comparison, 18.2 percent of residents in the Town of Weldon identify as “White alone” and 80.4 percent of residents identify as “Black or African American.” Approximately 3.0 percent of the state’s population identifies as “Asian alone,” while none of Weldon’s residents identify as “Asian alone.” Only 1.2 percent of Weldon’s residents identify as “Hispanic” which is significantly lower than the state’s average, which is 9.5 percent of the statewide population.



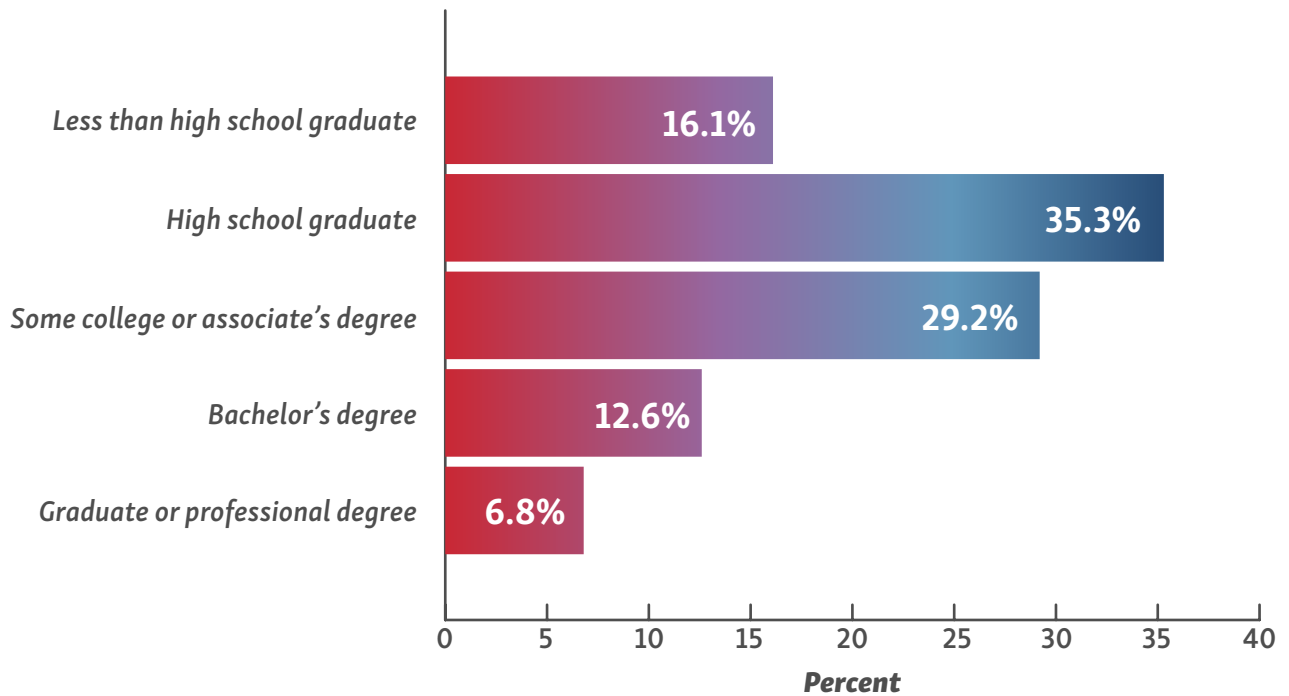
AGE

Weldon’s residents are slightly younger on average than North Carolina overall. The median age in the Town of Weldon is 36.8, while North Carolina has a median age of 38.9. Approximately 22 percent of the population is under the age of 18, and about 18 percent of the population is over the age of 65. Therefore, about 60 percent of the population is between the ages of 18 and 64.



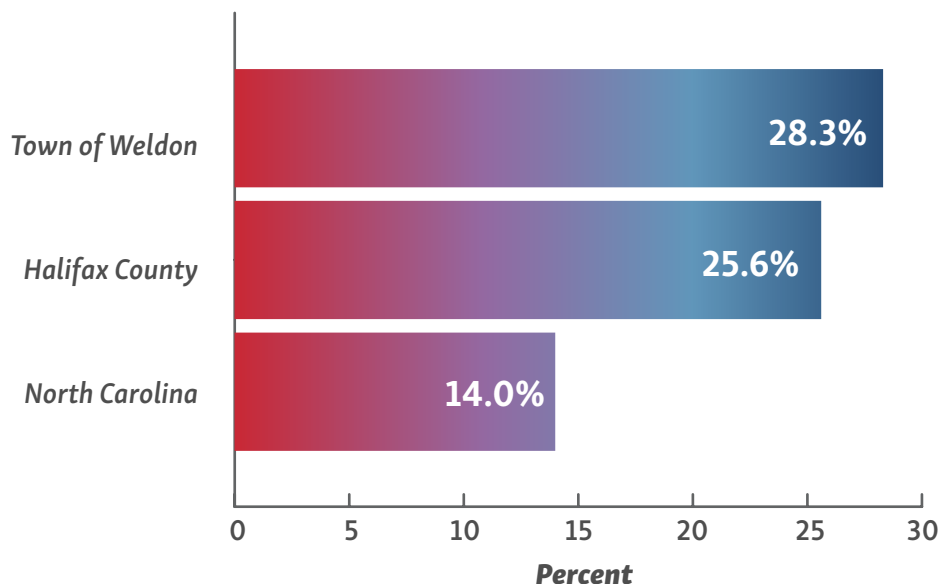
EDUCATIONAL ATTAINMENT

Thirty-five percent (35.3%) of Weldon's residents are high school graduates. This is higher than the state's average of 26.5 percent. Halifax Community College is located in Weldon and may support the 29.2 percent of residents that attended some college or obtained an associate degree.



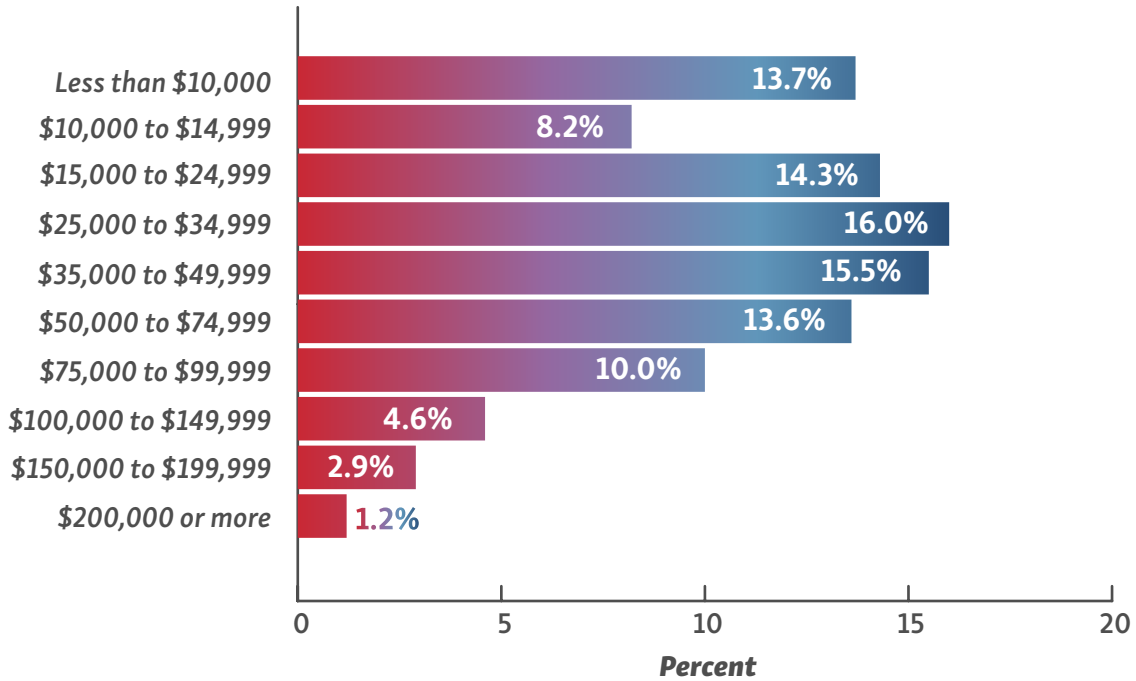
HOUSEHOLDS BELOW THE POVERTY LEVEL

Approximately 28 percent of residents within the Town of Weldon are living below the poverty level. This is comparable to the Halifax County poverty rate which is 25.6 percent of the population but double the North Carolina poverty level of 14.0 percent. Residents for whom poverty status is determined often have greater difficulty affording and maintaining vehicles and are more dependent on public transportation, walking, and biking. Investments in active transportation can help provide accessible and affordable travel options for all residents of Weldon.



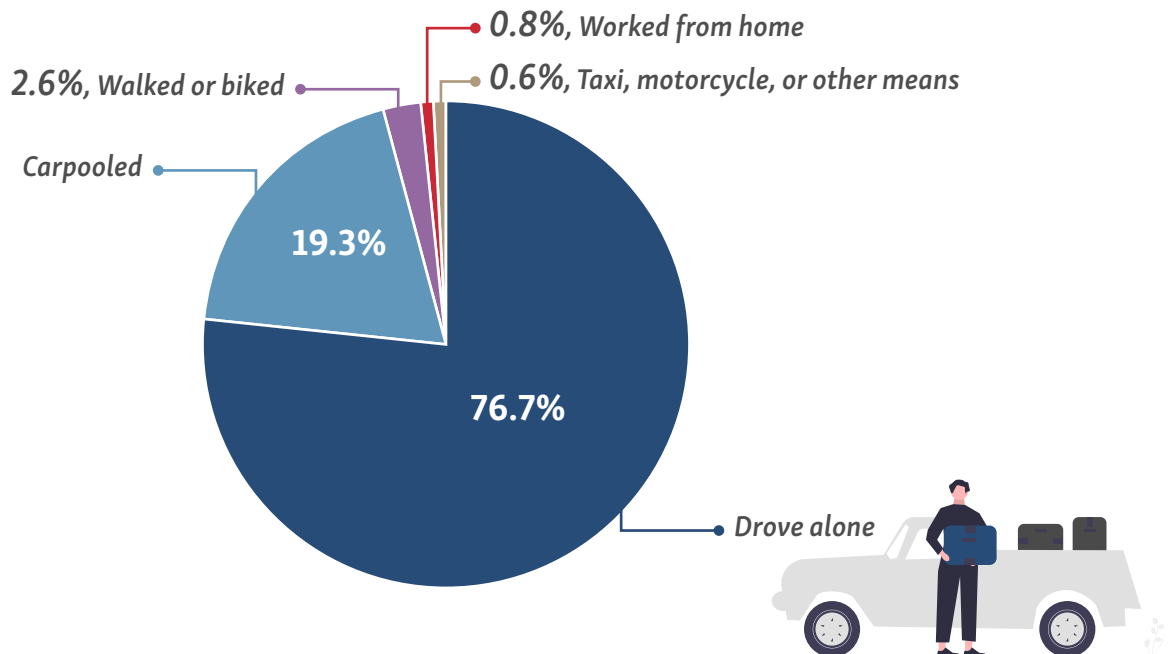
HOUSING + HOUSEHOLD INCOME

Weldon’s median household income is \$38,333 which is significantly lower than that of North Carolina’s which is \$56,642. Sixteen percent (16%) of Weldon’s residents make between \$25,000 and \$34,999 a year, while only 1 percent (1.2%) make \$200,000 or more a year. Additionally, the percentage of vacant housing units in the Town are almost double (22.6%) that of the state’s at 14.0 percent.



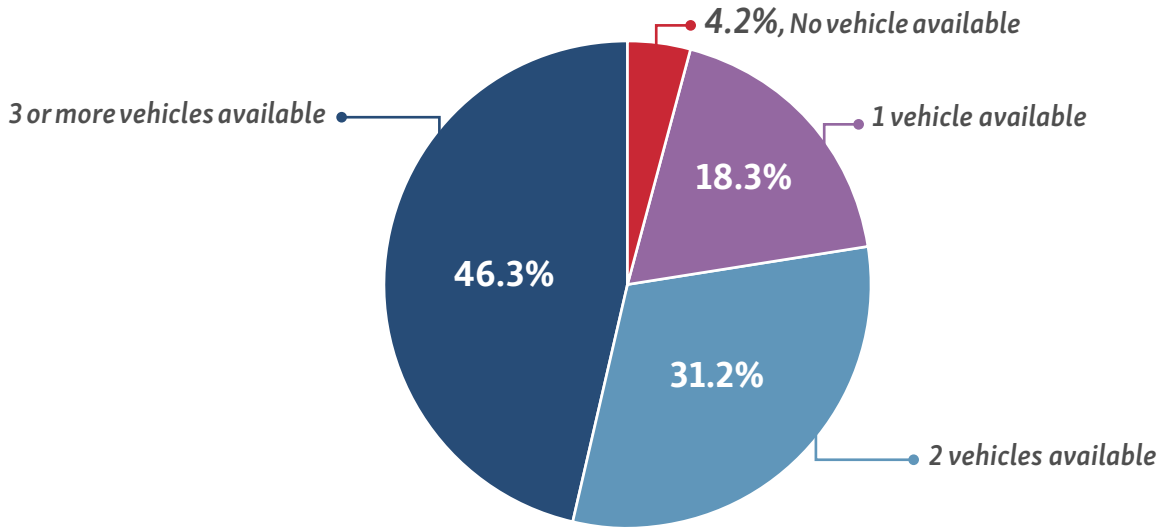
COMMUTING MODES + RATES

Weldon’s residents predominantly commute by single-occupancy vehicle, with 76.7 percent of workers driving alone to work, and of those workers, the mean commute time is about 21 minutes. Only 2.6 percent of workers commute by riding a bike or walking. Additional transportation modes are displayed in the graphic below. Since investments in multi-modal transportation often result in higher levels of residents participating in active commute mode shares, the development of Weldon’s bicycle and pedestrian network may increase active transportation commute rates if the projects presented in this plan are implemented in the future.



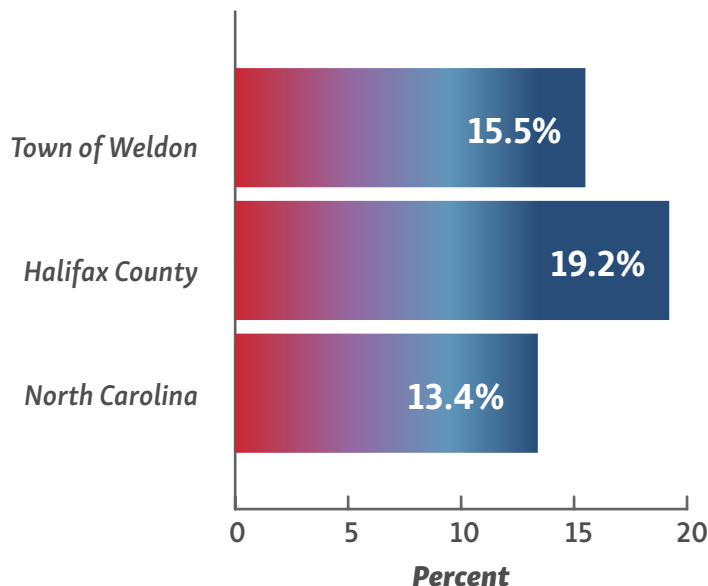
VEHICLE ACCESS

Most commuters in the Town of Weldon have access to a vehicle, with only 4.2 percent of workers reporting no vehicles available within their housing units. This statistic shows the importance of providing accessible and useful travel options via investments in active transportation infrastructure. Over 46 percent (46.3%) of Weldon's residents have access to two or more vehicles and 18.3 percent of residents have access to one vehicle.



DISABILITY

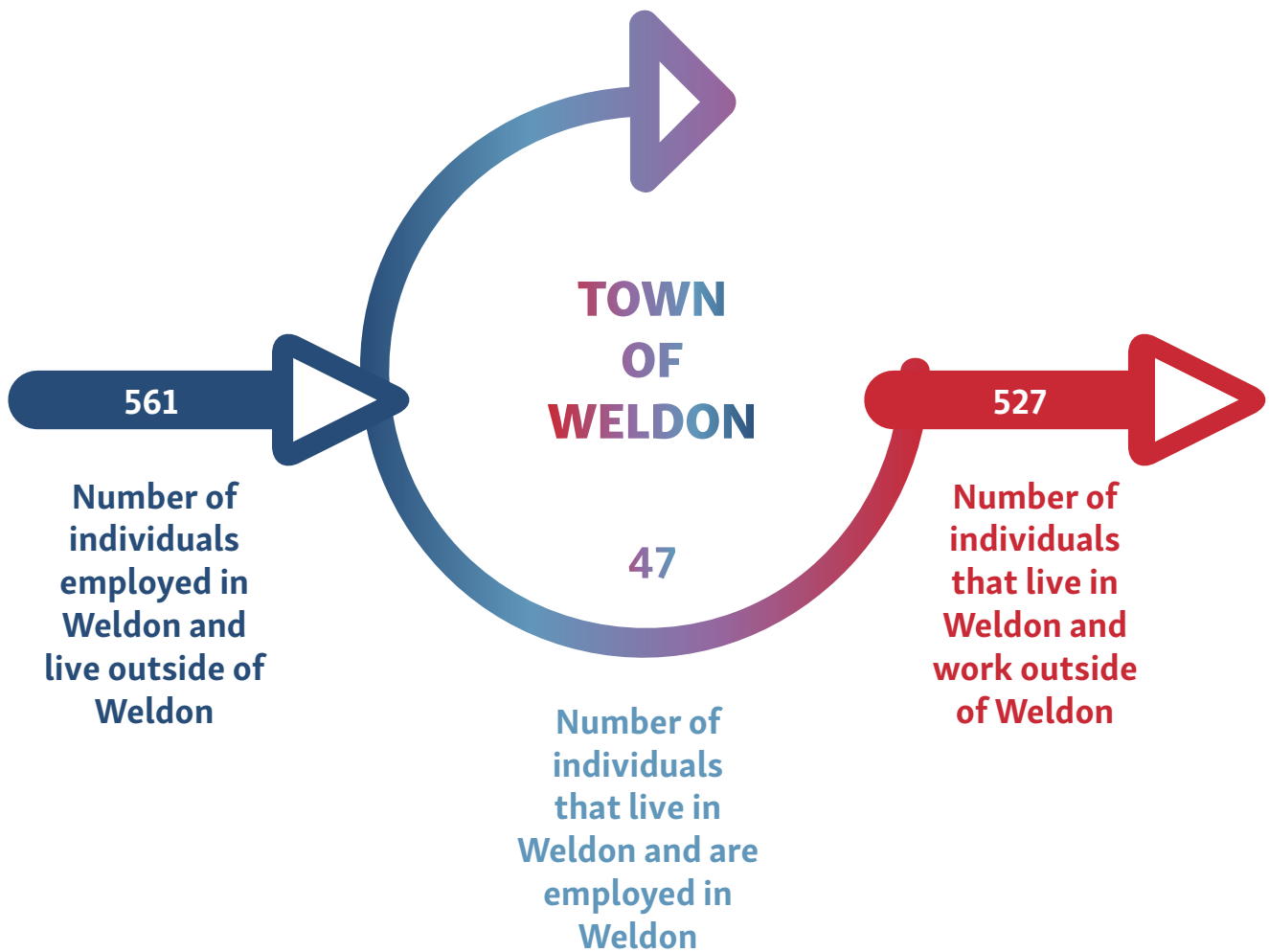
Approximately 16 percent (15.5%) of Town of Weldon residents report having a disability, a figure in line with the rates of 19.2 percent in Halifax County and 13.4 percent at the state level. The inadequacies of transportation networks in many North Carolina communities disproportionately affect vulnerable residents, including those with disabilities. It is important for the Town to prioritize the design and construction of accessible infrastructure, ensuring it accommodates residents of all ages and abilities.



EMPLOYMENT DENSITY

For all jobs reported in 2019 in Halifax County, census data indicates that manufacturing is the most popular industry sector at 15.0 percent. Within the Town of Weldon, jobs are densely located in the downtown area and near Halifax Community College.

According to the United Census Bureau, 561 individuals employed within the Town of Weldon live outside of the Town, while 527 individuals live within the Town but work elsewhere. Only 47 individuals live and work within the Town. Many jobs in Halifax County are located in neighboring Roanoke Rapids including ECU Health North Hospital, which was the largest employer in Halifax County in 2021 (employment range 500-999).



A cluster of employment centers is located in Downtown Weldon, with most employing 100 people or fewer. Jobs are concentrated on the US-158 corridor as it approaches Interstate 95 and Roanoke Rapids. These include several manufacturing operations north of the railroad corridor, retail and service industry operations, and a community college. Neighboring Roanoke Rapids is home to large retail employers. Halifax County's largest employer, ECU Health, is also located in Roanoke Rapids.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Employment Density (All Jobs 2019)

- 1-100 Jobs
- 101-500 Jobs
- 501-1,000 Jobs
- 1,001 or more Jobs
- Existing Shared Use Path
- Existing Sidewalk
- + Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶

ACTIVE TRANSPORTATION BENEFITS

Future construction of the bicycle and pedestrian facilities recommended in this Plan will provide numerous benefits to its users. Benefits achievable from these active transportation facilities in the Town of Weldon include but are not limited to the following: mobility and connectivity, increased safety, improved health and well-being, positive environmental and economic impacts, as well as the promotion of equity and accessibility.

WalkBikeNC, the statewide bicycle and pedestrian plan, established a vision for active transportation within North Carolina centered around the following five key benefits: health, economic competitiveness, safety, mobility, and stewardship. This plan suggests that the integration of bicycle and pedestrian accommodations promotes the following community benefits:

- Promotes physical, mental and social well-being by providing opportunities for outdoor recreation and community gathering spaces
- Provides easy walking or bicycling connections between neighborhoods, schools, parks, and trails
- Increases property values, promotes tourism, and supports local businesses by encouraging foot traffic
- Improves overall quality of life for its users

Active transportation facilities...



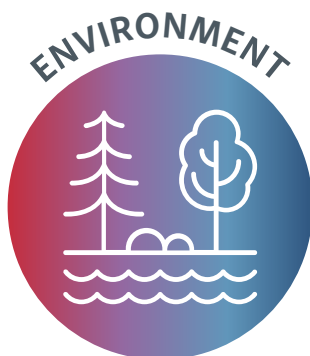
...contribute to an increase in multi-modal network connectivity and modes shift to bicycle and pedestrian commuting and travel.



...contribute to a reduction in bicycle and pedestrian crashes and lead to an increase in biking and walking as a result of safety enhancements.



...provide access to biking and walking, which lead to increased physical activity, improved mental well-being, decreased risks of chronic disease, and a reduction in healthcare costs.



...contribute to a reduction in greenhouse gas emissions, reduction in vehicle miles traveled, preserves wildlife habitats and natural areas, and improves water quality.



...contribute to increased tourism, increased sales revenue, and increased property values, which lead to job creation and business growth.



...contribute to a reduction of household transportation costs; expand access to jobs, services, and recreation; and provide first and last mile connections to transit.



MOBILITY + CONNECTIVITY

More than 45 percent of all driving trips in the United States are under 3 miles, and 60 percent of trips are less than 5 miles. These trips, which could be taken by bike or on foot in 20 to 30 minutes, represent opportunities for mode shifts to biking and walking in communities across the United States.

The design of complete streets, through NCDOT's Complete Street Policy, can help accommodate active transportation facility design in future roadway projects across the state. Providing active transportation facilities within close proximity to multiple points of interest and desired destinations can reduce the number of short motor vehicle trips.



SAFETY

The lack of bicycle and pedestrian infrastructure in communities across North Carolina poses safety risks for those who commute by active modes. In 2019, there were 6,205 pedestrians, 846 bicyclists, and 287 other non-motorists (e.g., persons riding micromobility devices) killed in crashes with motor vehicles in the United States. Factors that contribute to unsafe road conditions for bicyclists and pedestrians include the lack of separation from vehicular traffic, unprotected intersections, and poor bicycle and pedestrian connectivity.

When transportation networks are designed for all modes, bicyclists and pedestrians become less vulnerable to collisions with motorists and rates of bicycling and walking increase. In a National Association of City Transportation Officials (NACTO) study of seven cities that expanded their bikeway networks by 50 percent between 2007–2014, ridership more than doubled while risk of death and serious injury to people biking was halved.

Between 2015 and 2019, crash data reported five total crashes involving pedestrians. In 2016, A bicyclist was injured with suspected minor injuries. Design enhancements to active transportation infrastructure can help make users more visible to drivers. The implementation of bicycle and pedestrian programming can also educate both drivers and active transportation users on applicable laws and regulations associated with their respective facilities.



HEALTH + WELL-BEING

According to the USDOT, approximately one in four adults in the United States report that they do not engage in any physical activity outside of their jobs. The US Department of Health and Human Services (DHHS) estimates that inactivity contributes to \$117 billion in annual healthcare costs nationwide and causes nearly 10 percent of premature mortality. To improve and support physical and mental health, DHHS recommends adults get at least 150 minutes of moderate-intensity exercise, like brisk walking or bicycling, per week. When local municipalities invest in active transportation facilities like shared-use paths, bicycle lanes, and sidewalks, they invest in the overall health of their communities. These facilities provide a safe and comfortable way for people to engage in regular physical activity. Providing walkable and bikeable access to key destinations such as schools, shopping areas, jobs, neighborhoods, and recreation facilities promotes active lifestyles, provides opportunities for residents to meet health goals, and reduces dependence on automobiles.

In the 2018 report, *Evaluating the Economic Contribution of Shared Use Paths in NC*, researchers found that trail users generated an estimated 21.2 million miles of bicycling and 9.8 million miles of walking, spurring 5.4 million hours of physical activity per year. This activity is estimated to save approximately \$300,000 in healthcare related costs annually in the communities studied.



ENVIRONMENT

Between 1990 and 2018, greenhouse gas emissions due to transportation increased more than any other sector. Twenty-eight percent (28%) of all emissions are attributed to transportation-related activities. Passenger cars and trucks account for 60 percent of those emissions. Active transportation infrastructure encourages stewardship of our natural resources by providing communities with a fossil fuel-free alternative mode of transportation.

In addition, investments in active transportation also expands access to these facilities and leads to a reduction in vehicle miles traveled and CO₂ emissions. According to the 2018 report, *Evaluating the Economic Contribution of Shared Use Paths in NC*, researchers found that low-impact travel along the Duck Trail, Brevard Greenway, and Little Sugar Creek Greenway lead to an annual reduction of 53.7 million pounds of CO₂ emissions and 686,000 pounds of motor vehicle emissions, resulting in an annual environmental cost savings of \$707,000.

Focused efforts on reducing vehicle miles traveled (VMT) results in fewer negative environmental impacts. Shorter trips, such as using bicycles or traveling by foot are less polluting than long car trips on a per-mile basis. This difference can be attributed to higher emissions associated with “cold starts” in vehicles during the first few minutes of travel before pollution control devices begin to work effectively.



ECONOMY

North Carolina communities, especially those in rural areas, face a multitude of challenges in attracting sustainable economic development. Greenways, as well as bicycle and pedestrian facilities are proven economic generators and create unique opportunities for communities to expand their tourism industry, create jobs, and support local businesses. In *Evaluating the Economic Contribution of Shared Use Paths in NC (2018)*, researchers found that greenways provide significant economic benefits to communities. Trail users along the American Tobacco Trail, Brevard Greenway, Little Sugar Creek Greenway, and Duck Trail made purchases at businesses along these trails, which increased sales revenue and contributed to job creation in local communities. The sale revenue generated from these purchases is \$19.5 million annually and helps support 261 jobs each year. Additionally, the study found for every \$1 spent on trail construction, the return of investment from those benefits is \$1.72 annually.

Another inspiring example of the economic impact of trails is the Great Allegheny Passage (GAP), a 150-mile rail-trail connecting cities and towns between Pittsburgh, Pennsylvania, and Cumberland, Maryland. The 2015 Trail Towns Program report on trail usage and business growth along the GAP found that the estimated direct economic impact is \$50 million annually. With an estimated 1 million visitors per year, the regional economy is thriving. Residents who once had limited access to jobs and economic opportunities are now able to invest in local businesses along the trail. When over 100 continuous miles of the GAP were completed in 2007, there was a net gain of 65 new businesses and 25 business expansions from 2007 to 2014, which resulted in the creation of over 270 new jobs.



ACCESSIBILITY + EQUITY

Shortfalls of the transportation network in most North Carolina communities disproportionately impact vulnerable residents. People who rely on transit, biking, and walking live in areas that often lack access to safe and convenient multi-modal infrastructure, which diminishes access and opportunity to employment, services, and recreation. According to United States Census Bureau (ACS 5-year estimates), 28.3 percent of households in the Town of Weldon live near or below the poverty line, placing them at a significant disadvantage for accessing local jobs and services. The Town's poverty rate is double that of the state which is at 14.0 percent.

Ensuring residents have access to transportation that is affordable and convenient is fundamental to efforts reducing income inequality. Newly established bicycle and pedestrian connections within Weldon will expand access to locations including but not limited to employment centers, medical facilities, schools, and parks. In the 2018 report, *Evaluating the Economic Contribution of Shared Use Paths in NC*, researchers found that trail users in the communities studied reduced their annual transportation costs by \$1.83 million by having access to multimodal facilities to travel more frequently and safely by foot or by bike.

RESOURCES

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Weldon Mills Distillery at River Falls Park



02

Existing Conditions



OVERVIEW

The *Town of Weldon Project Acceleration Plan* surveyed existing conditions, reviewed relevant planning efforts and policies, and includes an Opportunities and Constraints Analysis to gain a better understanding of the needs for the study area. The findings gathered during this analysis were used to inform policy, program, and network recommendations made in the Plan.

The following sections are included in this chapter:

- Existing Facilities
- Previous Planning Efforts
- Policy Review
- Opportunities + Constraints

EXISTING FACILITIES

Following data collection, the project team conducted a one-day site tour on March 28, 2023, to gather additional data for the Plan. The team documented current conditions for bicyclists and pedestrians within the local transportation system, including existing and off-street networks and facilities, as well as the overall transportation network. Additional documentation was taken on existing facilities along road corridors and at intersections, roadway widths, shoulder treatments, lane markings, and posted speed limits. During the site visit, the team also made note of opportunities and constraints and observed maintenance issues and problem intersections or crossings. This information informed the Opportunities and Constraints Analysis, which is included at the end of this chapter. Photos from the site visit are provided within the Opportunities and Constraints Analysis with accompanying notes and captions.

This section contains annotated maps that cover the following topics within each consideration focus area:



Human Environment Considerations

- Existing + Proposed Active Transportation Facilities
- Bicycle + Pedestrian Crashes
- Traffic Volume
- Speed Limit
- Right-of-Way (ROW)
- NCDOT STIP Projects
- NCDOT HMIP Projects
- Historic Resources



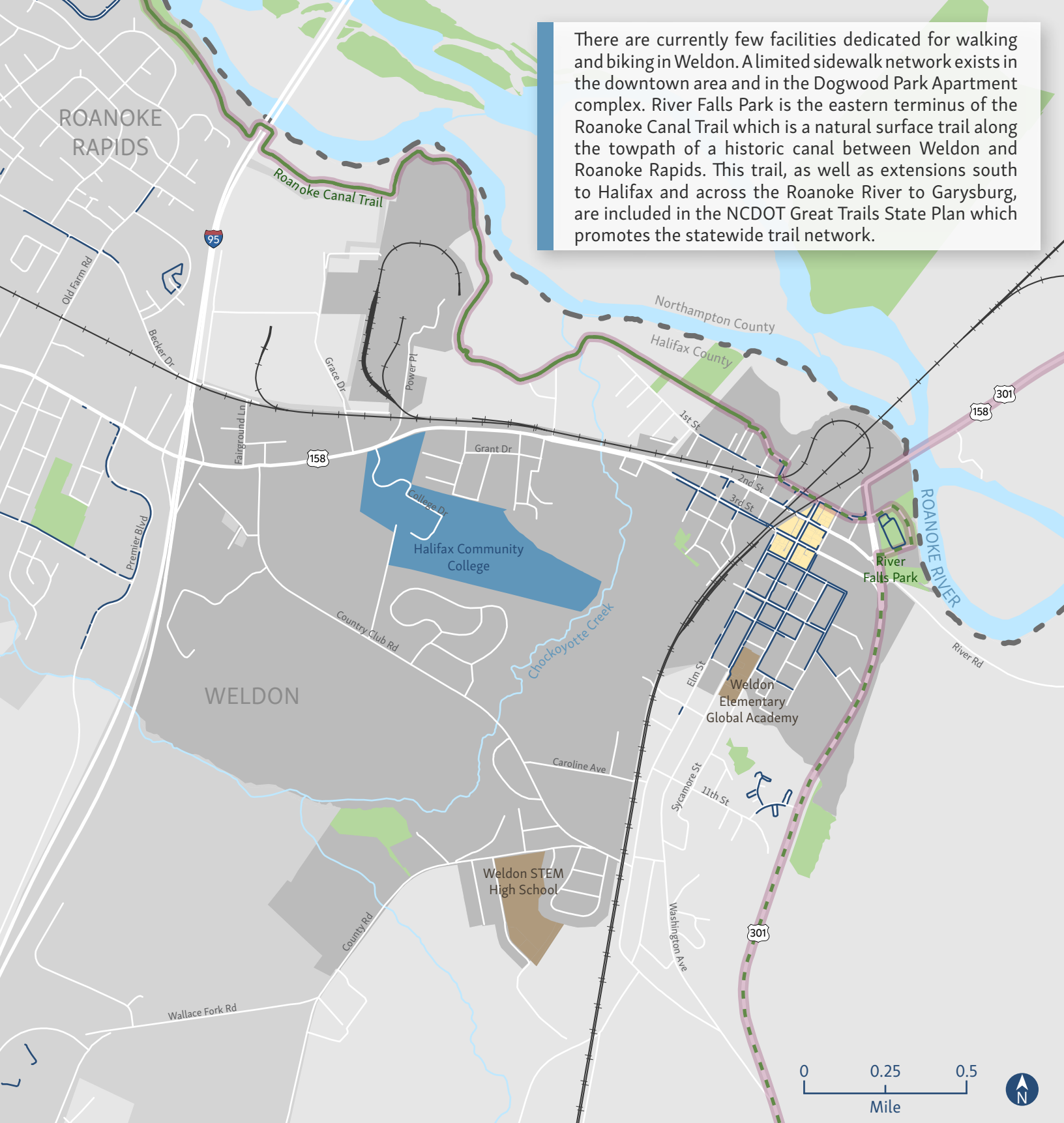
Natural Environment Considerations

- Floodplains + Wetlands
- Topography



Weldon residents are at risk when they must walk on the roadway in areas without sidewalks.

There are currently few facilities dedicated for walking and biking in Weldon. A limited sidewalk network exists in the downtown area and in the Dogwood Park Apartment complex. River Falls Park is the eastern terminus of the Roanoke Canal Trail which is a natural surface trail along the towpath of a historic canal between Weldon and Roanoke Rapids. This trail, as well as extensions south to Halifax and across the Roanoke River to Garysburg, are included in the NCDOT Great Trails State Plan which promotes the statewide trail network.



TOWN OF WELDON

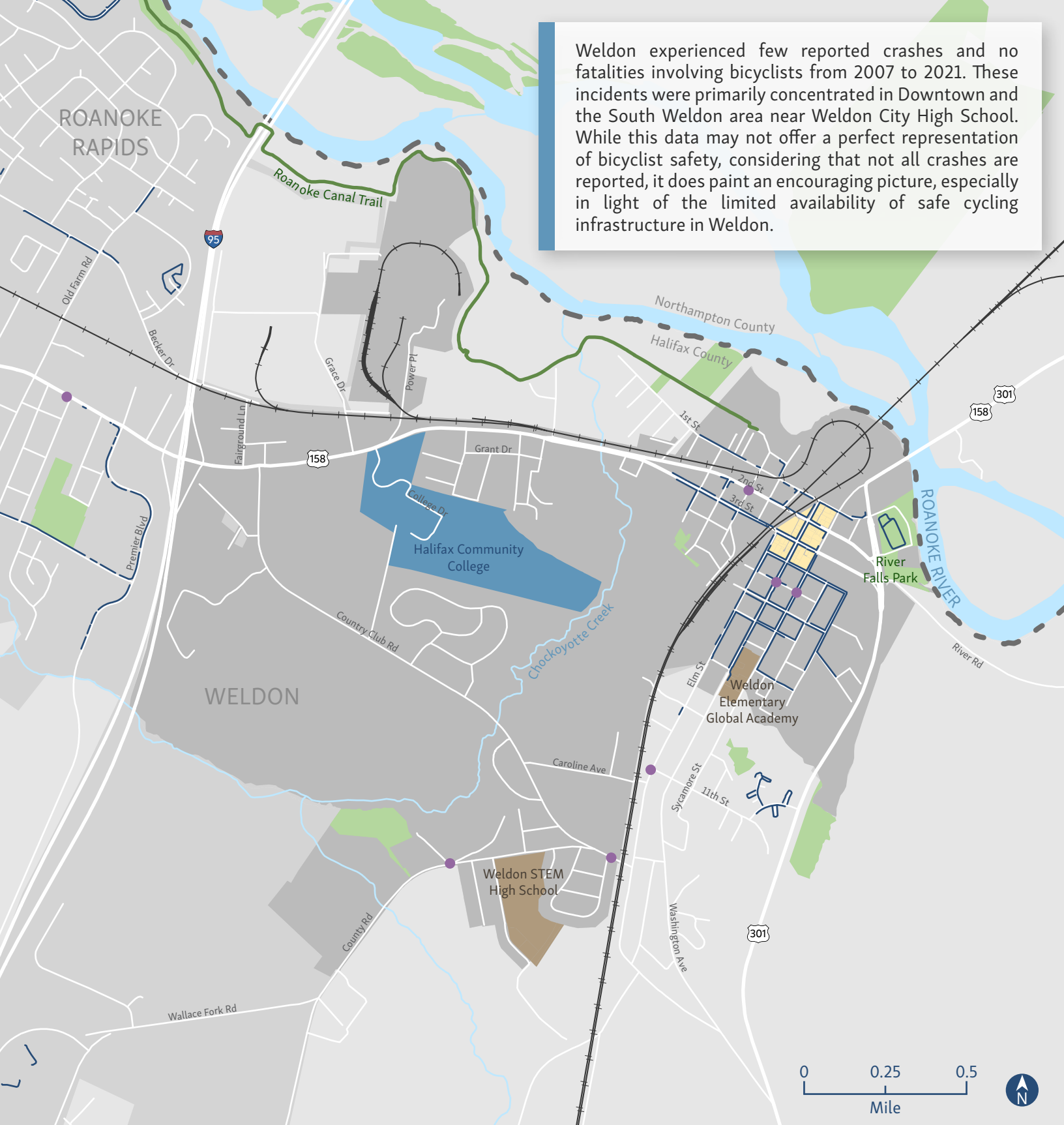
PROJECT ACCELERATION PLAN

Existing + Proposed Bicycle and Pedestrian Facilities

LEGEND ▶

- - - Proposed Shared Use Path
- Existing Shared Use Path
- Existing Sidewalk
- NCDOT Great Trails State Route
- + + + Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

Weldon experienced few reported crashes and no fatalities involving bicyclists from 2007 to 2021. These incidents were primarily concentrated in Downtown and the South Weldon area near Weldon City High School. While this data may not offer a perfect representation of bicyclist safety, considering that not all crashes are reported, it does paint an encouraging picture, especially in light of the limited availability of safe cycling infrastructure in Weldon.



TOWN OF WELDON

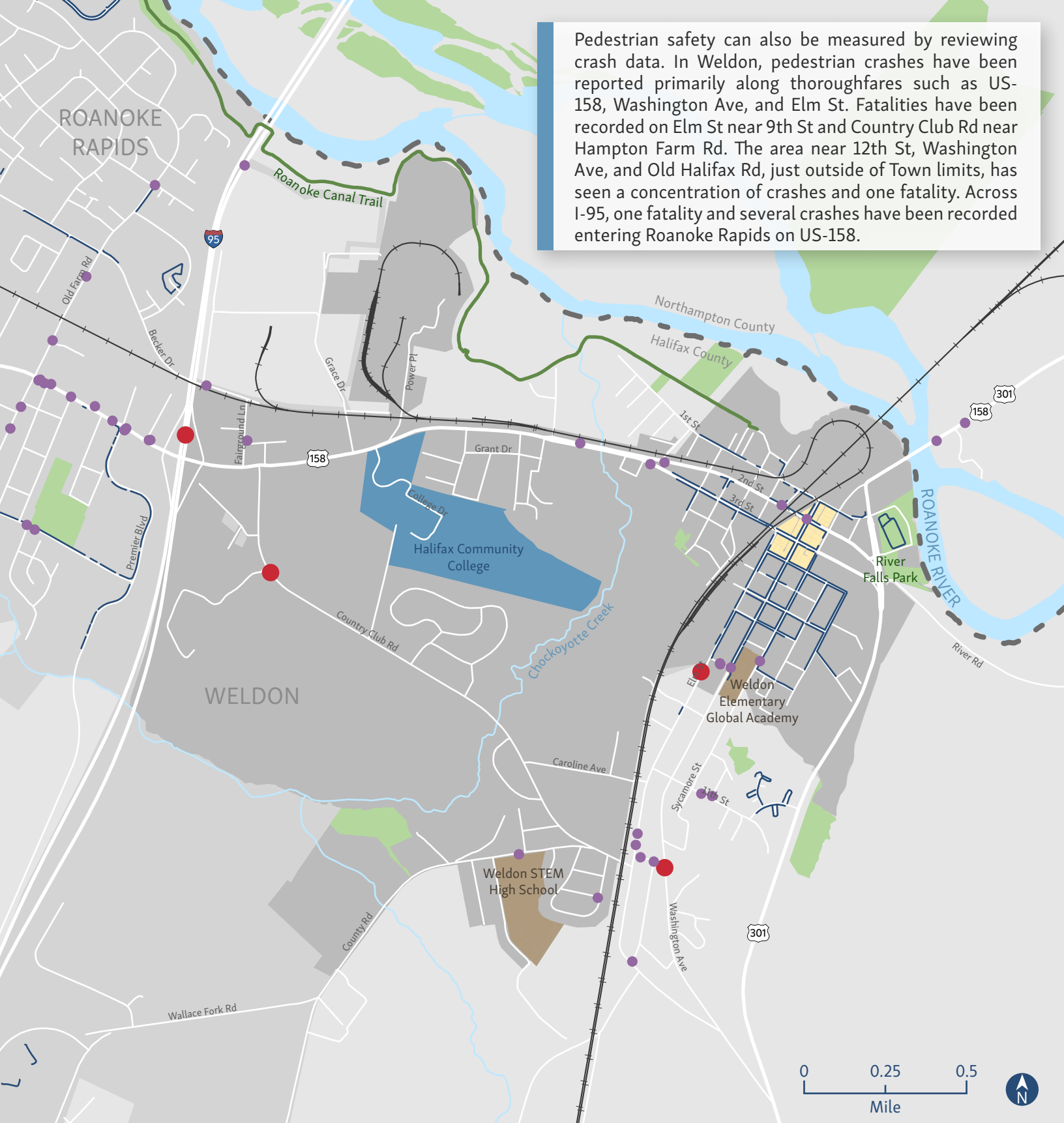
PROJECT ACCELERATION PLAN

Bicycle Crashes (2007-2021)

- Bicycle Crash
- Existing Shared Use Path
- Existing Sidewalk
- +— Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶

Pedestrian safety can also be measured by reviewing crash data. In Weldon, pedestrian crashes have been reported primarily along thoroughfares such as US-158, Washington Ave, and Elm St. Fatalities have been recorded on Elm St near 9th St and Country Club Rd near Hampton Farm Rd. The area near 12th St, Washington Ave, and Old Halifax Rd, just outside of Town limits, has seen a concentration of crashes and one fatality. Across I-95, one fatality and several crashes have been recorded entering Roanoke Rapids on US-158.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Pedestrian Crashes (2007-2021)

LEGEND ▶

- Pedestrian Crash
- Pedestrian Fatality
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

Traffic volume is a factor in planning bicycle and pedestrian infrastructure. Many primary roads in Weldon are maintained by NCDOT, which records Annual Average Daily Traffic (AADT) volumes for some corridors. US-158 east of I-95 is the busiest roadway in Weldon with an average of 16,000 vehicles per day. Many roads in Weldon carry only light traffic, which makes them more suitable for bicycle and pedestrian infrastructure. Country Club Rd, Washington Ave, and US-301 south of 2nd St each carry fewer than 5,000 vehicles per day.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

NCDOT Traffic Volume (2021 AADT)

- 1-2,000 AADT
- 2,001-6,000 AADT
- 6,001-12,000 AADT
- 12,001-18,000 AADT
- 18,001 AADT or more
- Existing Shared Use Path
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶

Traffic speed is a major consideration in the development of bicycle and pedestrian facilities as high-speed traffic makes many users uncomfortable. In Weldon, US-301 south of 3rd St is posted at 55 MPH. Other corridors, including US-301 north of 3rd St, County Rd, and Country Club Rd, are posted at 45 MPH. US-158 gradually drops from 45 MPH to 25 MPH as it enters Downtown Weldon. Old Halifax Rd, Grace Dr, and Washington Ave south of Weldon Elementary, are posted at 35 MPH, while most residential streets are 25 MPH.

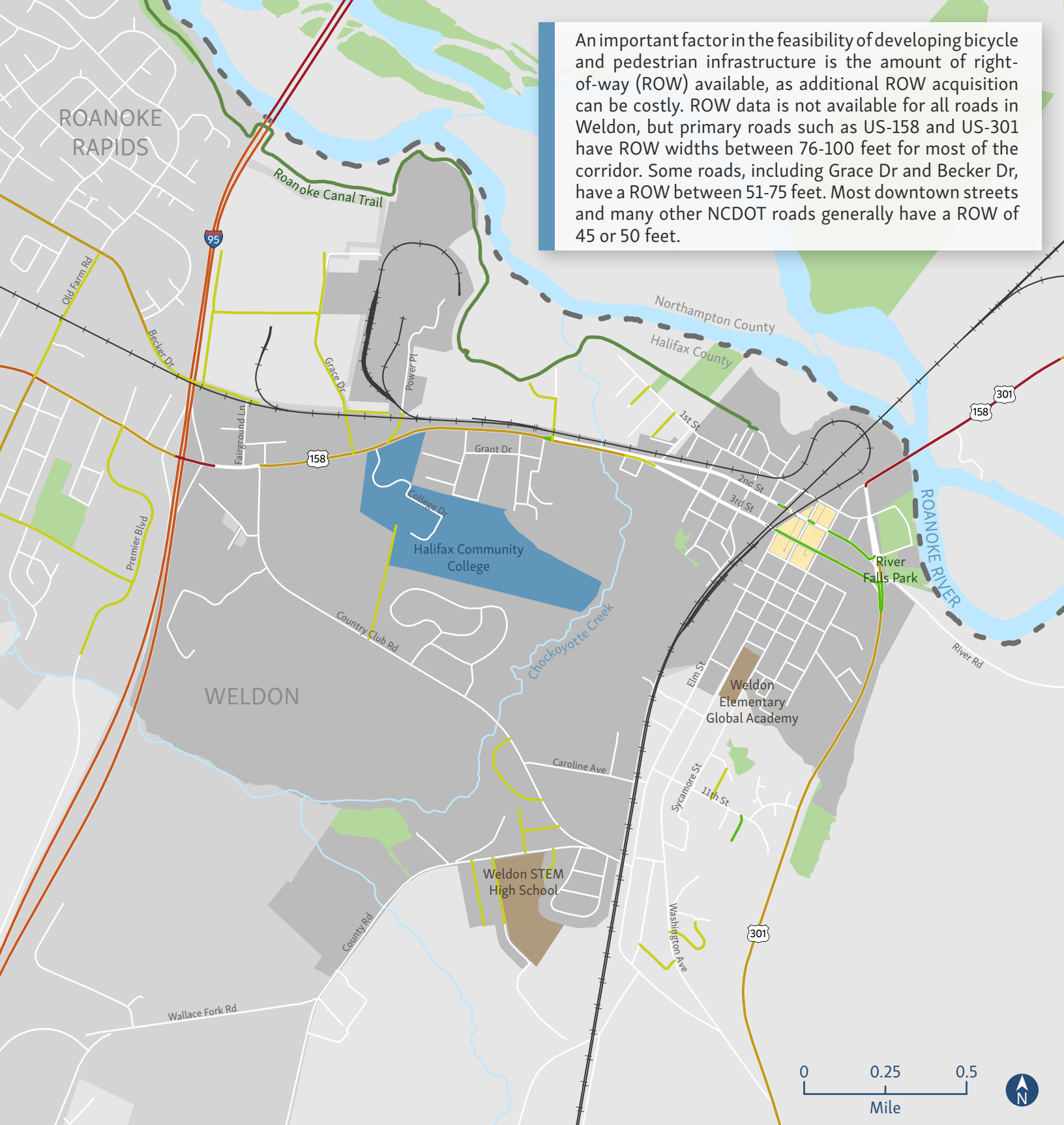


TOWN OF WELDON PROJECT ACCELERATION PLAN NCDOT Speed Limits

- 25 MPH or less
- 26-35 MPH
- 36-45 MPH
- 46-55 MPH
- 56-70 MPH
- Existing Shared Use Path
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶

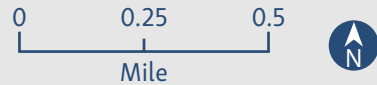
An important factor in the feasibility of developing bicycle and pedestrian infrastructure is the amount of right-of-way (ROW) available, as additional ROW acquisition can be costly. ROW data is not available for all roads in Weldon, but primary roads such as US-158 and US-301 have ROW widths between 76-100 feet for most of the corridor. Some roads, including Grace Dr and Becker Dr, have a ROW between 51-75 feet. Most downtown streets and many other NCDOT roads generally have a ROW of 45 or 50 feet.



TOWN OF WELDON PROJECT ACCELERATION PLAN NCDOT Right-of-Way

- | | | | |
|--|--------------------------|--|---------------------|
| | 50 feet or less | | Railroad |
| | 51-75 feet | | Roadway |
| | 76-100 feet | | Downtown Weldon |
| | 101-150 feet | | School |
| | 151 feet or more | | College |
| | Existing Shared Use Path | | Park/Conserved Land |
| | | | Water |
| | | | Weldon Town Limits |
| | | | Roanoke Rapids |
| | | | County Boundary |

LEGEND ▶

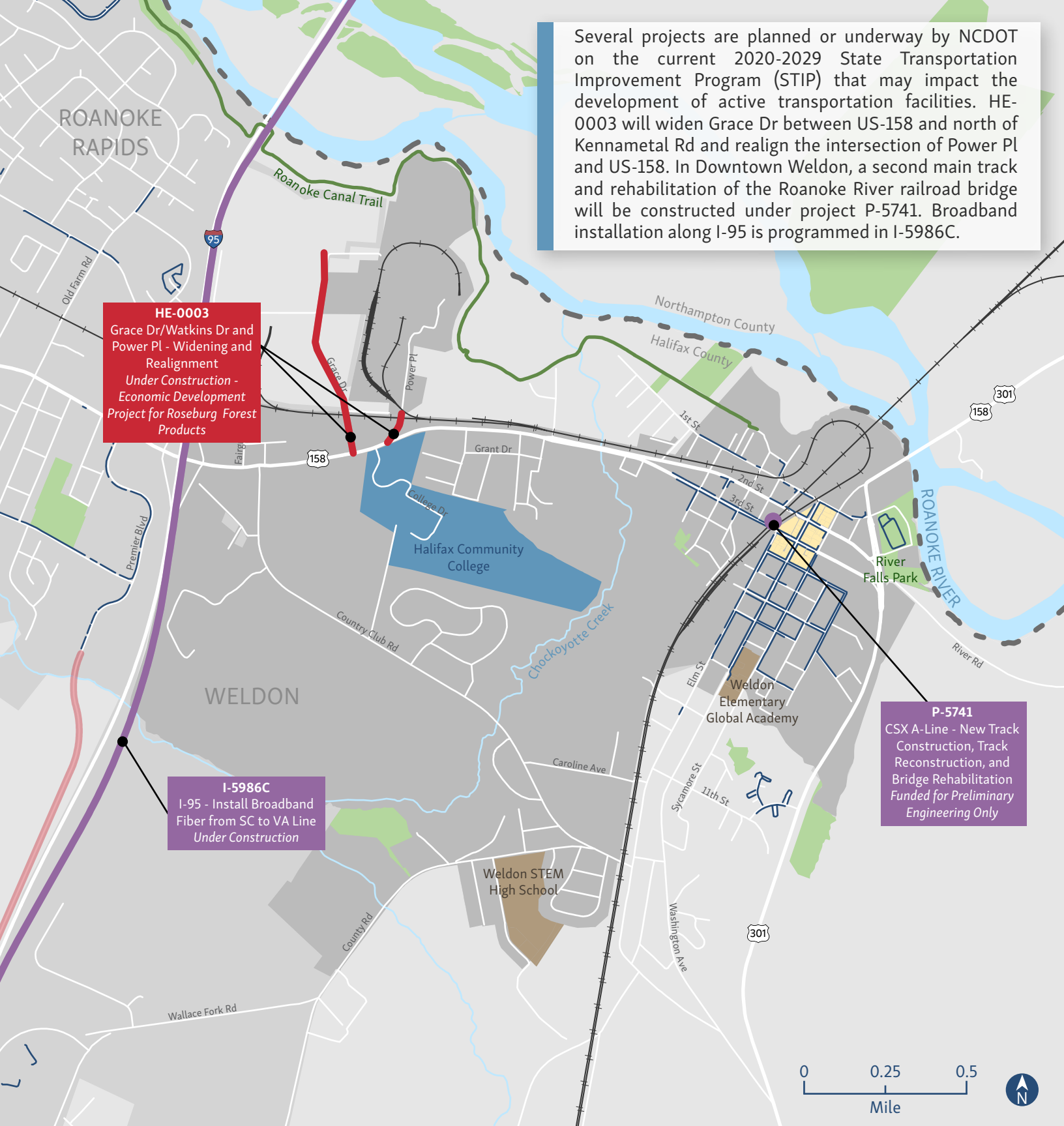


Several projects are planned or underway by NCDOT on the current 2020-2029 State Transportation Improvement Program (STIP) that may impact the development of active transportation facilities. HE-0003 will widen Grace Dr between US-158 and north of Kennametal Rd and realign the intersection of Power Pl and US-158. In Downtown Weldon, a second main track and rehabilitation of the Roanoke River railroad bridge will be constructed under project P-5741. Broadband installation along I-95 is programmed in I-5986C.

HE-0003
 Grace Dr/Watkins Dr and Power Pl - Widening and Realignment
Under Construction - Economic Development Project for Roseburg Forest Products

I-5986C
 I-95 - Install Broadband Fiber from SC to VA Line
Under Construction

P-5741
 CSX A-Line - New Track Construction, Track Reconstruction, and Bridge Rehabilitation
Funded for Preliminary Engineering Only



TOWN OF WELDON

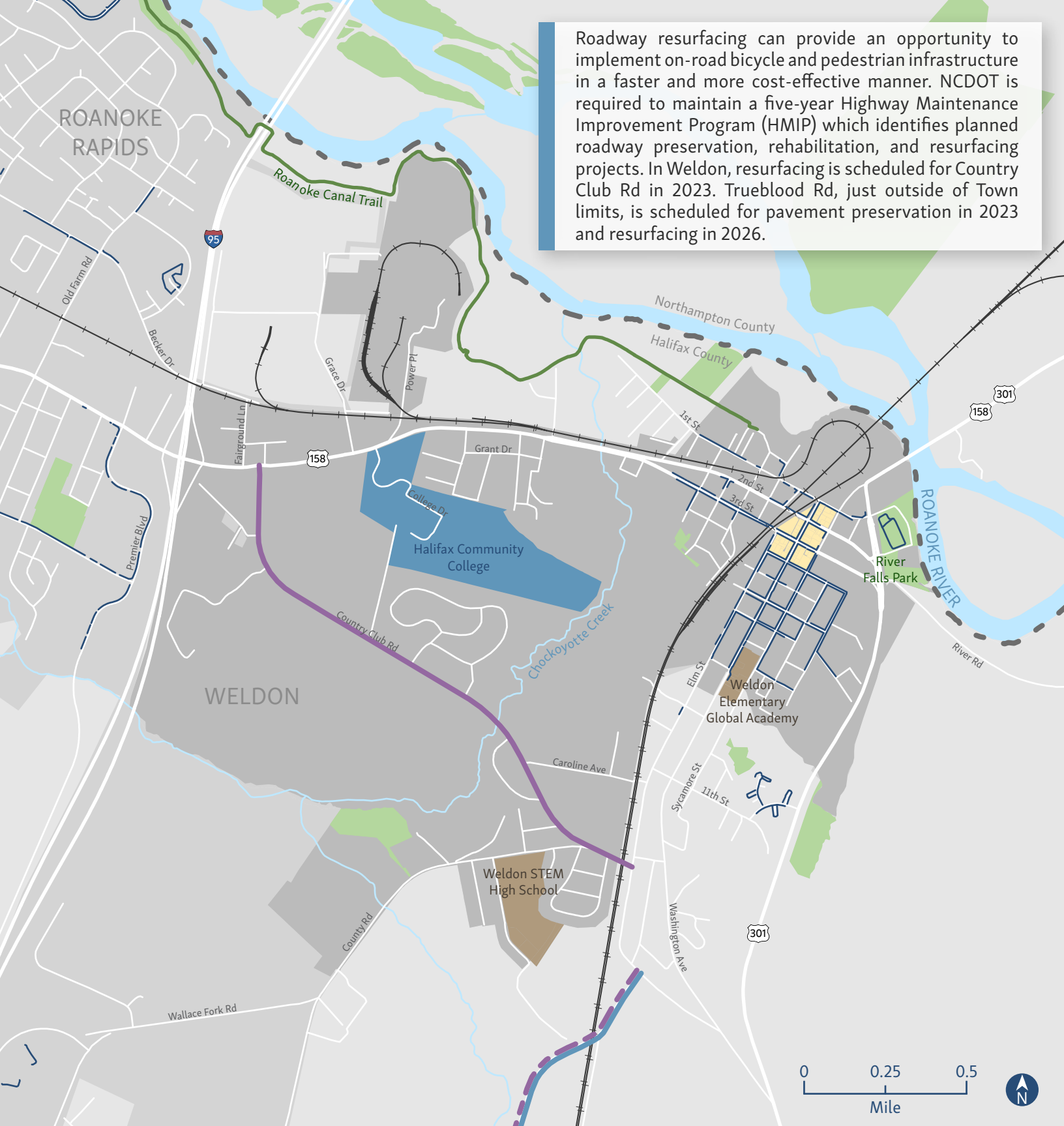
PROJECT ACCELERATION PLAN

NCDOT STIP Projects (2024-2033)

LEGEND ▶

- Statewide Highway
- Statewide Rail
- Division Highway
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
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- County Boundary

Roadway resurfacing can provide an opportunity to implement on-road bicycle and pedestrian infrastructure in a faster and more cost-effective manner. NCDOT is required to maintain a five-year Highway Maintenance Improvement Program (HMIP) which identifies planned roadway preservation, rehabilitation, and resurfacing projects. In Weldon, resurfacing is scheduled for Country Club Rd in 2023. Trueblood Rd, just outside of Town limits, is scheduled for pavement preservation in 2023 and resurfacing in 2026.

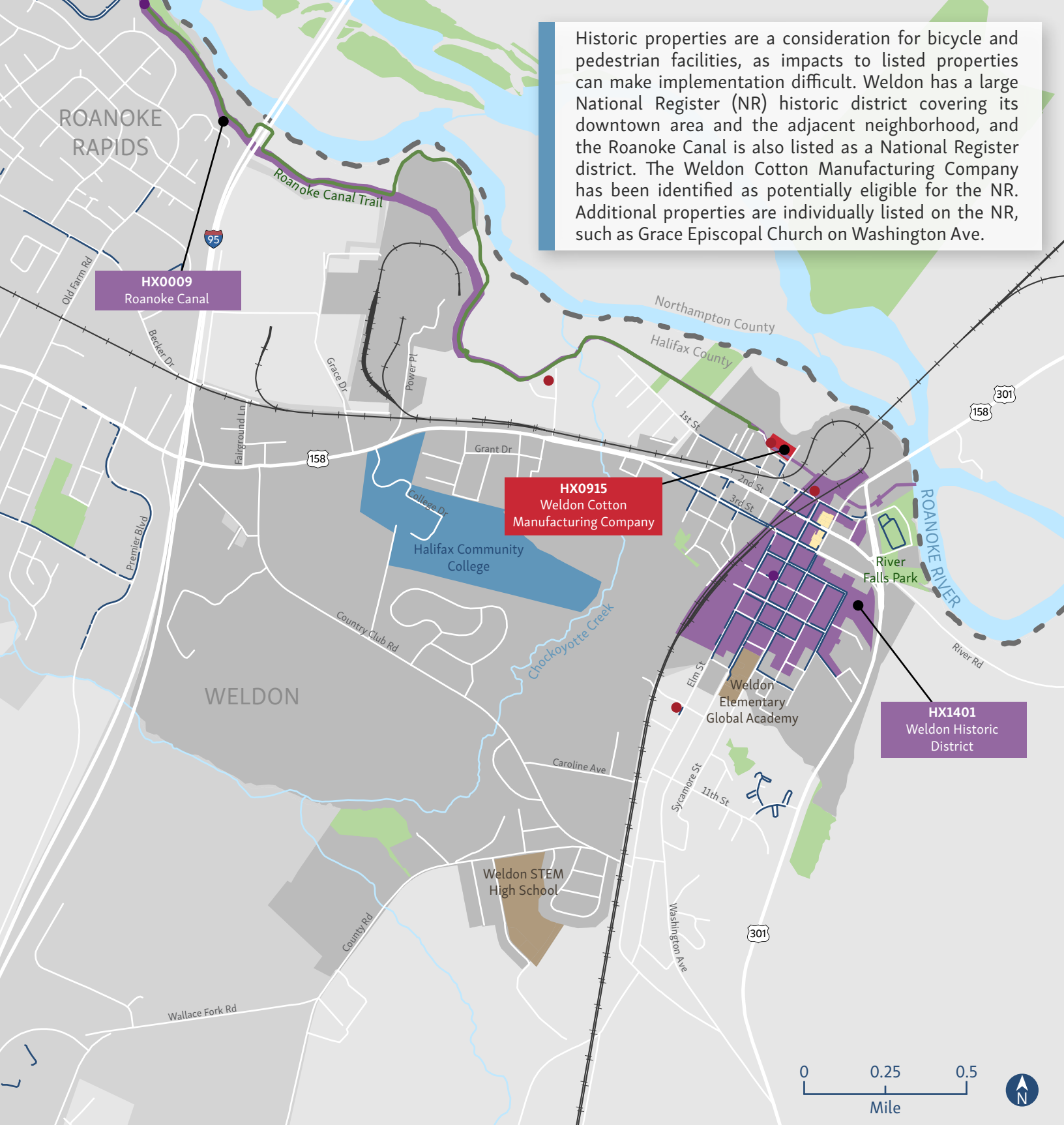


TOWN OF WELDON PROJECT ACCELERATION PLAN NCDOT HMIP Projects (2023-2027)

- 2023 Resurfacing
- - - 2023 Preservation
- 2026 Resurfacing
- Existing Shared Use Path
- Existing Sidewalk
- + + Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶

Historic properties are a consideration for bicycle and pedestrian facilities, as impacts to listed properties can make implementation difficult. Weldon has a large National Register (NR) historic district covering its downtown area and the adjacent neighborhood, and the Roanoke Canal is also listed as a National Register district. The Weldon Cotton Manufacturing Company has been identified as potentially eligible for the NR. Additional properties are individually listed on the NR, such as Grace Episcopal Church on Washington Ave.

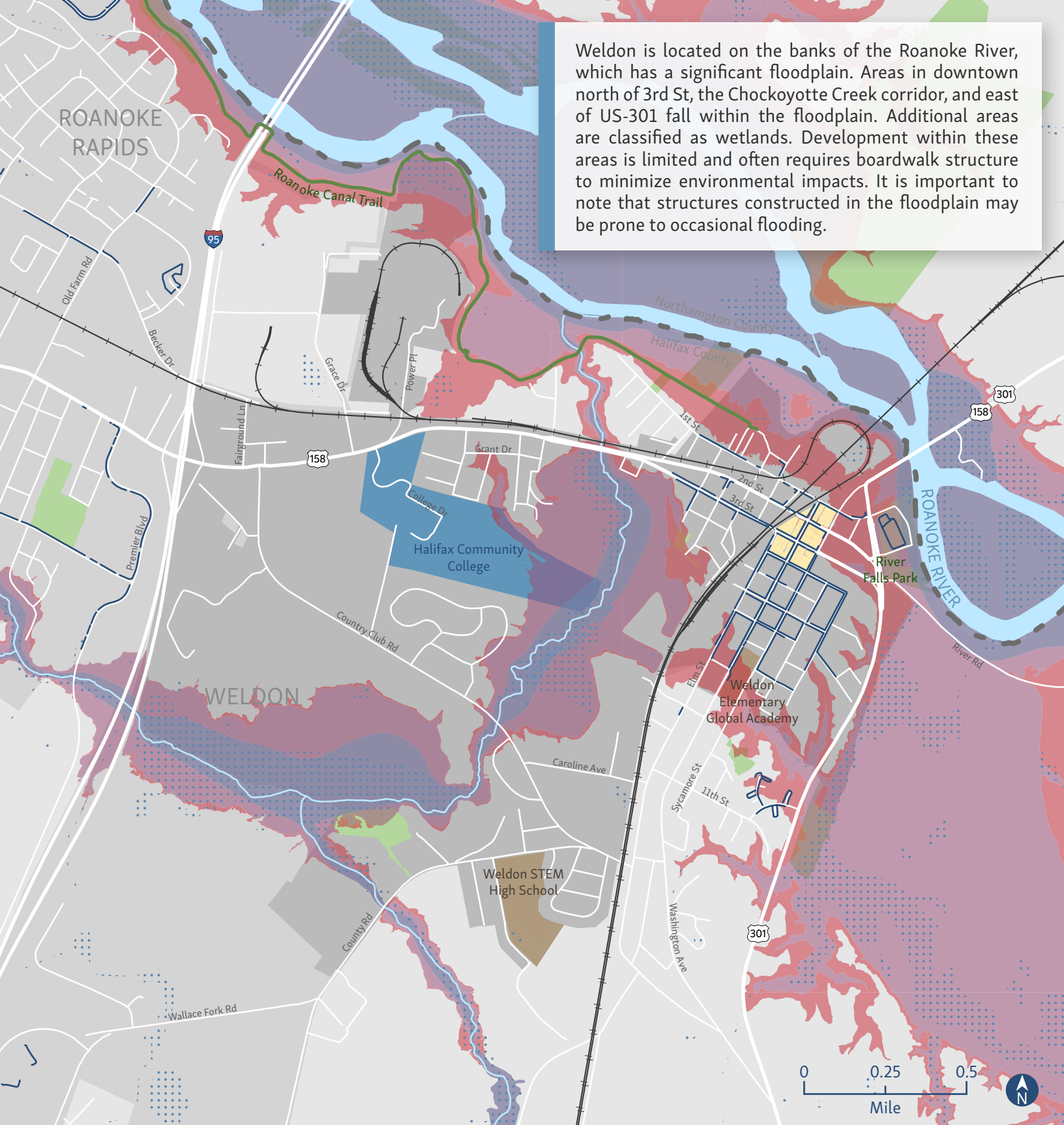


TOWN OF WELDON PROJECT ACCELERATION PLAN Historic Resources

- National Register (NR) District
- NR Individual Listing
- Study List (SL) District
- SL Individual Entry
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
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- County Boundary

LEGEND ▶

Weldon is located on the banks of the Roanoke River, which has a significant floodplain. Areas in downtown north of 3rd St, the Chockoyotte Creek corridor, and east of US-301 fall within the floodplain. Additional areas are classified as wetlands. Development within these areas is limited and often requires boardwalk structure to minimize environmental impacts. It is important to note that structures constructed in the floodplain may be prone to occasional flooding.

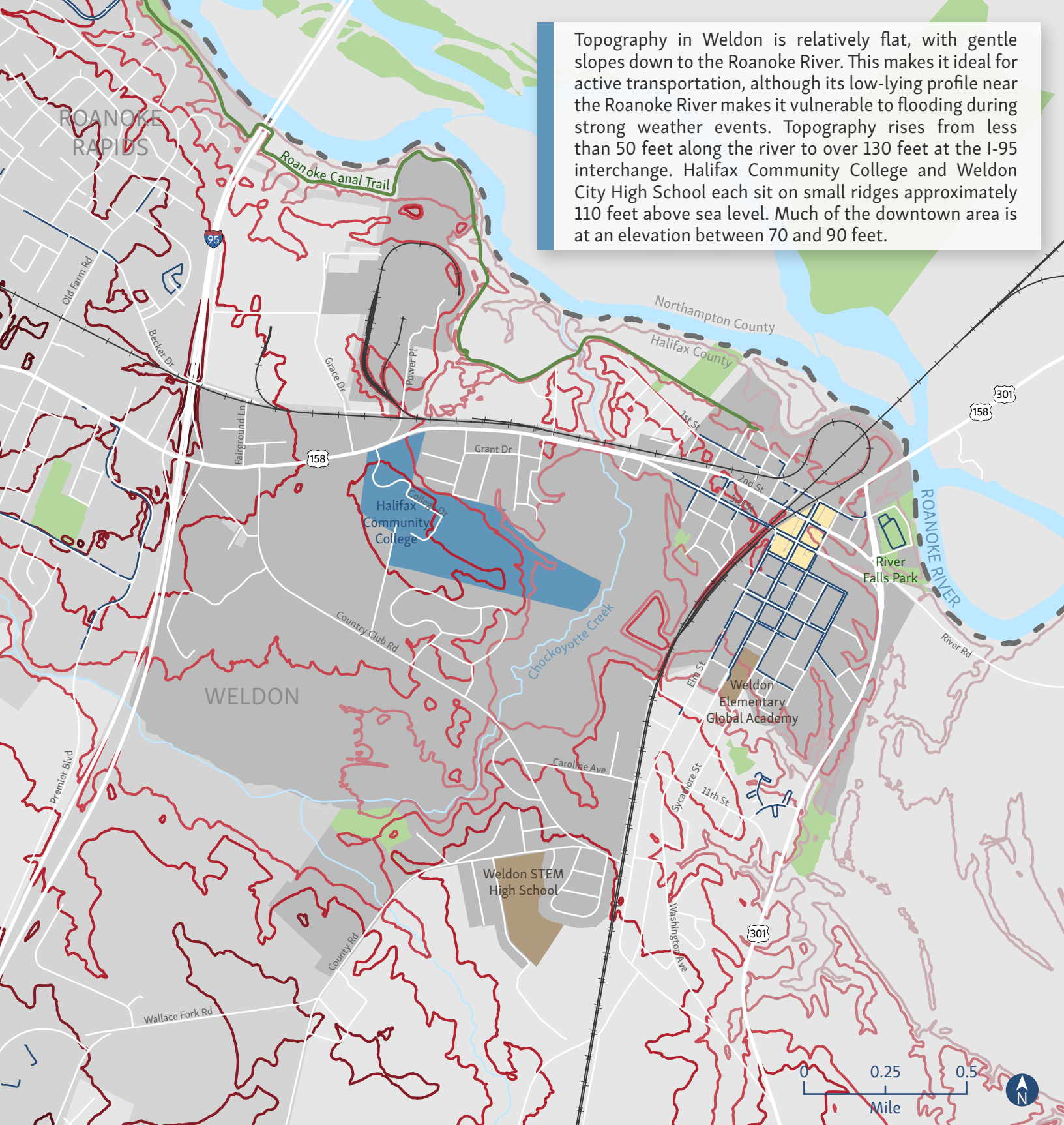


TOWN OF WELDON PROJECT ACCELERATION PLAN Wetlands + Flooding

LEGEND ▶

- Wetlands
- Floodway
- Base Flood Elevation (BFE)
- 0.2% Annual Chance Flood Hazard
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

Topography in Weldon is relatively flat, with gentle slopes down to the Roanoke River. This makes it ideal for active transportation, although its low-lying profile near the Roanoke River makes it vulnerable to flooding during strong weather events. Topography rises from less than 50 feet along the river to over 130 feet at the I-95 interchange. Halifax Community College and Weldon City High School each sit on small ridges approximately 110 feet above sea level. Much of the downtown area is at an elevation between 70 and 90 feet.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Halifax County Topography

- | | | | |
|--|--------------------------|--|---------------------|
| | 50 feet | | Railroad |
| | 70 feet | | Roadway |
| | 90 feet | | Downtown Weldon |
| | 110 feet | | School |
| | 130 feet | | College |
| | 150 feet | | Park/Conserved Land |
| | Existing Shared Use Path | | Water |
| | Existing Sidewalk | | Weldon Town Limits |
| | | | Roanoke Rapids |
| | | | County Boundary |

LEGEND ▶

PREVIOUS PLANNING EFFORTS

The Town of Weldon and Halifax County have prioritized multi-modal connectivity in planning efforts over the years. The following table provides a summary of key bicycle and pedestrian recommendations from previous plans and studies that are relevant to the *Town of Weldon’s Project Acceleration Plan* for multimodal facilities.

EXISTING PLAN/STUDY	KEY RECOMMENDATIONS RELATED TO WELDON
Town of Weldon Downtown Façade Improvement Program (2022)	The Town of Weldon’s Façade Improvement Program aims to assist both property and business owners located within the downtown area to improve storefront or building front improvements. The goal of the program is to encourage well-designed improvements, restore significant architectural elements of the structure, and coordinate the important features of a commercial or office storefront into a more visually attractive package. Landscaping including sidewalks and plantings are eligible for funding through this program. In addition, several of the other eligible improvements help contribute to a pedestrian-friendly environment and encourage walking in the downtown area. An enhanced downtown area coupled with increased foot traffic will promote further private investment in the Town, as well as promote economic development.

Roanoke Rapids Urban Area Comprehensive Transportation Plan (2008)

The Roanoke Rapids Urban Area Comprehensive Transportation Plan (CTP) is intended as a long-range planning document that will assist local governments and its representatives in making transportation decisions in the next 25 to 30 years. The plan is a joint effort between Roanoke Rapids, Peanut Belt Rural Planning Organization (RPO), and NCDOT. The following multimodal projects will be re-evaluated during the *Project Acceleration Plan*:

Bicycle Facilities:

- Existing on-road bicycle facilities need improvements on W 2nd St, US 301, and US 158

Pedestrian Facilities:

- All existing sidewalks within the Town require improvements
- Sidewalks are recommended on the following roads:
 - » Julian Allsbrook Hwy
 - » WC Rivers Dr
 - » Poplar St
 - » Turner St
 - » Mulberry St
 - » S Walnut St
 - » E 3rd St
 - » E 5th St
 - » Meadow Ln
 - » Sunset Dr
 - » Woodlawn Ave
 - » E 8th St
 - » Maple St
 - » Willow Dr
 - » Washington Ave
 - » W 11th St
 - » E 11th St
 - » Carolina Ave
 - » Country Club Rd
 - » Country Rd

EXISTING PLAN/STUDY**KEY RECOMMENDATIONS RELATED TO WELDON****Town of Weldon Land Development Plan (2006)**

The Town of Weldon's vision statement for its Land Use Plan focuses on the following:

- Capitalizing and promoting its rich history, especially the railroad heritage
- Promoting and celebrating the Town's connection to the Roanoke River and Roanoke Canal Trail
- Taking advantage of transportation opportunities (rail and highway)
- Becoming a travel destination (for outdoor sports, history)
- Creating a vibrant downtown
- Re-development of commercial areas
- Serving as a site for museums
- Promoting clean attractive neighborhoods
- Continuing to grow and develop

Goals and strategies documented in the plan are outlined below and include key recommendations related to the *Project Acceleration Plan*.

Recreation and Cultural Resources: The Town would like to continue developing the Riverfront Enhancement Project by installing the following additional facilities:

- Lighting and picnic facilities
- Expanding public use of the river
- Improving access to River Falls Park athletic fields
- Developing greenway trails that will link to the Roanoke Canal Trail

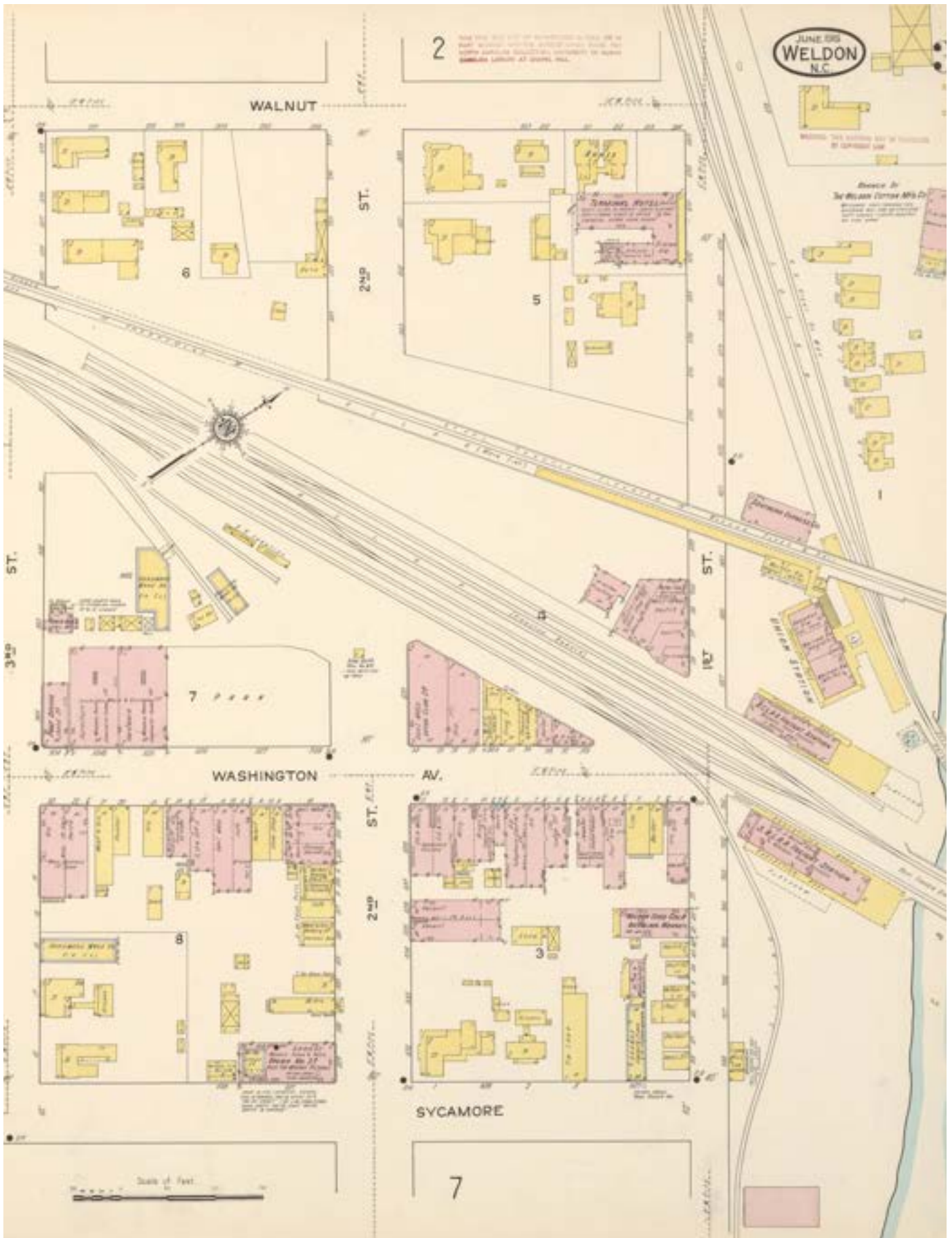
Commercial Corridors:

To recruit and retain attractive commercial development, the Town plans to improve the following within commercial corridors:

- Improve pedestrian safety along US 158 by constructing sidewalks
- Adopt a landscaping ordinance to improve the appearance of commercial properties along the business corridors

Transportation:

To develop a balanced transportation system that meets the need of the Town's citizens, Weldon plans to consider a trolley bus service to accommodate travel between tourist areas and Downtown Weldon.



Downtown Weldon, as depicted on a 1915 fire insurance map created by The Sanborn Map Company

POLICY REVIEW

The following tables provides a summary of key federal, state, and local policies from FHWA, NCDOT, and the Town of Weldon that may guide or impact the development of Weldon’s bicycle and pedestrian network.

EXISTING POLICY	KEY POLICIES TO GUIDE/IMPACT TOWN OF WELDON
<p>FHWA Guidance on Bicycle and Pedestrian Accommodation (2011)</p>	<p>Under the US Department of Transportation Policy Statement on Bicycle and Pedestrian Accommodation, “The DOT policy is to incorporate safe and convenient walking and bicycling facilities into transportation projects. Every transportation agency, including DOT, has the responsibility to improve conditions and opportunities for walking and bicycling and to integrate walking and bicycling into their transportation systems...transportation agencies are encouraged to go beyond minimum standards to provide safe and convenient facilities for these modes.” There are no Federal laws or regulations that prohibit shared use paths or bicycle use along or near Interstate highways or other freeways. Bicycle and pedestrian accommodations may be allowed on Interstate and other major highways and freeways. Bridges are essential in any transportation network, and many Interstate or other freeway bridges often are the only possible bridges across rivers, canyons, railroads, other highways, or other major barriers. Major highway bridges often are necessary links for nonmotorized transportation networks.</p> <p>Under 23 U.S.C. 217(g), transportation plans must consider bicycle and pedestrian accommodations.</p> <p>23 U.S.C. 217(g) Planning and Design. --</p> <ol style="list-style-type: none"> 1. In General. --Bicyclists and pedestrians shall be given due consideration in the comprehensive transportation plans developed by each metropolitan planning organization and State in accordance with sections 134 and 135, respectively. Bicycle transportation facilities and pedestrian walkways shall be considered, where appropriate, in conjunction with all new construction and reconstruction of transportation facilities, except where bicycle and pedestrian use are not permitted. 2. Safety considerations. --Transportation plans and projects shall provide due consideration for safety and contiguous routes for bicyclists and pedestrians. Safety considerations shall include the installation, where appropriate, and maintenance of audible traffic signals and audible signs at street crossings.
<p>NCDOT Bridge Policy (2000)</p>	<p>This policy establishes design elements of new and reconstructed bridges on the NC Highway System. Vertical clearances for new structures shall be designed above all sections of pavement including the useable shoulder. Future widening and pavement cross slope will be considered in design clearance. Vertical clearances for facilities are as follows: over interstates, freeways, and arterials: 16’-6” to 17’-0” ; over local and collector roads and streets: 15’-0” to 15’-6” ; over all railroads: 23’-0” to 23’-6” or less if approved by railroads; pedestrian overpasses and sign structures vertical clearance: 17’-0” to 17’-6”. Sidewalks shall be included on new bridges with curb and gutter approach roadways that are without control of access. A minimum handrail height of 42” is required.</p>

EXISTING POLICY**KEY POLICIES TO GUIDE/IMPACT TOWN OF WELDON****NCDOT Roadway Design Manual (2018)**

The Roadway Design manual provides general design information, design criteria, and plan preparation guidance for NCDOT roadways. Guidance on clear zones can be referenced in Part 1, Chapter 1-4N. Guidance states that the recommended clear zone range for flat, level terrain adjacent to a straight section of a 60-mph highway with an average daily traffic of 6,000 vehicles is a width of 30 to 32 feet. For steeper slopes on a 70-mph roadway, the clear zone range increases to 38 to 46 feet. Additional clear zone guidance is provided for roadway facilities based on design speed, design ADT, and roadside slope.

Guidance on multimodal design elements can be referenced in Part 1, Chapter 4 Sections 4.14, 4.15, and 4.16. Guidance states that shared-use paths, often referred to as greenways, are paths physically separated from motor vehicle traffic and used by pedestrians, bicyclists, and other non-motorized users. The width of a shared-use path may vary, based on expected user volumes and context.

- Desirable width – 12 to 14 feet
- Minimum width – 10 feet; 8 feet in exceptionally constrained areas
- Vertical clearance, minimum – 8 feet

NCDOT Complete Streets Policy (2019)

The NCDOT Complete Streets Policy Update was adopted by the Board of Transportation in August 2019. This policy requires NCDOT to consider and incorporate multimodal facilities in the design and improvement of all transportation projects in North Carolina. The adopted Comprehensive Transportation Plan (CTP) is considered the controlling plan for the identification of nonmotorized facilities to be evaluated as part of a roadway project. The CTP may include and/or reference locally adopted plans for public transportation, bicycle and pedestrian facilities, and greenways. Bicycle, pedestrian, and public transportation facilities that appear in the CTP directly or by reference will be included as part of the proposed roadway project, and NCDOT is responsible for the full cost of the project. Bicycle, pedestrian, and transit facilities incidental to a roadway project where a need has been identified through the project scoping process but not identified in an adopted plan may be included in the project. Inclusion of these incidental facilities requires the local jurisdiction to share the incremental cost of constructing the improvements based on population thresholds. The policy also establishes maintenance responsibility for active transportation facilities. Bicycle, pedestrian, and transit improvements inside a municipal boundary are subject to local maintenance. For bicycle, pedestrian, and transit improvements outside of a municipal boundary where a county maintenance agreement has not been executed to maintain the facility, NCDOT will maintain the facility after construction if the bicycle or pedestrian facility lies within NCDOT ROW. Projects that have not completed environmental review prior to August 2019 are subject to the Complete Streets Policy.

Town of Weldon Zoning Ordinance (2008)

The Town of Weldon's Zoning Ordinance details several instances that support the development of bicycle and pedestrian facilities within the Town. These findings are outlined below.

Site plan submissions must include sidewalks, trails, and pedestrian paths.

306.6 Planned Unit Developments - Required Common Area and Open Space: A minimum of 20 percent of the gross acreage shall be reserved for open space. A minimum of 10 percent of the required reserved open space in a planned unit development shall be developed for active recreational purposes. This area shall have free and easy access via streets, walkways, dedicated easements, or rights-of-way. The common areas and open space required by this section shall be deeded to an owner's association.

Design Standards for Off-Street Parking: In the event any parking stall abuts upon a walkway there shall be a space of 3.5 feet between the wheel bumper or curb and the edge of the walkway.

EXISTING POLICY**KEY POLICIES TO GUIDE/IMPACT TOWN OF WELDON****Town of Weldon
Subdivision
Regulations (2009)**

One of the goals outlined in the regulatory document is to coordinate the use of land and buildings and the circulation of traffic, with particular regard for the avoidance of congestion in the streets and highways, and the creation of pedestrian facilities appropriate for the various uses of the land and buildings. Pedestrian facility design standards within the Town are outlined below.

407.3 Sidewalks: Sidewalks shall be required as follows for all major subdivisions:

- R40 District: Not Required
- R-20 District: Required on one side of street
- R-10 District: Required on both sides of the street
- R-8, R-MF, R-MH Districts: Required on both sides of the street
- Non-residential Subdivisions: Required on both sides of the street

Sidewalks shall be a minimum of 5 feet in width. A 4-foot vegetative strip with grass plantings shall separate the sidewalk from the curb. The abutting property owner or a homeowner’s association shall be responsible for mowing the vegetative areas.

407.4 Crosswalks: Pedestrian ways or crosswalks, not less than 15 feet in width, shall be provided, where deemed essential in the opinion of the Planning Board to provide adequate pedestrian circulation or access to schools, shopping areas, churches, parks, playgrounds or other similar facilities.

407.5 Street Trees: Street trees shall be installed for all residential subdivisions of 50 units or more with an established homeowner’s association. Trees shall be planted in a 7-foot tree lawn area located between the street and the sidewalk. It shall be the responsibility of the subdivider to install street trees and the homeowner’s association shall assume maintenance responsibilities. In such cases, street trees shall be planted at intervals of 40 feet. All street trees shall be on the approved list of street trees identified by the Town of Weldon Zoning Ordinance.

407.6 Street Lighting: All public streets, sidewalks, or other common areas or facilities in subdivisions shall be sufficiently illuminated to ensure the public safety and security of property. Street lighting shall be installed in new subdivisions pursuant to a street lighting plan which shall be submitted to the Planning Board for approval. All streetlights shall be compatible with any applicable standards of the electric utility provider.

**Town of
Weldon Code of
Ordinances (2021)**

The Town of Weldon’s Code of Ordinances includes policies that affect bicycling and walking and the development of multimodal facilities in the community.

Bicycles may not be operated on a sidewalk in any commercial zone in Weldon.

The Townwide speed limit is 35 mph, except on non-state-maintained streets within residential districts where speeds are limited to 25 mph. In the business district, the speed limit is 20 mph.



This Plan champions policies aimed at reshaping Weldon into a community that is friendlier to both cyclists and pedestrians

OPPORTUNITIES + CONSTRAINTS

The following table provides a summary of the opportunities and constraints of existing facilities that informed the development of Weldon’s recommended bicycle and pedestrian network. Numbered sections in this table correspond with specific opportunities and constraints identified on the map following the table.

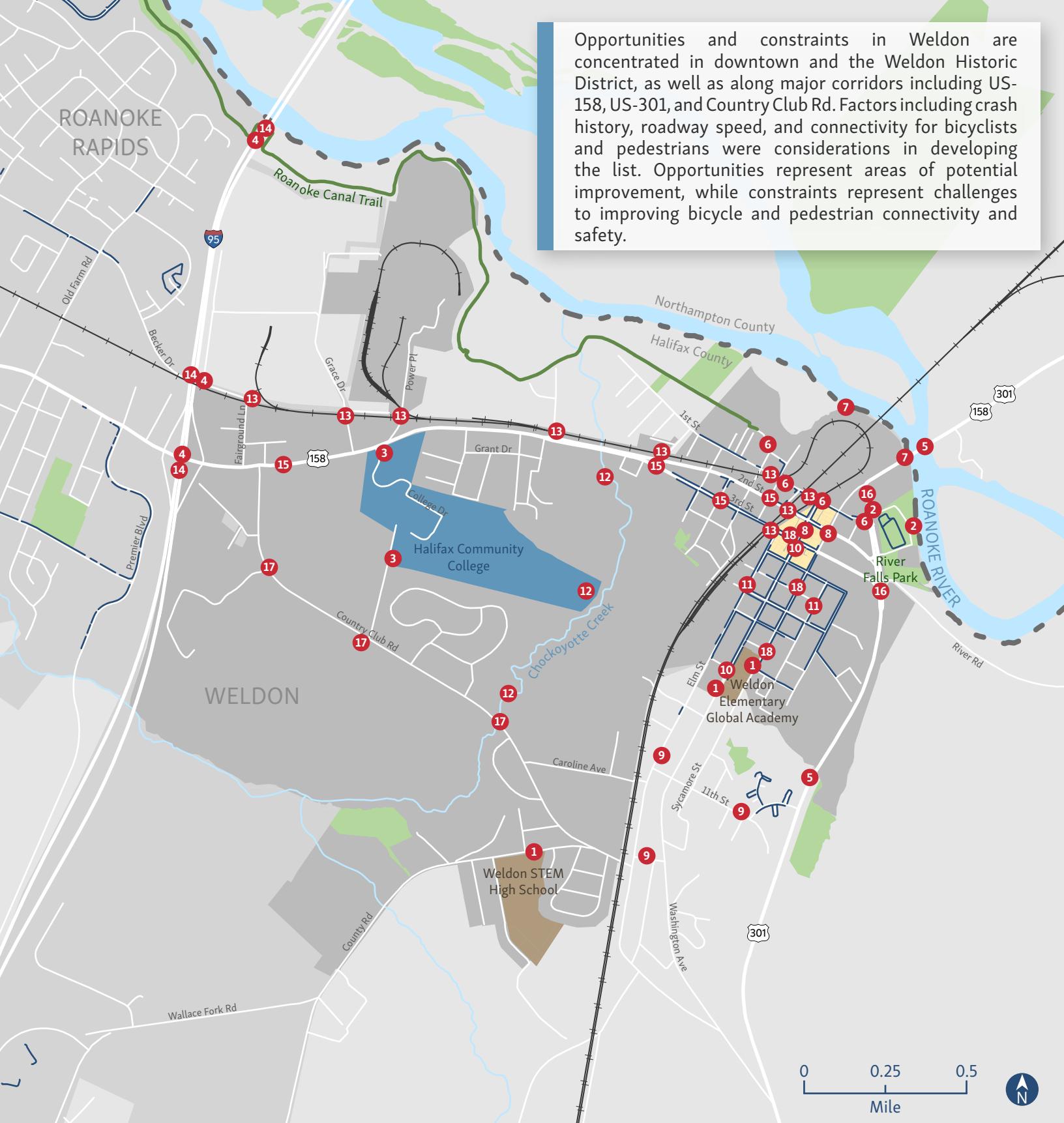
OPPORTUNITY/CONSTRAINT	DESCRIPTION
1. Connections to Schools	<p>While Weldon’s two public schools have some pedestrian connectivity via sidewalks, there is an overall lack of bicycle and pedestrian facilities that provide safe multi-modal connections to schools for all ages and abilities. Weldon Elementary Global Academy, which has a history of high pedestrian crash rates along neighboring streets, is connected to the pedestrian network to the north with sidewalks and crosswalks on Washington Ave and Sycamore St but lacks connections to neighborhoods in South Weldon. Additionally, some crosswalks have faded markings, and most sidewalks need maintenance or Americans with Disabilities Act (ADA) accessibility improvements. Weldon City High School has no multimodal connections, and County Rd has a posted speed of 45 mph, which contributes to unsafe conditions for bicyclists and pedestrians commuting to and from school.</p>
2. Connections to River Falls Park	<p>River Falls Park is among the major destinations in Weldon and its only park. It features a walking loop and access to the Roanoke River, where a riverside walkway has been proposed. The park is also the eastern terminus of the Roanoke Canal Trail. Bicycle and pedestrian access to the park is unsafe due to lack of multimodal facilities along and crossing US-158/US-301. There are no crosswalks between the park and downtown Weldon, but there is a narrow pedestrian underpass just north of E 1st St. This tunnel needs maintenance and does not meet ADA accessibility requirements.</p> <p>A proposed redevelopment concept for River Falls Park includes a paved sidepath along 1st St connecting the Roanoke Canal Trail to amenities within the park, including a proposed riverbank trail and connections to proposed trails along the Weldon Distillery property. A decorative crosswalk is also proposed along 1st St to provide trail users with a visible and safe crossing of US-158/US-301.</p>
3. Connections to Halifax Community College	<p>Halifax Community College is one of the largest employers in the region and enrolled over 3,700 students between 2021 and 2022. A fitness trail with workout stations is located near the campus entrance along US-158, which is limited to pedestrians. The campus lacks bicycle and pedestrian connections to adjacent neighborhoods and the commercial corridor along US-158, requiring most students, faculty, and staff to drive to campus.</p>
4. Connections to Roanoke Rapids	<p>Many employees working in Weldon call Roanoke Rapids home, and most Weldon residents commute to employment centers in Roanoke Rapids and the surrounding region, making this a critical connection. I-95, the boundary between the two communities, also serves as a major barrier to bicycle and pedestrian connectivity. There are two existing roadway crossings at US-158 and Becker Dr, neither of which have pedestrian or bicycle accommodations. However, there is existing right-of-way adjacent to US-158 beneath I-95 that could accommodate bicycle and pedestrian facilities. The Roanoke Canal Trail also uses the underpass beneath I-95 as it crosses the Roanoke River but is unpaved and does not have any nearby access points.</p>

OPPORTUNITY/CONSTRAINT	DESCRIPTION
5. Regional Connections / Great Trails State Corridor	<p>Weldon is part of a larger planned network of bicycle and pedestrian trails throughout the Roanoke Valley region. The NCDOT Great Trails State Plan proposed sidepaths along the US-301 corridor connecting to the Roanoke Canal Trail via 1st St. The proposed facilities would link Weldon to Garysburg and Northampton County to the north and to Weldon Middle School and Halifax to the south.</p>
6. Roanoke Canal Trail	<p>The Roanoke Canal Trail provides a walking and bicycling connection between Weldon and Roanoke Rapids, parallel to the Roanoke River. The trail is unpaved along most of the corridor and has limited access points. The trail segment between Riverside Mill and River FrontFalls Park is in poor condition, with sidewalks in need of repair, missing crosswalks, and major gaps along Water St and 1st St. This section includes two at-grade railroad crossings and an unsafe crossing of US-158/US-301 which will both require extensive coordination to improve trail accessibility.</p> <p>A proposed streetscape concept for 1st St would improve the existing sidewalk, which is the current route for the Roanoke Canal Trail across Downtown. High-visibility, decorative crosswalks and wider sidewalks are proposed to improve crossing safety. There is also an opportunity to coordinate with the Roanoke Canal Museum to improve wayfinding signage as the current signs are small and difficult to read from existing facilities.</p>
7. Roanoke River	<p>The Roanoke River is a travel barrier due to the limited number of crossings. US-158/US-301 is the only crossing in Weldon and is currently unsafe for pedestrians and bicyclists. The existing bridge carries two lanes of traffic at a posted speed limit of 45 mph, with approximately 8,800 vehicles crossing per day on average. While traffic levels are relatively low, the vehicle speeds make this an unsafe crossing for pedestrians and bicyclists in the absence of dedicated facilities. There is a wide shoulder on either side which may be reconsidered for multimodal access. River access is also an amenity that residents and visitors in Weldon have expressed a desire to improve. Extending the Roanoke Canal Trail to the river from 1st St to River Falls Park provides an opportunity to improve Roanoke River access.</p>
8. Downtown Weldon	<p>Downtown Weldon includes the most comprehensive sidewalk network within the Town as most streets have existing sidewalks and crosswalks at intersections. There is an opportunity to build on this network by filling gaps on Sycamore St and W 2nd St, in particular. Existing maintenance conditions of sidewalks and ADA accessibility issues on Washington Ave between 2nd St and 3rd St pose challenges to overall bicycle and pedestrian connectivity and need improvements. Downtown is also home to a variety of destinations that serve residents and visitors of Weldon, including a grocer, library, and restaurant. Providing safe walking and biking access to these destinations is important to the community's overall quality of life.</p>

OPPORTUNITY/CONSTRAINT	DESCRIPTION
9. Connections to South Weldon	Improving bicycle and pedestrian connectivity between Weldon and South Weldon is critical as both communities are served by the same key destinations and essential services. Existing facilities along Elm St, Washington Ave, and Sycamore St lack safe multimodal connections, especially to the elementary school leaving residents in South Weldon to walk and bike to and from school along the roadway. A bicycle crash has been reported at 11th St and Elm St, and many pedestrian crashes and two fatalities have been reported at locations near the elementary school and near the 12th St/Elm St intersection. Neighborhoods south of the elementary school and east of the railroad corridor are not incorporated in Weldon’s municipal limits. Weldon should coordinate with Halifax County and NCDOT on strategies to improve the overall connectivity in these areas.
10. Washington Ave	Washington Ave is the primary corridor through Downtown Weldon and the neighborhoods south of Downtown. Sidewalks are present along Washington Ave between Weldon Elementary and 1st St, but maintenance and ADA accessibility challenges remain where tree roots have caused the sidewalks to buckle and crack, creating uneven walkways. Sidewalks are also narrow and in need of weeding and branch removal maintenance in some areas. As one of the key transportation corridors through Weldon, Washington Ave would also benefit from bicycle facilities for those commuting by bike.
11. Weldon Historic District	The Weldon Historic District includes many beautiful homes, churches, and businesses, as well as the majority of Weldon’s pedestrian network. However, sidewalk gaps remain on Elm St, 5th St, Maple St, and along adjacent streets in the Historic District. Streets in this area are largely low-speed / low trafficked corridors, making this neighborhood ideal location for on-road bicycle accommodations.
12. Chockoyotte Creek	Chockoyotte Creek runs through Weldon before reaching the Roanoke River and presents an opportunity for a greenway connection between Country Club Rd to US-158. An existing utility easement runs parallel to the creek, and residential neighborhoods border the corridor in multiple locations. The campus of Halifax Community College extends nearly to the creek on the west side. However, much of the corridor is within a floodplain, and wetlands on either end of the corridor would make trail development challenging.
13. Railroad Corridors	Weldon has a long and storied history as a railroad hub, and the legacy remains with two active rail corridors owned by CSX operating through Town. The railroads pose a challenge to bicycle and pedestrian connectivity within Weldon as crossings are limited. In Downtown Weldon multiple at-grade crossings make an unpleasant user experience for bicyclists and pedestrians, and underpasses beneath the mainline viaduct have missing or poorly maintained sidewalks. Several crossings, including Fairground Ln, 1st St, and County Rd, have poor visibility and contributed to multiple bicycle and pedestrian crashes in these areas.

OPPORTUNITY/CONSTRAINT	DESCRIPTION
14. I-95 Corridor	I-95 forms the boundary between Weldon and Roanoke Rapids and is a major barrier to bicycle and pedestrian connectivity between the two communities. Existing roadway crossings at US-158 and Becker Dr lack pedestrian or bicycle accommodations. However, existing ROW along US-158 beneath I-95 may provide an opportunity for bicycle and pedestrian facilities. The Roanoke Canal Trail also uses an underpass beneath I-95 as it crosses the Roanoke River but is unpaved and does not have any nearby access points.
15. US-158 Corridor	US-158, the major transportation corridor between Weldon and Roanoke Rapids, is a popular 4-lane highway, characterized by high speed limits. Existing roadway conditions and the absence of dedicated bicycle and pedestrian facilities contribute to unsafe conditions for bicyclists and pedestrians traveling to and from Roanoke Rapids, Halifax Community College, and other key destinations along US-158. Four pedestrian crashes and one bicycle crash have occurred on the corridor between Downtown Weldon and I-95, with all but one located on the one-way segments of 2nd St and 3rd St.
16. US-301 Corridor	Roadway characteristics of the US-301 corridor within Weldon consist of two wide travel lanes, speed limits ranging between 45-55 mph, and an overbuilt intersection with US-158 with multiple turning lanes. These conditions make US-301 an unsafe corridor for pedestrians and bicyclists. Only one dedicated crossing for pedestrians exists, a tunnel at E 1st St connecting Downtown Weldon to River Falls Park. This tunnel is narrow, in need of maintenance, and is not ADA-compliant.
17. Country Club Rd Corridor	Another major corridor within Weldon is Country Club Rd, which provides a critical link between US-158 and South Weldon. Traffic volumes are below 5,000 vehicles per day, but a posted speed limit of 45 mph puts pedestrians and bicyclists at risk. A pedestrian fatality at Country Club Rd and Hampton Farm Rd was recorded in 2012. This corridor provides access to Halifax Community College as well as the Chockoyotte Country Club and several residential communities, but roadway improvements along the bridge crossing the Chockoyotte Creek would be required for any bicycle and pedestrian facilities proposed along this corridor.
18. Sidewalk Maintenance / ADA Accessibility Improvements	Sidewalks throughout Weldon, especially Downtown and neighborhoods immediately south, need extensive maintenance improvements such as weed removal and spot reconstruction. Additionally, ADA accessibility is a concern for pedestrians at intersections without curb ramps and along the Downtown segment of Washington Ave where the grade of the sidewalk sits below the adjacent roadway. As of Spring 2023, Town staff have been coordinating with NCDOT to fund intersection improvements along Town-owned streets. Curb ramp improvements are proposed along Elm St at 4th St, 5th St, 6th St, and 7th St; Sycamore St at 4th St, 5th St, 6th St, 7th St, and 9th St; Maple St at 4th St, 6th St, 7th St, and 9th St; Cedar St at Meadow Ln, 6th St, Woodlawn Ave, 8th St, and 9th St; Sunset Dr at Woodlawn Ave; and W. 1st St at N. Mulberry St, Chestnut St, Water St, and Coward St.

Opportunities and constraints in Weldon are concentrated in downtown and the Weldon Historic District, as well as along major corridors including US-158, US-301, and Country Club Rd. Factors including crash history, roadway speed, and connectivity for bicyclists and pedestrians were considerations in developing the list. Opportunities represent areas of potential improvement, while constraints represent challenges to improving bicycle and pedestrian connectivity and safety.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Opportunities + Constraints

LEGEND ▶

- # Opportunity or Constraint
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary



Railroads frequently present obstacles to the mobility of bicyclists and pedestrians in Weldon

03

Community Engagement





PRIORITY
PROJECTS

OVERVIEW

Community engagement plays a vital role in every planning process. The most effective plans are firmly rooted in the realities and visions of the communities that create them. The *Project Acceleration Plan* relies on input from community members, Steering Committee members, supporting agencies, and non-profit organizations to inform Weldon's multimodal network, encompassing various forms of active transportation and recreation opportunities for visitors and residents alike. Several engagement events took place throughout the study process, including:

- 3 Steering Committee Meetings
- 1 Public Meeting
- 1 Community Survey

The following sections are included within Chapter 3:

- Steering Committee Meetings
- Public Meeting
- Community Survey

STEERING COMMITTEE MEETINGS

A Steering Committee, comprised of local officials, regional and state agencies, as well as local residents and business owners, played a crucial role in supporting the development of the *Project Acceleration Plan*. Members on the Committee convened on three occasions during the project’s duration. Steering Committee members actively contributed to the plan by reviewing and providing feedback on pertinent data, community engagement initiatives, network recommendations, and implementation strategies. Additionally, they took an active role in distributing information and communication materials to the public during outreach events and activities. The table below lists individual members and their respective affiliations.

STEERING COMMITTEE MEMBER	AFFILIATION
Lindbergh Harlow	Town of Weldon
Nancy Sandoval	Town of Weldon Board of Commissioners
JW Shearin	Town of Weldon Planning Board
Stephanie Harmon	Peanut Belt RPO
Kayla Taylor	ECU Health
Alice Irby	Weldon in Action
Adrian King	Weldon Resident
Brianna Neville	Weldon Mills Distillery
Gray Keeter	NCDOT Division 4
James Salmons	NCDOT Division 4
Nick Morrison	NCDOT IMD
Chris Rountree	Halifax County
Christina Wells	Halifax County
Dr. Patrena Elliot	Halifax Community College

STEERING COMMITTEE MEETING #1



The first Steering Committee meeting provided members with an overview of the *Project Acceleration Plan* and an analysis of existing conditions within the Town of Weldon. The meeting facilitated discussions regarding definitions of success for the Plan, as well as opportunities and constraints within the study area. Key findings from the meeting are highlighted below.

- Steering Committee members are eager to create a plan that supports safe and accessible multimodal connections within around the Town of Weldon.
- Members want to see more multimodal connectivity between Downtown Weldon and the rest of the community.
- Members consider additional lighting an important amenity to their future multimodal network.
- Members want a multimodal network that will attract economic development within the Downtown/Riverwalk area.
- One member is interested in seeing comprehensive plan that showcases an ideal and walkable community within Weldon (especially between the high school and the community college).
- Steering Committee members are interested in supporting additional multimodal connections between the existing Canal Trail and the Roanoke River.



STEERING COMMITTEE MEETING #2

The second Steering Committee provided members with an overview of results from the community survey, shared proposed plan goals, and discussed opportunities and constraints for the bicycle and pedestrian network. The meeting also presented the draft proposed bicycle and pedestrian network, draft proposed intersection and crossing treatments, draft proposed program and policy recommendations, and draft prioritization criteria and methodology. Key findings from the meeting are highlighted below.

- Many community members would like to walk or bike in Weldon, but they feel it is unsafe with current infrastructure.
- Steering Committee members would like to see an improved connection between the Roanoke Canal Trail and River Falls Park, including access to the Roanoke River.
- Halifax Community College prefers to remove additional signage from their campus to avoid a cluttered appearance.



STEERING COMMITTEE MEETING #3

On November 30, 2023, the project team presented key findings from the public meeting, priority project cut sheets, implementation strategies, and the draft plan to the Steering Committee. Key findings from the final Steering Committee meeting are highlighted below.

- Attendees said they will primarily use the Roanoke Canal Trail for health and exercise purposes, as well as to access the river. The recommended facilities in this plan will help users reach the river and the park through a seamless loop.
- Feasibility studies may be necessary to advance priority projects, such as the Chockoyotte Creek Greenway and the Roanoke Canal Trail, into design and construction. Grant funding could support future feasibility studies for the projects recommended in this plan.
- One attendee noted that there may be a sewer easement located adjacent to the Chockoyotte Creek. This easement may require further investigation during a feasibility study to determine the future alignment for the proposed Chockoyotte Greenway.
- Attendees noted that some residents park along Washington Ave near Weldon Elementary and closer to Downtown Weldon. Parking impacts should be considered when implementing bike lanes proposed along Washington Ave.
- Committee members were in agreement that the plan will be a great resource for the Town in improving multi-modal connections and expanding the existing trail network.

PUBLIC MEETING

A public meeting was held on August 22, 2023, to share the proposed network recommendations, intersection and crossing treatment recommendations, policy and program recommendations, and implementation plan. The purpose of the meeting was to gauge public support and gather feedback on draft recommendations. Approximately 15 people attended the meeting, which was held at Halifax Community College.

Key meeting takeaways include the following:

- Attendees expressed strong support for proposed greenways and trails in Weldon, including a greenway along Chockoyotte Creek and an extension of the Roanoke Canal Trail to River Falls Park along the Roanoke River.
- There is a great need for safe walking and bicycling facilities in Weldon, particularly along US-158.
- Students and employees at Halifax Community College would benefit from safe walking and bicycling facilities connecting to the campus.
- A network of improved walking and bicycling facilities in Weldon could encourage recreational and environmental tourism within the community.
- Existing sidewalks in the downtown area require maintenance and improvements.

Please see **Appendix C: Additional Community Engagement** for a map showing feedback provided by the public pertaining to their priority projects for the Town.



Community members providing feedback at the *Project Acceleration Plan* Public Meeting



Community members reviewing the proposed intersection treatments

COMMUNITY SURVEY

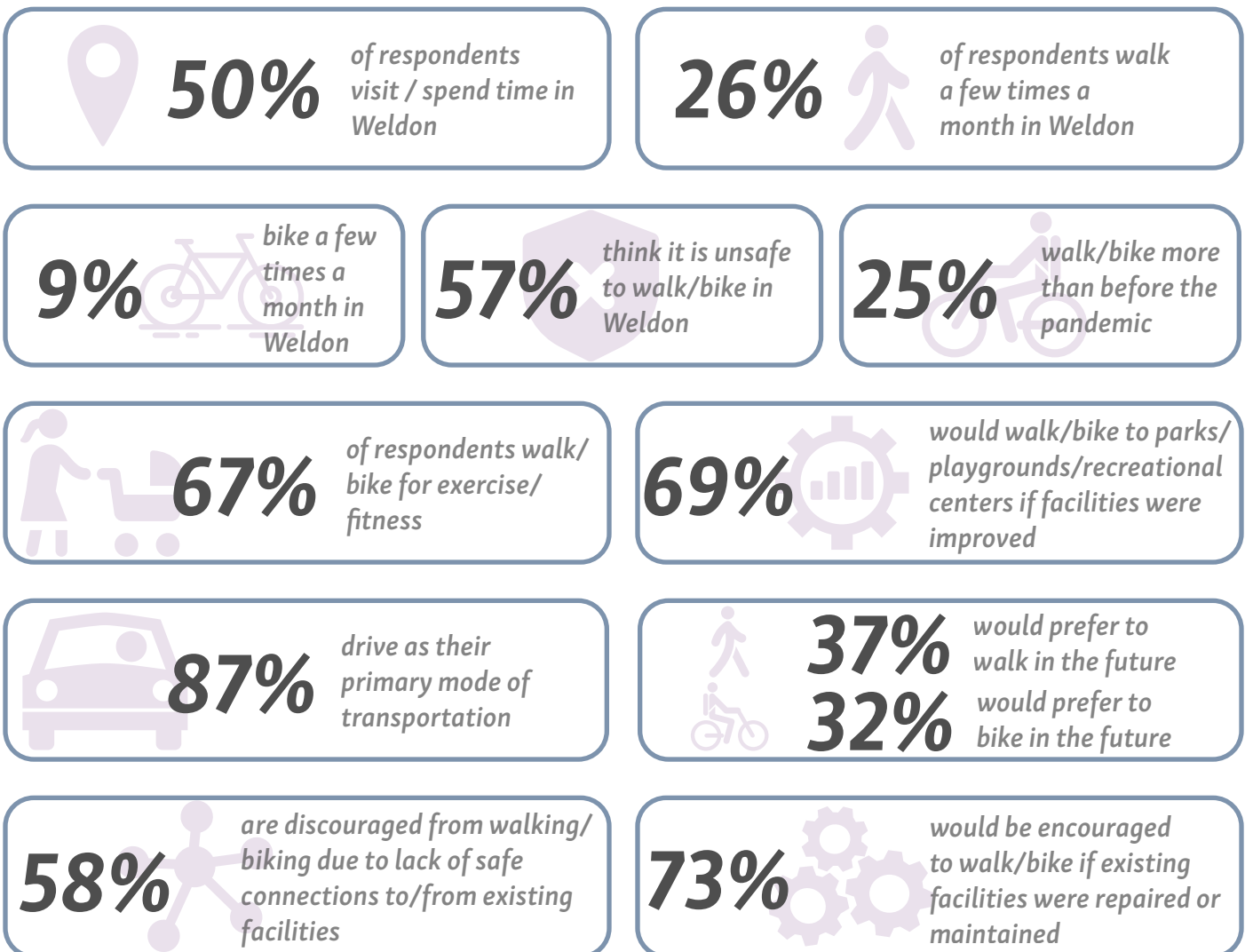
The project team launched a community survey on April 14, 2023, and it was open for public comment through May 19, 2023, via PublicInput.com. The survey was linked on the Town’s website, and members of the Steering Committee also disseminated survey information to the community. The survey attracted 84 participants who provided 186 comments related to the project. The feedback obtained through this survey supported the framework for developing the proposed bicycle and pedestrian network in the Town of Weldon.

The project team distributed the survey to help accomplish the following:

- Introduce the project and gauge public support.
- Solicit and compile public comment on destinations, opportunities and challenges, user preferences, and route preferences.
- Fulfill requests for information.
- Develop an email contact list for interested parties.

The survey contained the following three sections: project-specific questions, interactive mapping questions, and optional demographic questions. Public feedback is summarized on the following pages by theme or specific comment. Overall, the comments collected were generally positive and include several different perspectives on the project.

Key takeaways from the survey include the following:





SURVEY OPEN
APR 14 - MAY 19,
2023

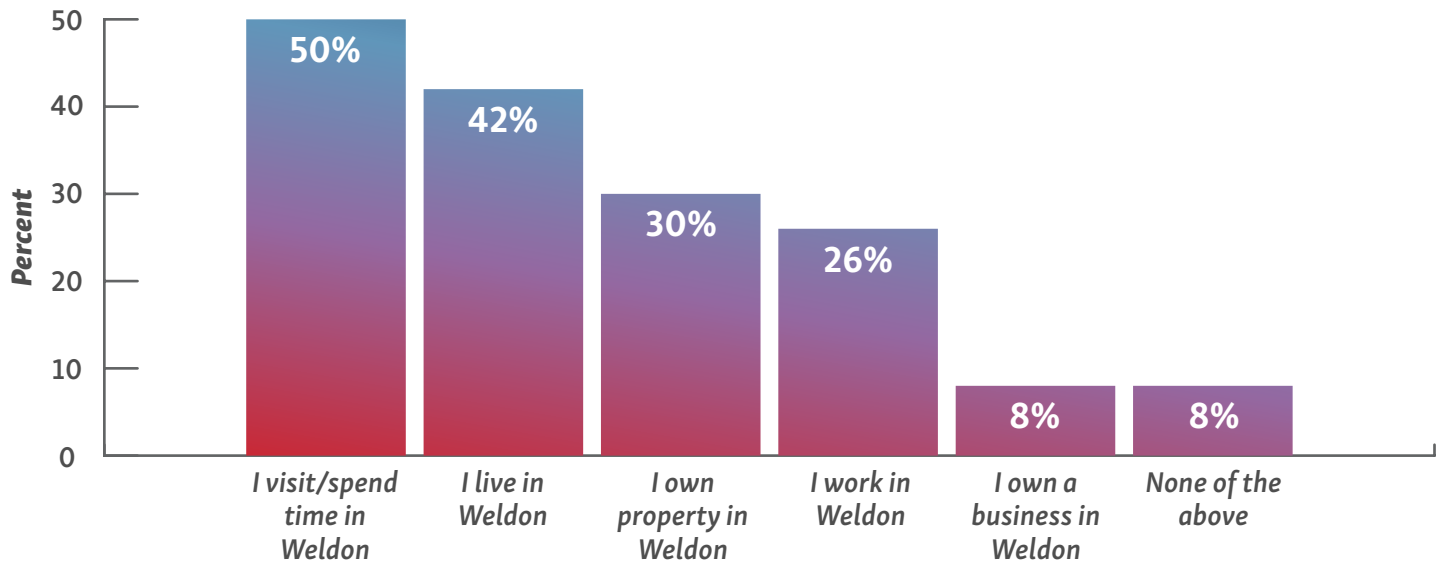


84
PARTICIPANTS

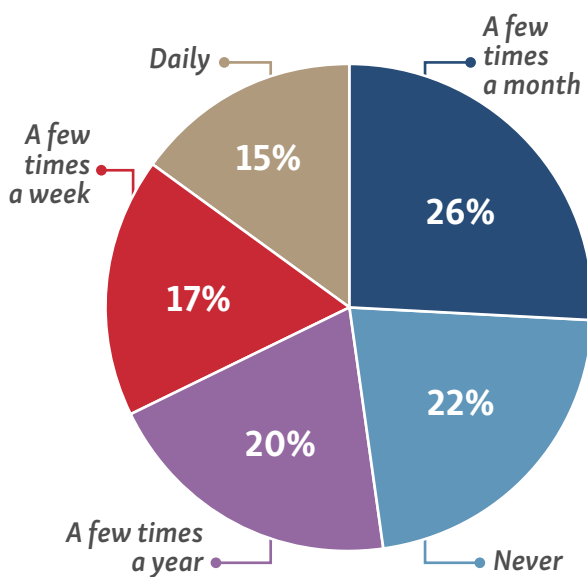


11 QUESTIONS
+ 2 MAPPING
EXERCISES

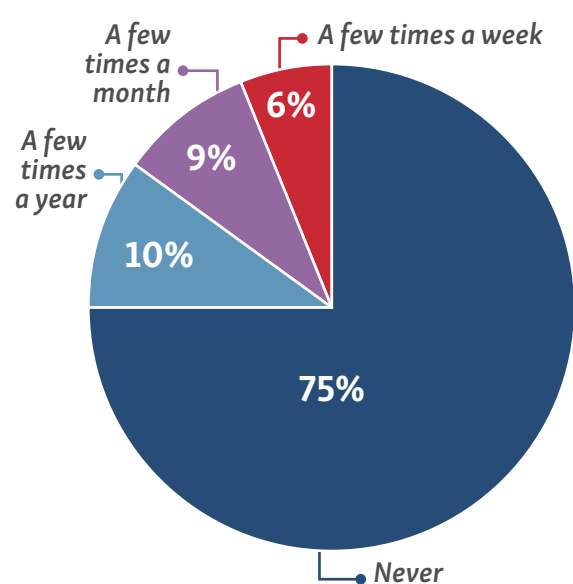
What is your relationship to Weldon? Select all that apply.



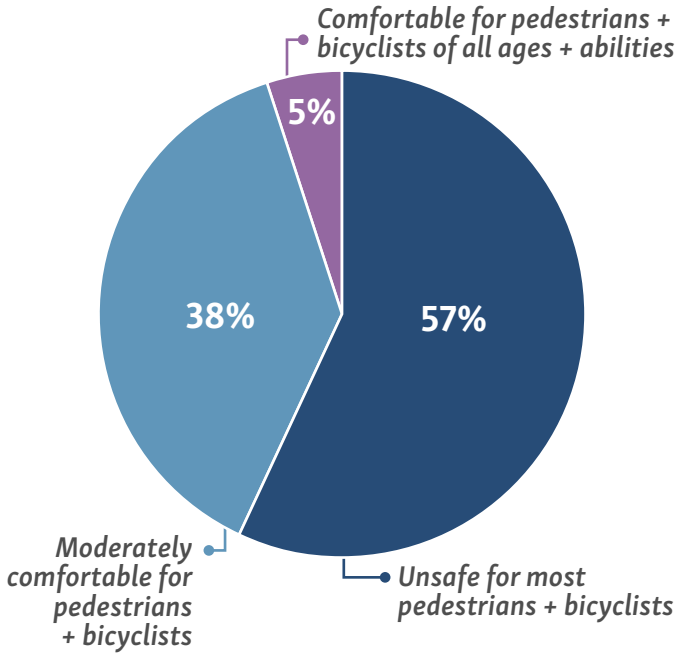
How frequently do you walk in Weldon?



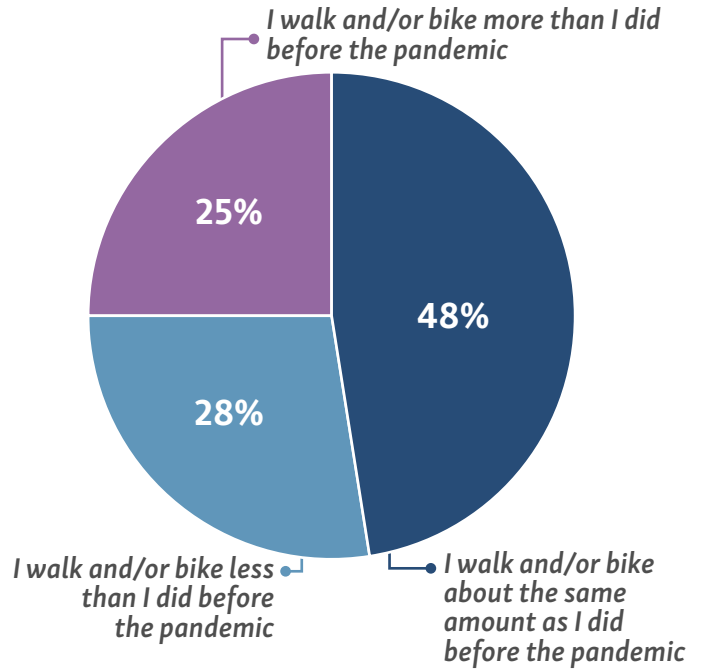
How frequently do you bike in Weldon?



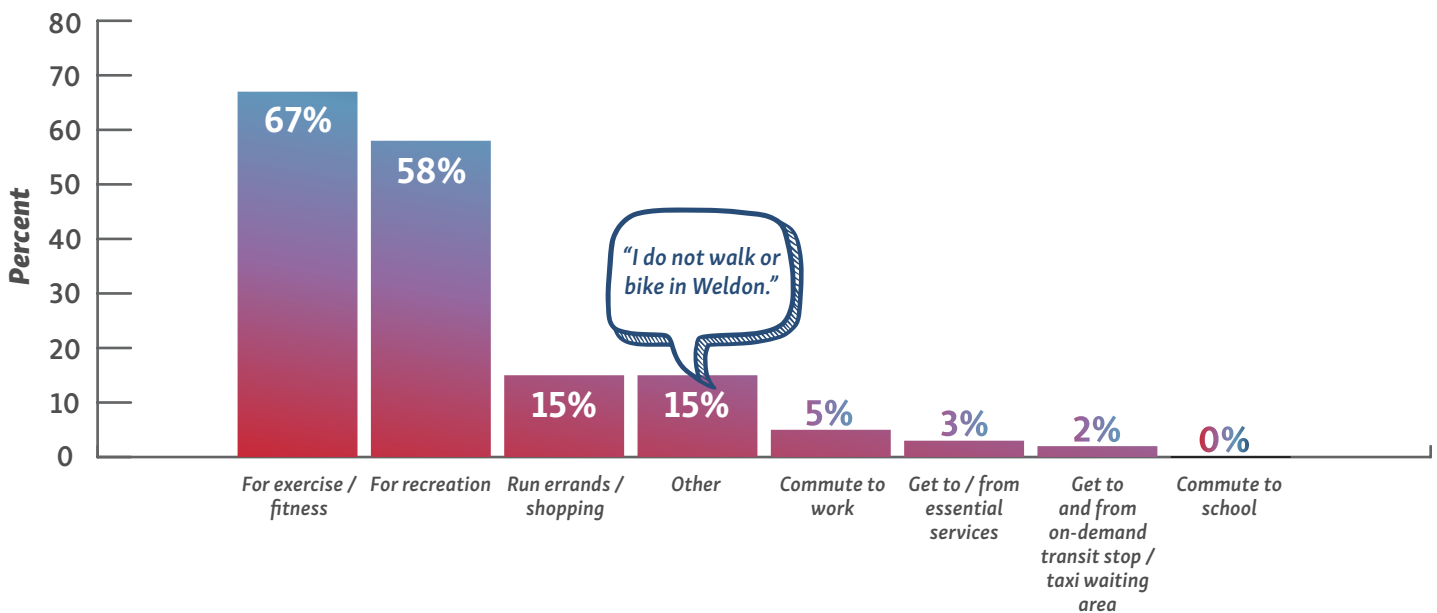
How do you rate levels of comfort for those currently walking and biking in Weldon?



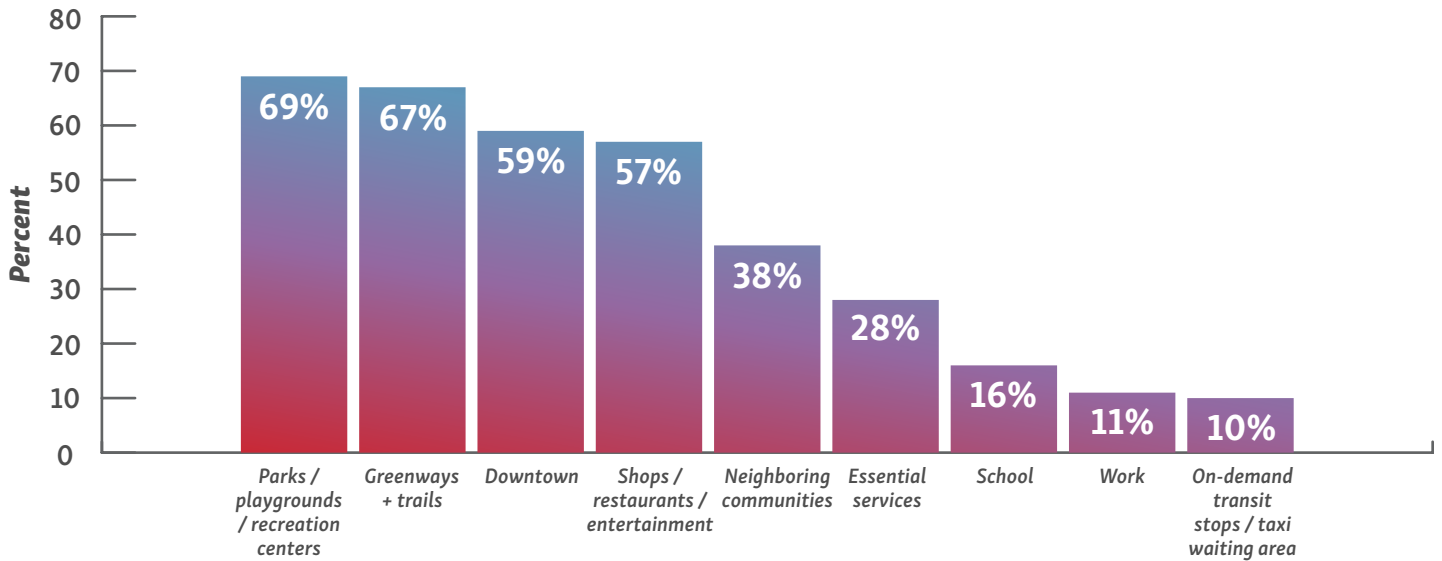
Have your walking and biking habits changed because of the COVID-19 pandemic?



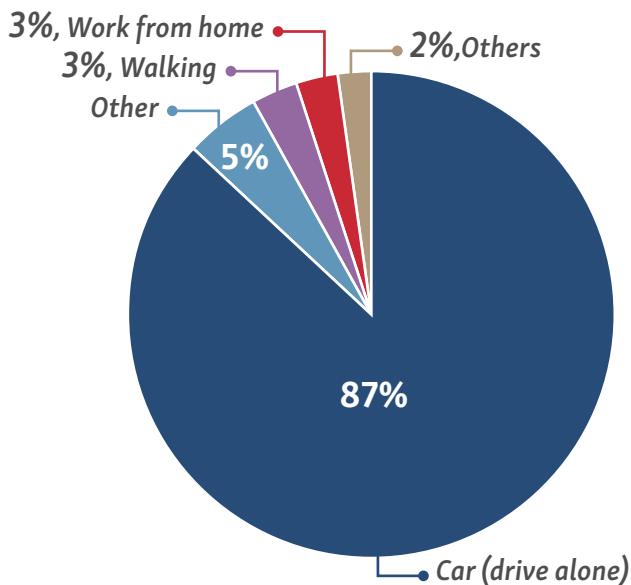
**What are the primary reasons that you or members of your household walk or bike in Weldon?
Select all that apply.**



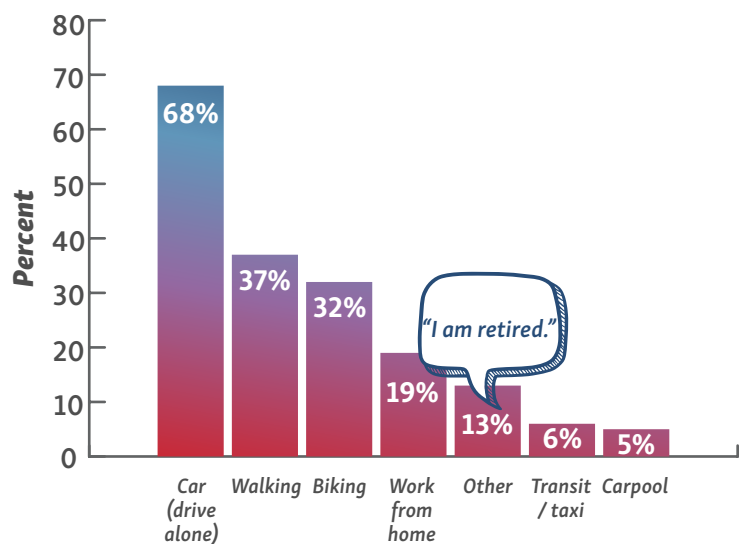
Which destinations would you like to walk or bike to in Weldon if bicycle and pedestrian facilities were improved? Select all that apply.



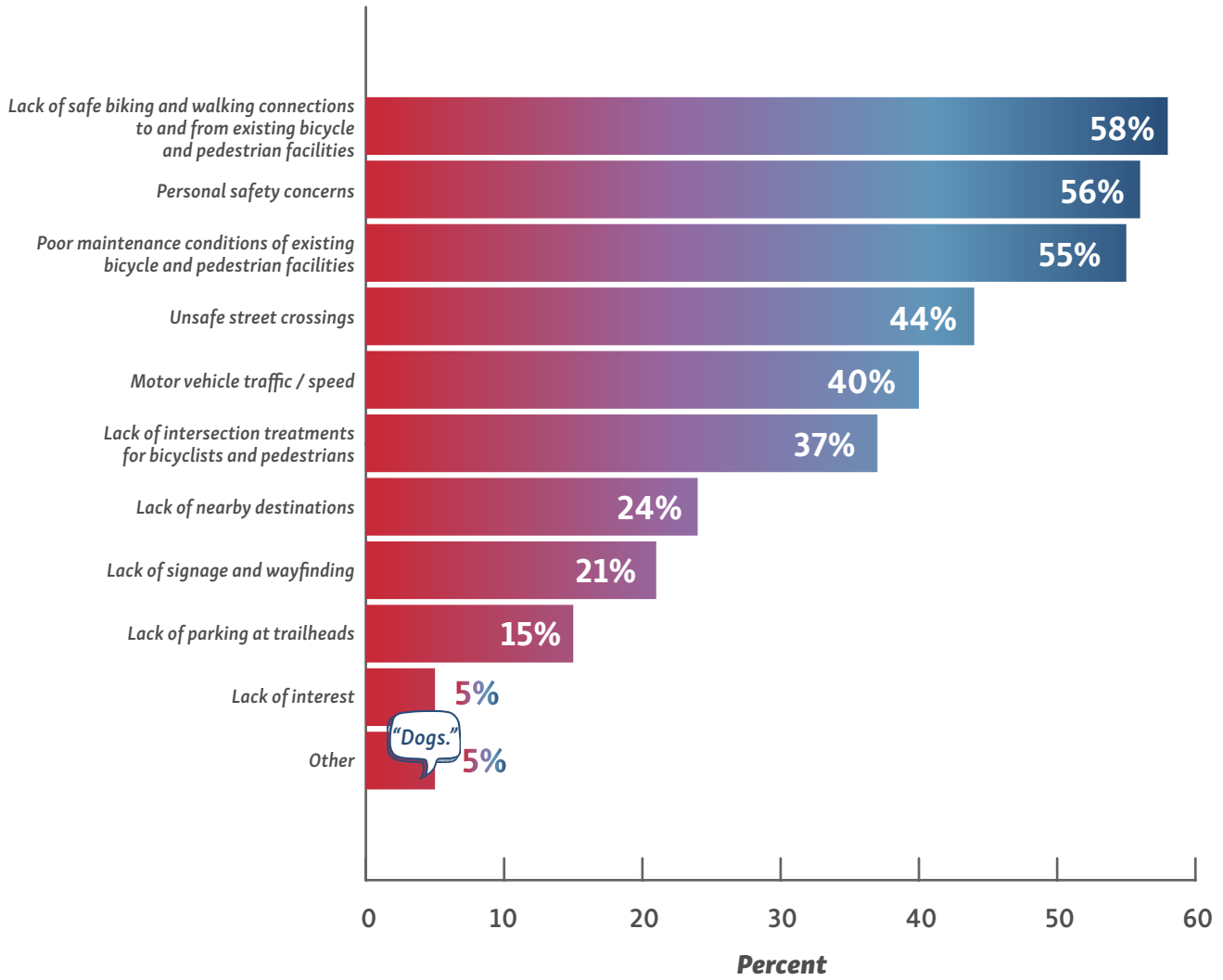
What is your primary mode of transportation for commuting to work and/or school?



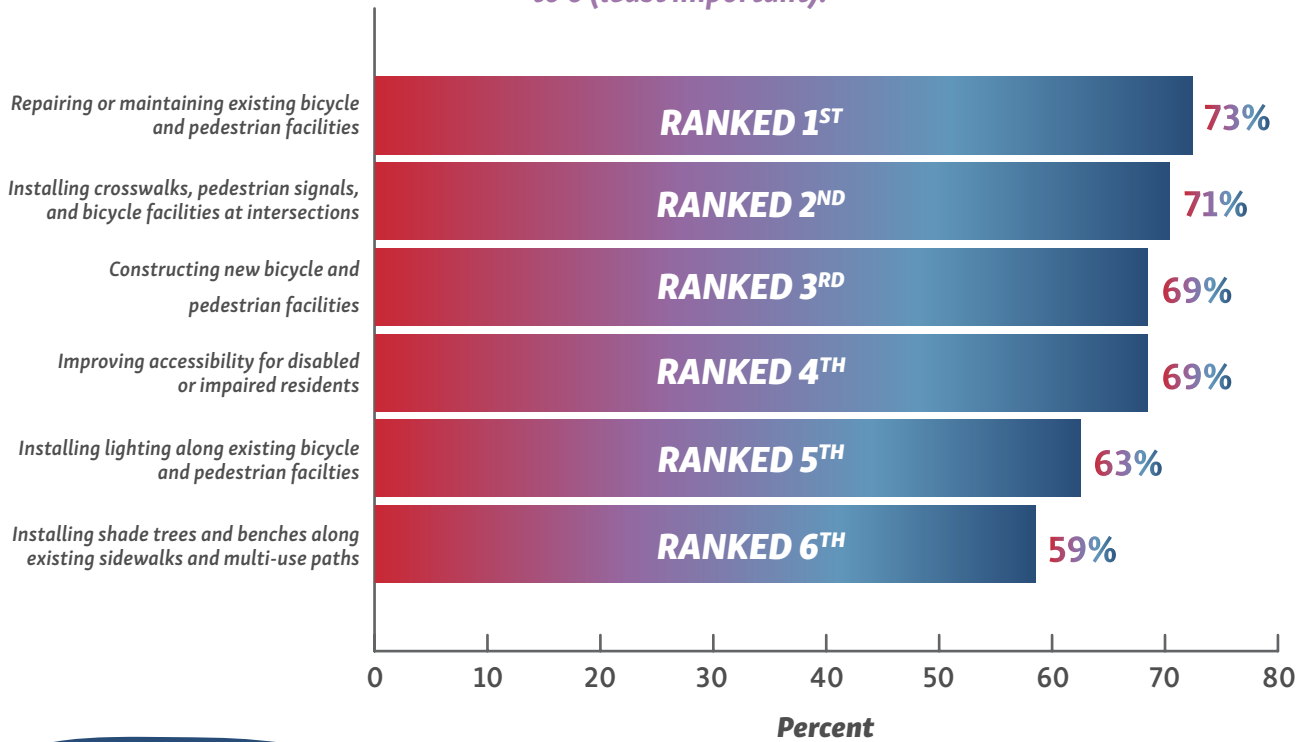
Now consider your desired commute in the future. Which modes would you like to use? Select all that apply.



**What factors discourage you from walking and biking in Weldon?
Select all that apply.**



Rank which infrastructure or program improvements would encourage you to use bicycle and pedestrian facilities more frequently in Weldon? Please rank your top 6 from 1 (most important) to 6 (least important).

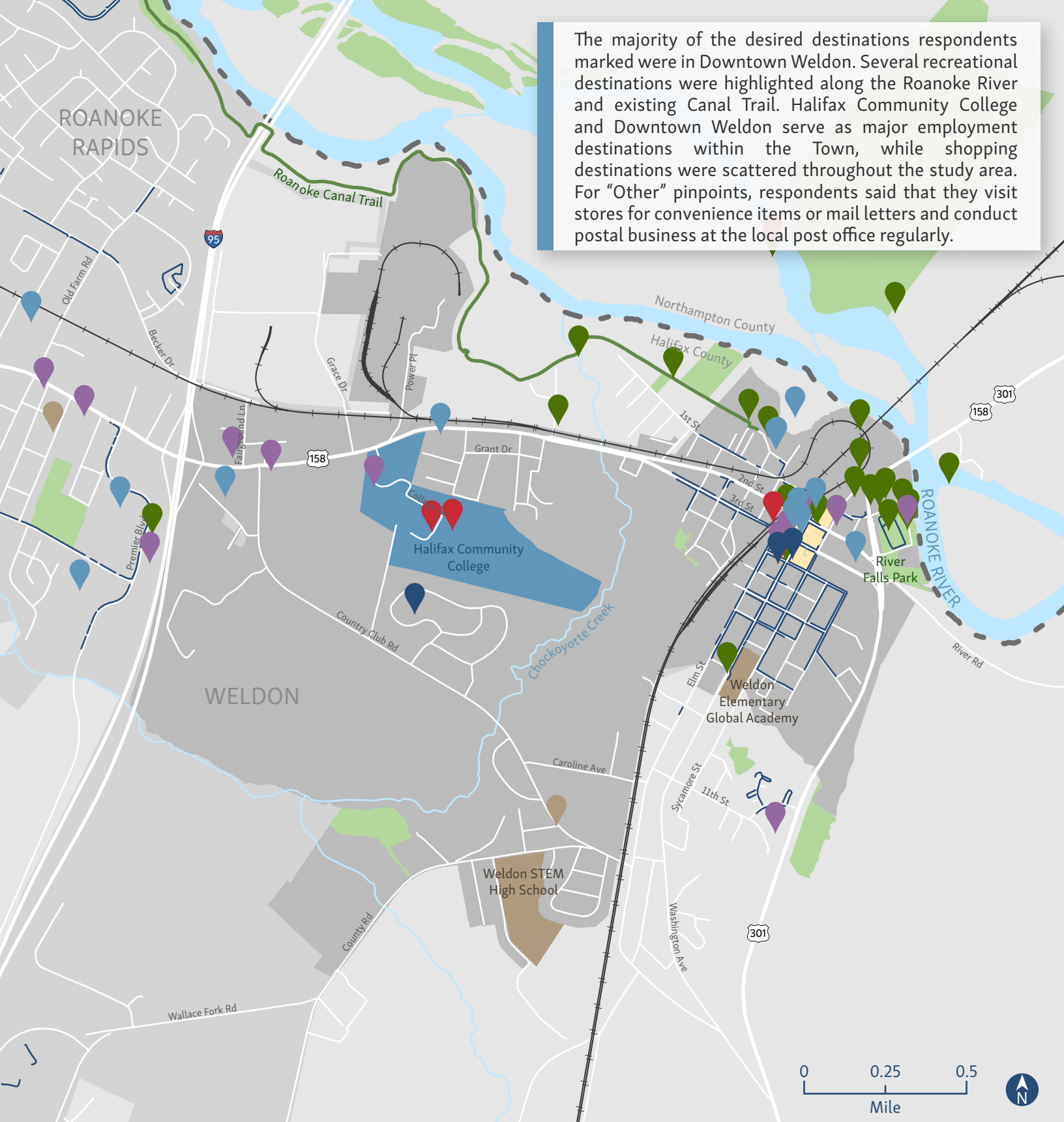


“As an avid cyclist I bicycle in Weldon many times a month - especially at River Falls Park and through the Main Street area.”

“I would love to see areas that people in the community walk with no sidewalk/greenway have a safe established area for pedestrian travel.”

“Weldon’s community is primarily a pedestrian community with many residents dependent solely on sidewalks, bike paths, and safe crosswalks. Unfortunately, Weldon presently is in great need of all!”

The majority of the desired destinations respondents marked were in Downtown Weldon. Several recreational destinations were highlighted along the Roanoke River and existing Canal Trail. Halifax Community College and Downtown Weldon serve as major employment destinations within the Town, while shopping destinations were scattered throughout the study area. For "Other" pinpoints, respondents said that they visit stores for convenience items or mail letters and conduct postal business at the local post office regularly.



TOWN OF WELDON

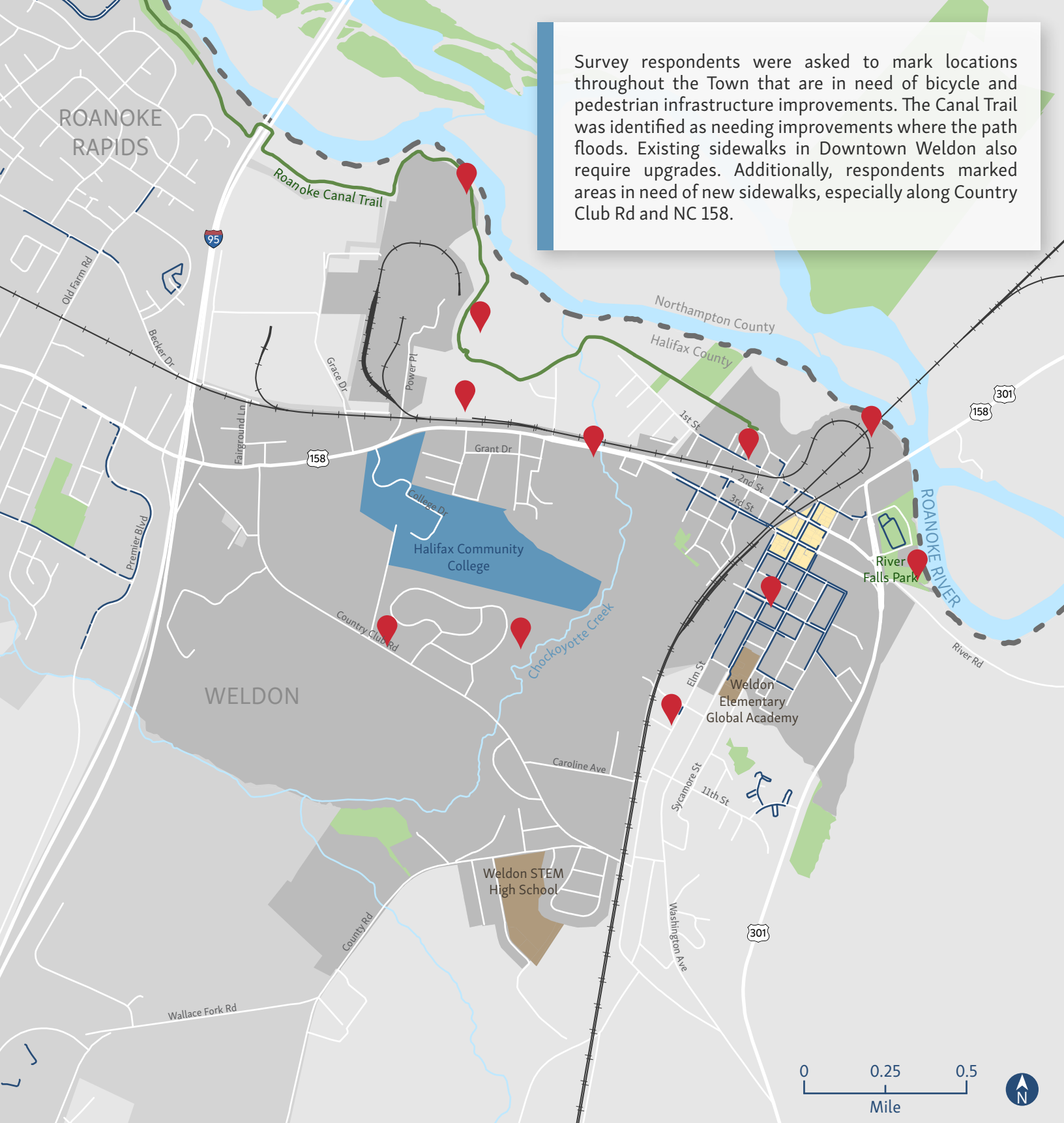
PROJECT ACCELERATION PLAN

Public Survey—Desired Walking + Bicycling Destinations

LEGEND ▶

- Employment Destination
- Recreational Destination
- Residential Destination
- School Destination
- Shopping Destination
- Other Destination
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

Survey respondents were asked to mark locations throughout the Town that are in need of bicycle and pedestrian infrastructure improvements. The Canal Trail was identified as needing improvements where the path floods. Existing sidewalks in Downtown Weldon also require upgrades. Additionally, respondents marked areas in need of new sidewalks, especially along Country Club Rd and NC 158.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Public Survey—Bicycle + Pedestrian Infrastructure Improvements

- Improvement Location
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

LEGEND ▶



04

**Network
Recommendations**



OVERVIEW

This chapter covers design standards for the recommended multimodal network projects in the plan, with detailed descriptions and visuals of typical cross sections for specific project facility types. It introduces the proposed Town network, highlighting essential elements and crossing treatment enhancements for safe and efficient multimodal corridor traversal, improving overall transportation accessibility. Finally, it explores the prioritization methodology, findings, and project cut sheets, offering detailed insights into individual priority projects in the Town.

The following sections are covered in this chapter:

- Design Guidance
- Recommended Projects
- Prioritization
- Priority Projects

DESIGN GUIDANCE

Establishing a secure and easily accessible bicycle and pedestrian system connecting individuals, parks, and community destinations necessitates a strategic set of recommendations, including proposed strategies for network, policy, and amenity development. This chapter outlines design standards and typical sections for sidewalks, shared use paths, and on-street bicycle facilities to guide the development of such a system.

FACILITY TYPES

Identifying suitable multi-modal facilities for a community's active transportation network involves a context-sensitive approach, considering factors such as roadway design, network connectivity, and land use. Bicycle and pedestrian facility selection along a roadway is influenced by roadway speeds and traffic volumes. As vehicle speed and volume increase, the need for physical separation grows to ensure comfortable walking and bicycling.

Network considerations prioritize interconnected bicycle and pedestrian facilities, enabling people of all ages and abilities to reach their destination safely and conveniently. Land use considerations hinge on an area's density, determining the feasibility of supporting multimodal travel. High-density areas can accommodate various bicycle and pedestrian facilities, while low-density areas require options that facilitate longer-distance travel between destinations.

The recommended facility types in this plan include greenways / shared use paths, sidepaths, sidewalks, bicycle lanes, protected bicycle lanes, and shared lane markings / bicycle boulevards. These primary facility types are outlined on the following pages. For further insights into facility design, please refer to **Appendix B: Design Resources**.



Roanoke Canal Trail

DESIGN STANDARDS + TYPICAL SECTIONS

The standards, methods, and materials employed in the design of bicycle and pedestrian facilities significantly influence the safety and experience of the diverse user base engaging in these spaces for various recreational, utilitarian, health, and transportation purposes. This section outlines key elements of facility design to provide guidance for the Town of Weldon and its partners in planning, designing, constructing, and maintaining bicycle and pedestrian facilities. The objective is to create connections to diverse destinations, ensure a comfortable experience for a variety of users, and construct facilities at a sustainable and manageable scale.

GREENWAY / SHARED USE PATH

Weldon's bicycle and pedestrian facility recommendations introduce a greenway and shared-use path system that aligns with recent developments in active transportation and planning. These recommendations stem from community and stakeholder input, an assessment of existing conditions, identification of key destinations and connections through the planning process, and a thorough prioritization process. The envisioned system aims to establish a network of greenways and shared-use paths, ensuring accessibility and a sense of safety for individuals of all ages and abilities.

MATERIALS SELECTION

Considerations for selecting materials for greenways encompass factors such as the intended facility type (recreational or commuter), anticipated user activities (e.g., walking, bicycling, running, rollerblading), the age and ability of trail users, environmental conditions, construction costs, maintenance burdens and expenses, and funding source requirements, among others. Below, are descriptions and illustrations of various materials, providing a comprehensive overview for informed decision-making.

PAVED SURFACE

Paved trail surfaces, like asphalt or concrete, enhance accessibility for users of all ages and abilities. Asphalt is often the preferred and cost-effective choice for paved trails due to its popularity. While concrete offers greater durability, it comes at a higher cost than asphalt. Consequently, concrete trails are commonly found in urban settings, where projected user volumes are high or the trail may be subject to frequent vehicular loading. They are also preferred in areas prone to heavy flooding forces that could potentially damage the trail.



Concrete paved trail - Lower Booker Creek Trail (Chapel Hill, NC)



Asphalt paved trail - American Tobacco Trail (Durham, NC)

NATURAL SURFACE

Two natural surfaces under consideration in this study are compacted aggregates and compacted native soil, both recognized as budget-friendly alternatives implemented in trail systems across North Carolina and beyond.

Compacted aggregates, particularly granite fines, have proven to be durable, cost-effective, and easily obtainable for constructing natural trail surfaces. Their compaction capabilities, especially when sourced locally from quarries, create a level and accessible surface for most users. Proper compaction and management of adjacent drainage are crucial for success, with annual maintenance required to ensure proper drainage and prevent the formation of water rills and grooves during regular rain events. Establishing drainage during construction is vital to prevent erosive effects from concentrated runoff crossing the trail perpendicularly.

Another cost-effective option is compacted native soil, which is easily constructed but relies heavily on material compaction and proper drainage. The existing soil must achieve compaction levels of 95-98% for trail integrity. If native soil is unsuitable, importing material is an option at an additional cost, and installation may require a roller and small excavating equipment based on topography. Preventing perpendicular surface drainage is essential to avoid washout and rills, and annual maintenance, including re-compaction and additional soil installation in rain-prone areas, is necessary for maintaining the trail's integrity.



Compacted aggregate unpaved trail - Peavine Trail (Marion, NC)



Compacted soil unpaved trail - American Tobacco Trail (Apex, NC)

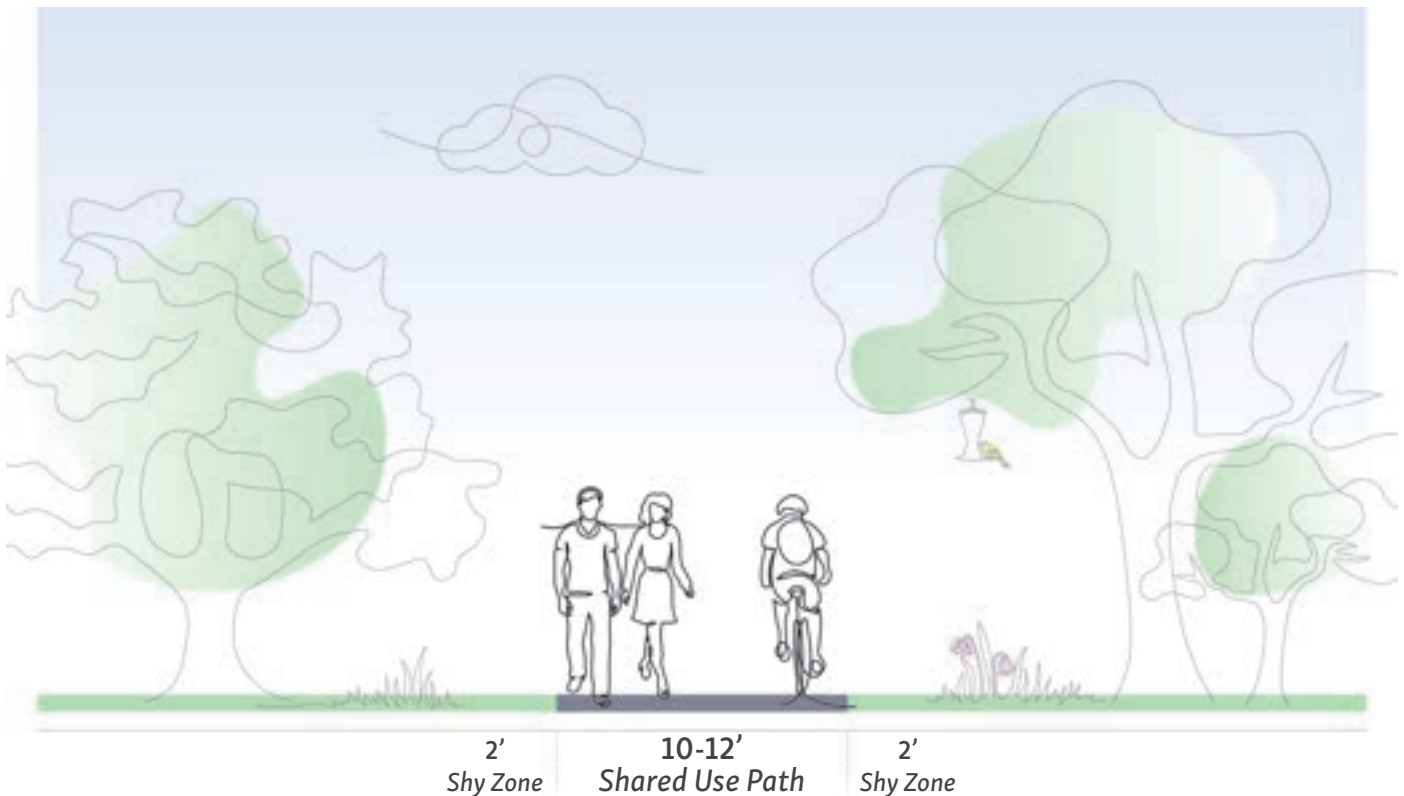
TYPICAL CROSS SECTION

Greenways, designed independently of roadways, maintain complete separation from traffic. Nevertheless, when selecting bicycle and pedestrian facilities, it is crucial to view greenways and shared-use paths as viable multimodal options for roadways with traffic volumes exceeding 6,000 vehicles per day (vpd) and speed limits exceeding 35 mph. Greenways can serve as alternative network connections to arterial and collector roadways, making them suitable for both low-density and high-density areas as corridor connections.

Greenways Preferred: For the preferred greenway or shared-use path typical section, a 12 ft wide paved trail is recommended, striking a balance between minimal long-term maintenance and improved eligibility for various funding sources. Given the specific site conditions within the Town of Weldon, expected greenway usage, and cost considerations, asphalt pavement is the preferred choice. However, there may be instances where limited sections of concrete pavement are necessary to address specific site conditions. To ensure the full 12 ft greenway width remains entirely usable, it is crucial to maintain shoulders or shy zones of 2 ft or greater, keeping them clear of obstacles.

Greenways Constrained: In scenarios where environmental or right-of-way constraints exist, a slightly narrower 10 ft wide natural surface or asphalt trail may be considered. This alternative may also be suitable for neighborhood connections to greenway corridors. When contemplating the use of natural surfaces for constrained or connector segments, it's essential to evaluate the overall life-cycle costs, which might be higher due to long-term maintenance requirements. As in the preferred typical section, shoulders or shy zones of 2 ft or greater should be kept obstacle-free to maintain the full greenway width usability.

Not drawn to scale.



Greenways: Preferred: 12' // Constrained: 10'

SIDEPATH

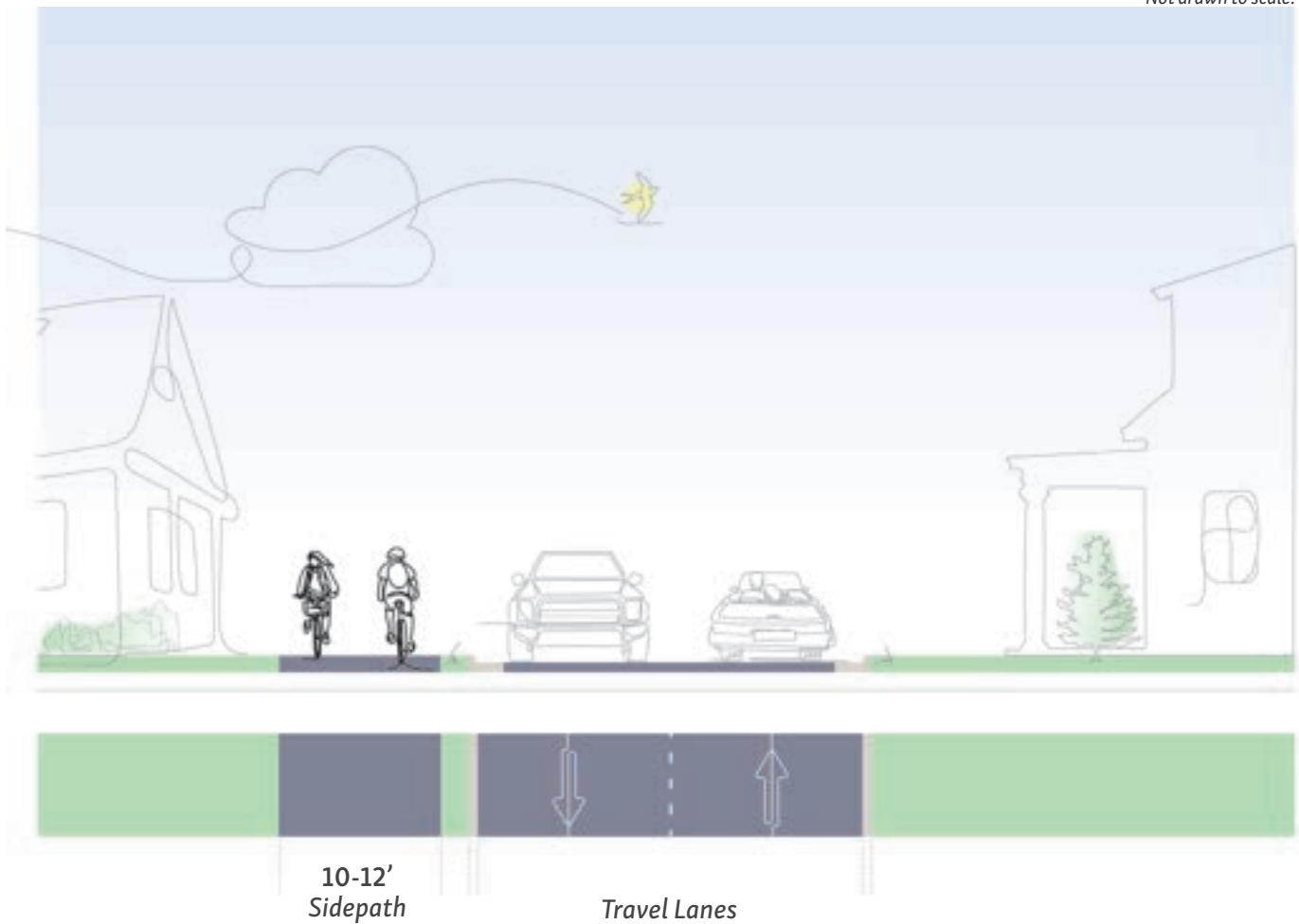
A sidepath is a bidirectional shared-use path that runs parallel to and alongside a roadway. These paths provide a secure and low-stress environment for both cyclists and pedestrians, particularly in areas with heavy traffic and high-speed conditions. Typically implemented on roads with daily traffic volumes exceeding 6,000 vehicles per day (vpd) and speed limits of 35 miles per hour or higher, sidepaths are commonly situated on arterial and collector roadways. They are especially recommended for deployment in high-density areas, providing designated space for both bicyclists and pedestrians.

For the Town, it is recommended to have a 12 ft paved sidepath with at least a 2 ft buffer between the facility and the roadway. In constrained environments, sidepaths may be narrowed to 10 ft.



Sidepath in Durham, NC (lane markings optional)

Not drawn to scale.



10-12'
Sidepath

Travel Lanes

Sidepaths: Preferred: 12' // Constrained: 10'

Buffer: 2' with curb+gutter minimum //

4' in subdivisions with curb+gutter per Subdivision Ordinance // 5' without

SIDEWALK

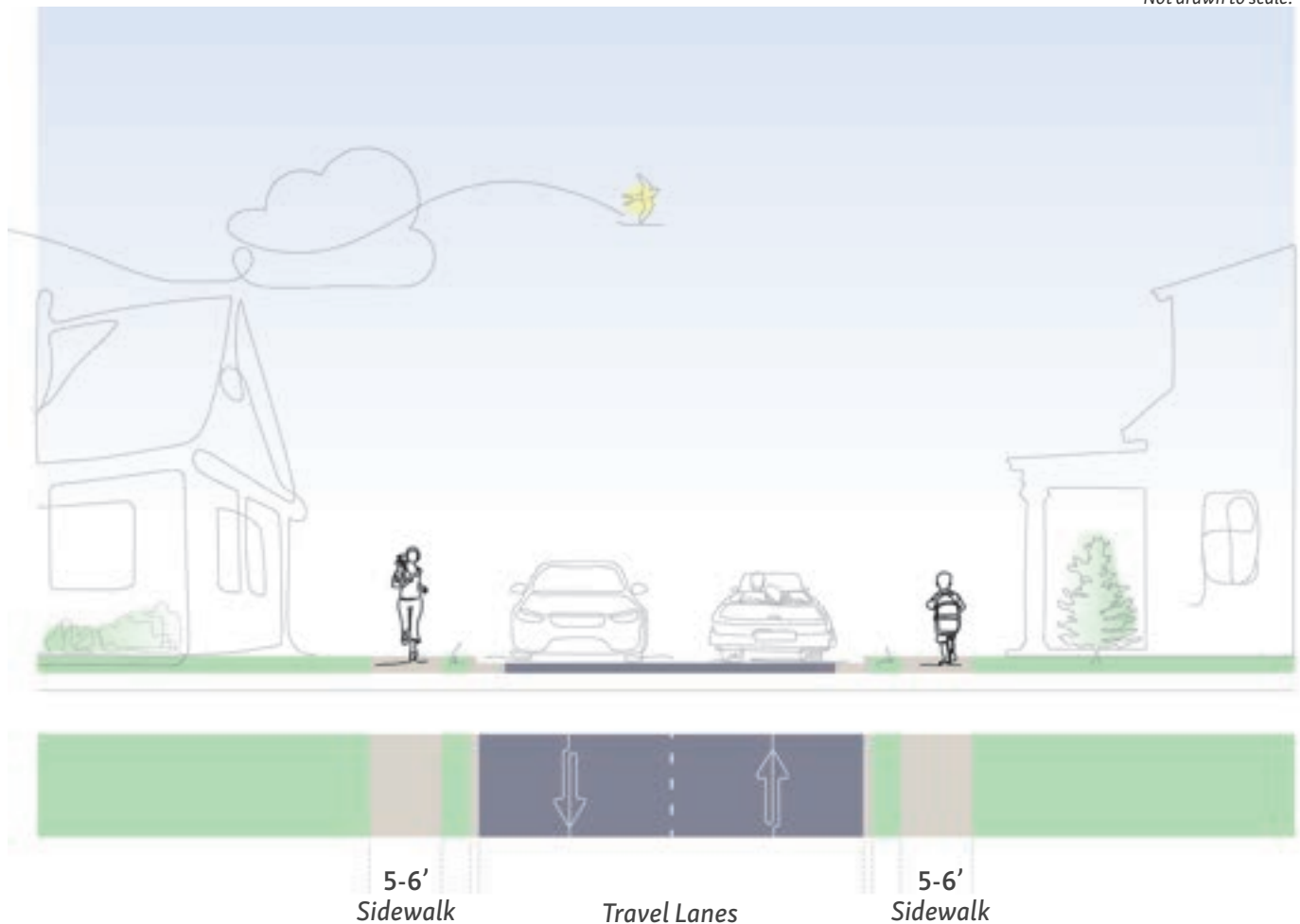
A sidewalk is a bidirectional path that offers a dedicated travel area for pedestrians within the transportation network. Physically separated from the roadway by either a curb or an unpaved buffer space, sidewalks are a recommended feature for all roads, except those categorized as low-speed and low-volume. It is advisable to consider sidewalks for roadway corridors with traffic volumes exceeding 2,000 vehicles per day (vpd) and speed limits higher than 10 mph. Additionally, sidewalks are recommended for all types of roadways where pedestrian activity is anticipated. Sidewalks are especially appropriate in areas characterized by diverse land uses, proving beneficial for short-distance travel along roadways featuring pedestrian-generating developments, such as neighborhoods, schools, and employment centers.



Sidewalks on W 3rd St in Downtown Weldon

Within the Town, 6 ft sidewalks with a minimum 2 ft buffer between the facility and the roadway should be considered. In constrained environments, sidewalks may be narrowed to 5 ft.

Not drawn to scale.



5-6'
Sidewalk

Travel Lanes

5-6'
Sidewalk

Sidewalks: Preferred: 5' // Constrained: 6'

Buffer: 2' with curb+gutter //

4' in subdivisions with curb+gutter per Subdivision Ordinance // 5' without

BICYCLE LANE

A bicycle lane is a designated portion of the roadway for cyclists, demarcated by pavement markings and/or signage. These lanes provide a dedicated facility for bicycles where enhanced connectivity for cyclists is desired. Typically situated on the right side of the road, between the travel lane, parking lane, and curb or roadway edge, bicycle lanes may also incorporate a painted buffer if space permits.

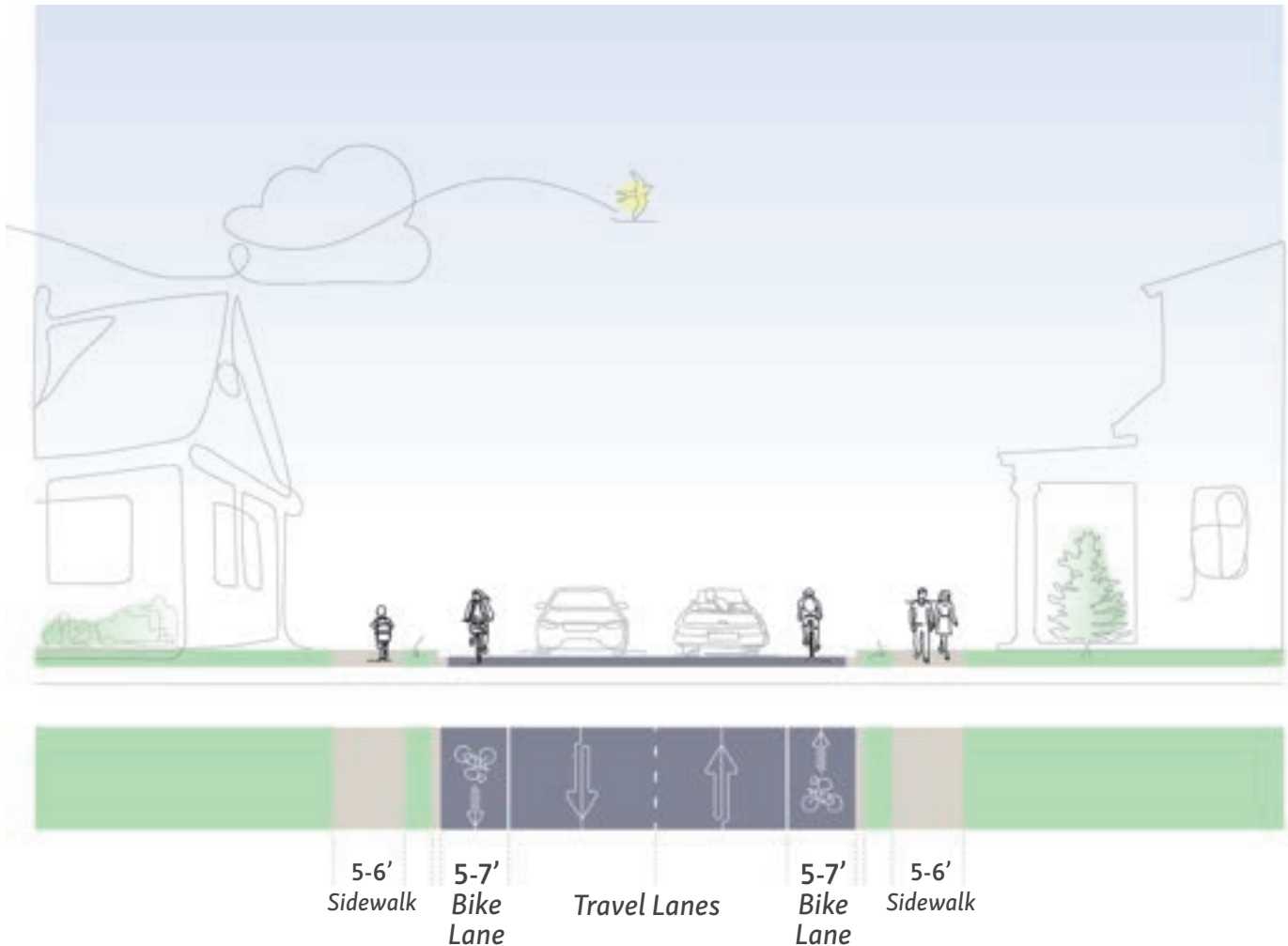
Consideration for implementing bicycle lanes is recommended for roadway corridors with traffic volumes exceeding 3,000 vehicles per day (vpd) and speed limits of 25 mph or higher. If speed limits surpass 35 mph or traffic volumes exceed 6,000 vpd, it is advisable to include physical separation, such as bollards. Bicycle lanes are well-suited for areas with diverse land uses that support bicycle trips, accommodating both short- and long-distance bicycle travel.



Bicycle lane on Granite Falls Blvd in Rolesville, NC

The recommended width for bicycle lanes in the Town is 6-7 ft, while in constrained spaces, a width of 5 ft is recommended.

Not drawn to scale.



Bicycle Lanes: Preferred: 7' // Constrained: 5'

PROTECTED BICYCLE LANE

A protected bicycle lane is a dedicated cycling path physically separated from traffic by elements such as parking, a curb, or other barriers. These lanes are designed to provide a low-stress experience for bicyclists, particularly along routes with high-speed or high-volume traffic. Typically positioned on the right side of the roadway, between travel or parking lanes and a curb or the edge of the roadway, protected bicycle lanes incorporate a physical barrier such as bollards, planting boxes, parked cars, or a curb. On one-way streets, the bicycle lane may be situated on the left side of the roadway.

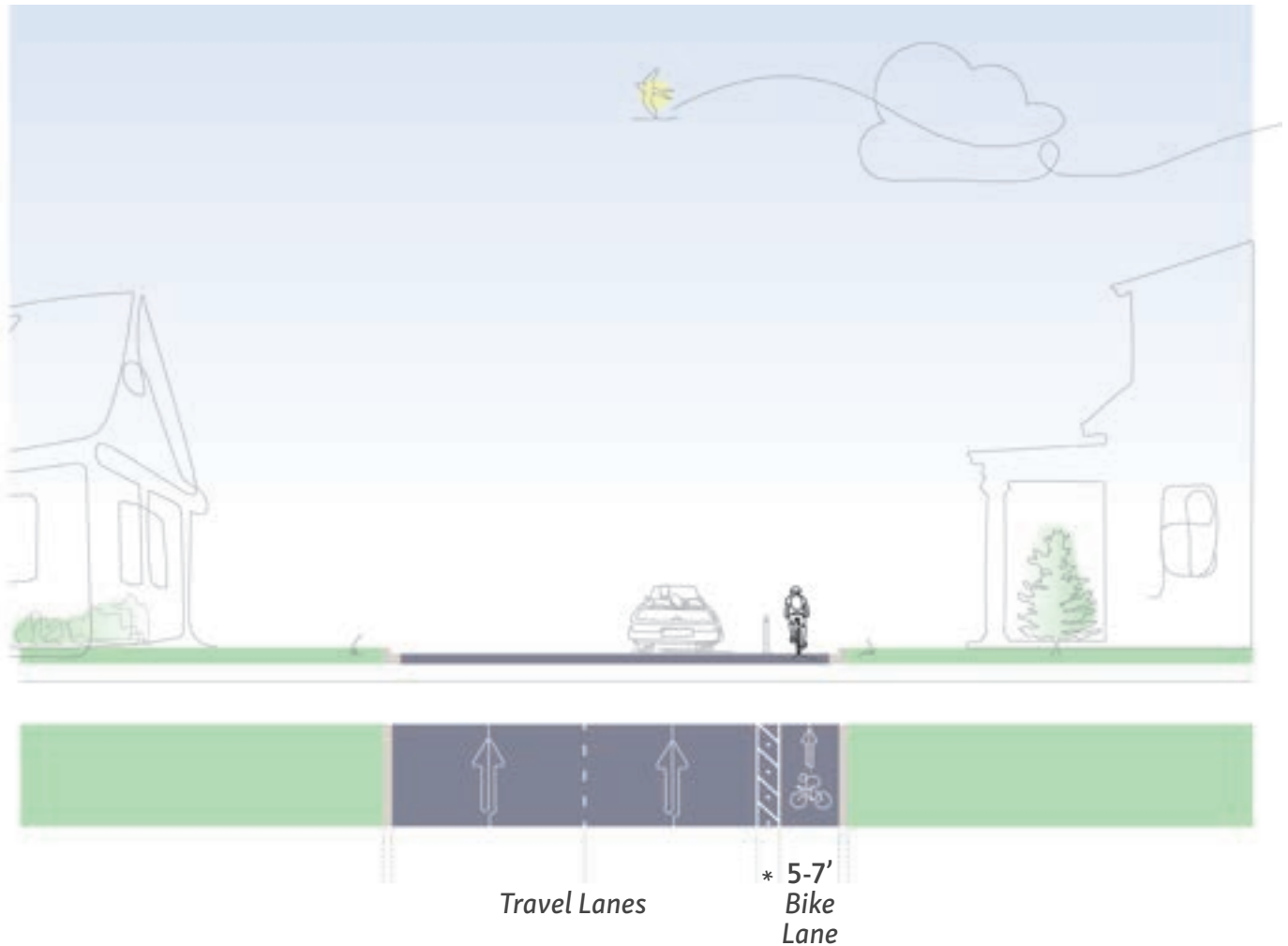
It is recommended to consider protected bicycle lanes for roads with traffic volumes exceeding 6,000 vehicles per day (vpd), speed limits of 35 mph or higher, multiple travel lanes, parking lanes, or locations where bicyclists might feel uneasy due to specific roadway design characteristics.

The recommended width for bicycle lanes in the Town is 6-7 ft, while in constrained spaces, a width of 5 ft is recommended. Bike lane buffers should be 1.5 to 4 ft in width.



Protected bicycle lane on N West St in Raleigh, NC

Not drawn to scale.



Bicycle Lanes: Preferred: 7' // Constrained: 5'
***Bike Lane Buffers:** 1.5-4'

SHARED LANE MARKING / BICYCLE BOULEVARD

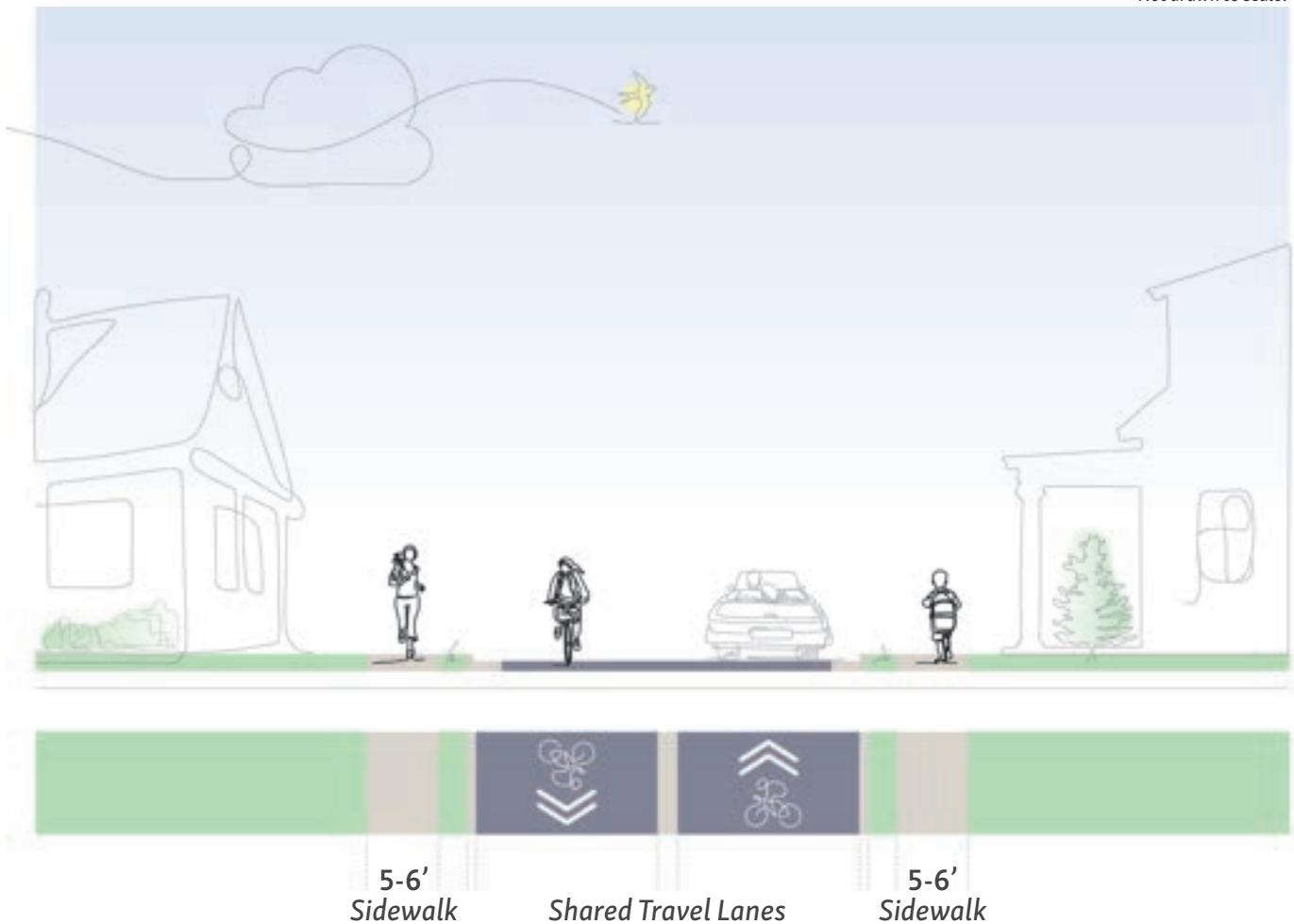
A shared lane marking, commonly known as a “sharrow” or bicycle boulevard, designates a roadway for joint use by both bicyclists and automobiles. Bicycle boulevards are specifically designed to offer low-stress bicycle connections along local streets characterized by low vehicle speeds and traffic volumes. Recognizable by shared lane markings on the roadway, these boulevards may also include signage to indicate the route or alert drivers to the presence of bicyclists.

Best suited for streets with traffic volumes below 2,000 vehicles per day (vpd) and speed limits below 20 mph, bicycle boulevards are particularly fitting for residential neighborhoods. They facilitate connectivity to various residential and commercial destinations. Creating a network of bicycle boulevard corridors within a community contributes to establishing a safe and comfortable bicycling environment for individuals of all ages and abilities.



Shared lane marking on Granite Falls Blvd in Rolesville, NC

Not drawn to scale.

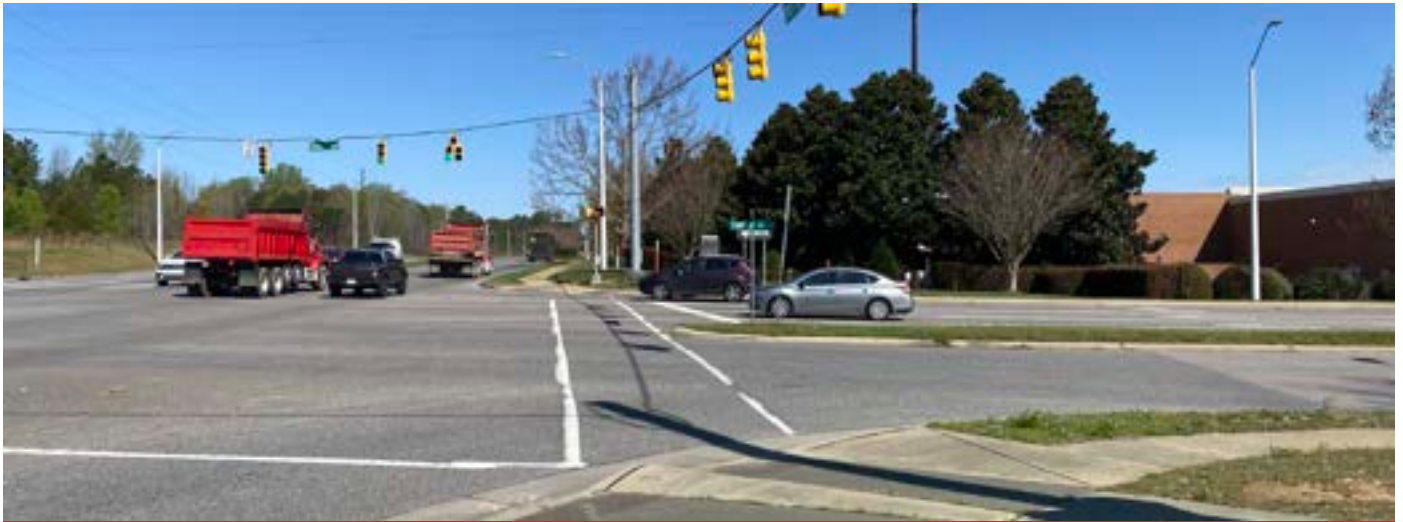


INTERSECTION TREATMENTS + CROSSINGS

Numerous conflicts among bicyclists, pedestrians, and motor vehicles tend to arise at intersections. Historically, roadway design choices have frequently heightened exposure and risk for bicyclists and pedestrians, diminishing the safety and comfort of the overall bicycle and pedestrian network. Such decisions have, in turn, discouraged biking and walking between destinations. Recognizing this, communities striving to enhance safety for users of all ages and abilities must prioritize the critical aspect of intersection and crossing design.

CROSSWALK

A crosswalk is a designated place for pedestrians and greenway users to cross a road at an intersection or midblock crossing. Standard transverse crosswalk markings are recommended to alert motorists of pedestrian crossing points across roadways. Inlay or thermoplastic tape is also recommended for highly reflective crosswalk markings. Crosswalks should be accompanied by Manual on Uniform Traffic Control Devices (MUTCD) pedestrian warning and advance yield signs W11-2, W16-7P, and R1-6 to alert motorists of a crosswalk.



Crosswalk on Timber Dr in Garner, NC

HIGH-VISIBILITY CROSSWALK

A crosswalk is a designated place for pedestrians and greenway users to cross a road at an intersection or midblock crossing. High-visibility crosswalk markings, such as bar pairs, continental, and ladder are recommended to alert motorists of pedestrian crossing points across roadways. Inlay or thermoplastic tape is also recommended for highly reflective crosswalk markings. Crosswalks should be accompanied by Manual on Uniform Traffic Control Devices (MUTCD) pedestrian warning and advance yield signs W11-2, W16-7P, and R1-6 to alert motorists of a crosswalk.



High-visibility crosswalk on Spring Dr in Garner, NC



High-visibility greenway crosswalk at a street intersection

HIGH-VISIBILITY MID-BLOCK CROSSWALK

A mid-block crosswalk is a designated place for pedestrians and greenway users to cross a road between intersections. Mid-block crosswalks are often placed in locations that allow people to cross at locations such as schools, parks, and business districts, or at uncontrolled intersections. High-visibility crosswalk markings, such as bar pairs, continental, and ladder striping are recommended to alert motorists of bicycle and pedestrian crossing points across roadways. Inlay or thermoplastic tape is also recommended for highly reflective crosswalk markings. High-visibility mid-block crosswalks should be accompanied by Manual on Uniform Traffic Control Devices (MUTCD) pedestrian warning and advance yield signs W11-2, W16-7P, and R1-6 to alert motorists of a crossing.



High-visibility mid-block crosswalk on Aversboro Rd in Garner, NC



High-visibility mid-block crosswalk on Washington Ave in Weldon

RECTANGULAR RAPID FLASH BEACON (RRFB)

A Rectangular Rapid Flash Beacon (RRFB) is a pedestrian-activated signal used in combination with pedestrian or trail crossing warning signage to improve safety at uncontrolled, marked crosswalks. The device includes two rectangular-shaped yellow indicators, each with an LED-array-based light source, that flash with high frequency when activated. RRFBs should be accompanied by MUTCD pedestrian warning and advance yield signs W11-2 and W16-7P to alert motorists of a crosswalk.

RRFBs are placed on both ends of a crosswalk. If the crosswalk contains a pedestrian refuge island or other type of median, an RRFB should be placed to the right of the crosswalk and on the median (instead of the left side of the crosswalk). RRFBs typically draw power from standalone solar panel units but may also be wired to a traditional power source.



Rectangular rapid flash beacon along a trail



Rectangular rapid flash beacon

BICYCLE THROUGH LANE MARKINGS / INTERSECTION CROSSING MARKINGS

Bicycle through lane markings or intersection crossing markings serve to delineate the planned route for bicyclists. These markings not only direct bicyclists along a secure and straightforward path through intersections, encompassing driveways and ramps, but also establish a distinct boundary between the paths of through bicyclists and either through or crossing motor vehicles in the adjacent lane. The application of striping, colors, or other visual methods is utilized to effectively draw attention to these pathways.



Intersection crossing markings with dotted lines and shared lane markings (NACTO)



Intersection crossing markings with dotted lines and colored conflict areas (NACTO)

MEDIAN REFUGE ISLAND

Median refuge islands are protected spaces placed in the center of the street to facilitate bicycle and pedestrian crossings. Crossings of two-way streets are facilitated by allowing bicyclists and pedestrians to navigate only one direction of traffic at a time.



Median refuge island

BICYCLE + PEDESTRIAN NETWORK

The table below provides a comprehensive list of bicycle and pedestrian projects recommended in this Plan, as well as accompanying intersection and crossing treatments.

PROJECT	FROM	TO	TYPE	INTERSECTION + CROSSING TREATMENT
US-158, north side	I-95	Halifax Community College (HCC)	Sidewalk	Crosswalk, High-Visibility Crosswalk
US-158, south side	HCC	Poplar St	Sidewalk	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk
W 1st St	Water St	Walnut St	Sidewalk	Crosswalk, High-Visibility Crosswalk
Water St/Mill St	Water St	Walnut St	Sidewalk	Crosswalk, High-Visibility Crosswalk
Poplar St	W 1st St	W 3rd St	Sidewalk	Crosswalk, Mid-Block Crosswalk, Rectangular Rapid Flashing Beacon
W 2nd St	S Walnut St	W 3rd St	Sidewalk	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk, Rectangular Rapid Flashing Beacon
W 3rd St	W 2nd St	Elm St	Sidewalk	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk, Railroad Crossing
S Walnut St	W 1st St	W 3rd St	Sidewalk	Crosswalk, High-Visibility Mid-Block Crosswalk
1st St at Railroad Crossing	Weldon Memorial Library	Washington Ave	Sidewalk	Railroad Crossing
5th St	Elm St	Maple St	Sidewalk	Crosswalk
E 6th St	Sycamore St	Maple St	Sidewalk	Crosswalk
7th St	Elm St	Maple St	Sidewalk	Crosswalk
E 8th St	Washington Ave	Sycamore St	Sidewalk	High-Visibility Crosswalk
W 9th St	Elm St	Washington Ave	Sidewalk	High-Visibility Crosswalk
11th St	Elm St	Dogwood Dr	Sidewalk	Crosswalk, High-Visibility Crosswalk
Dogwood Dr	E 11th St	US-301	Sidewalk	High-Visibility Crosswalk
Washington Ave	Elm St	US-301	Sidewalk	High-Visibility Crosswalk
Elm St/Old Halifax Rd	W 3rd St	Washington Ave	Sidewalk	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk
Washington Ave	W 9th St	11th St	Sidewalk	Crosswalk, High-Visibility Crosswalk

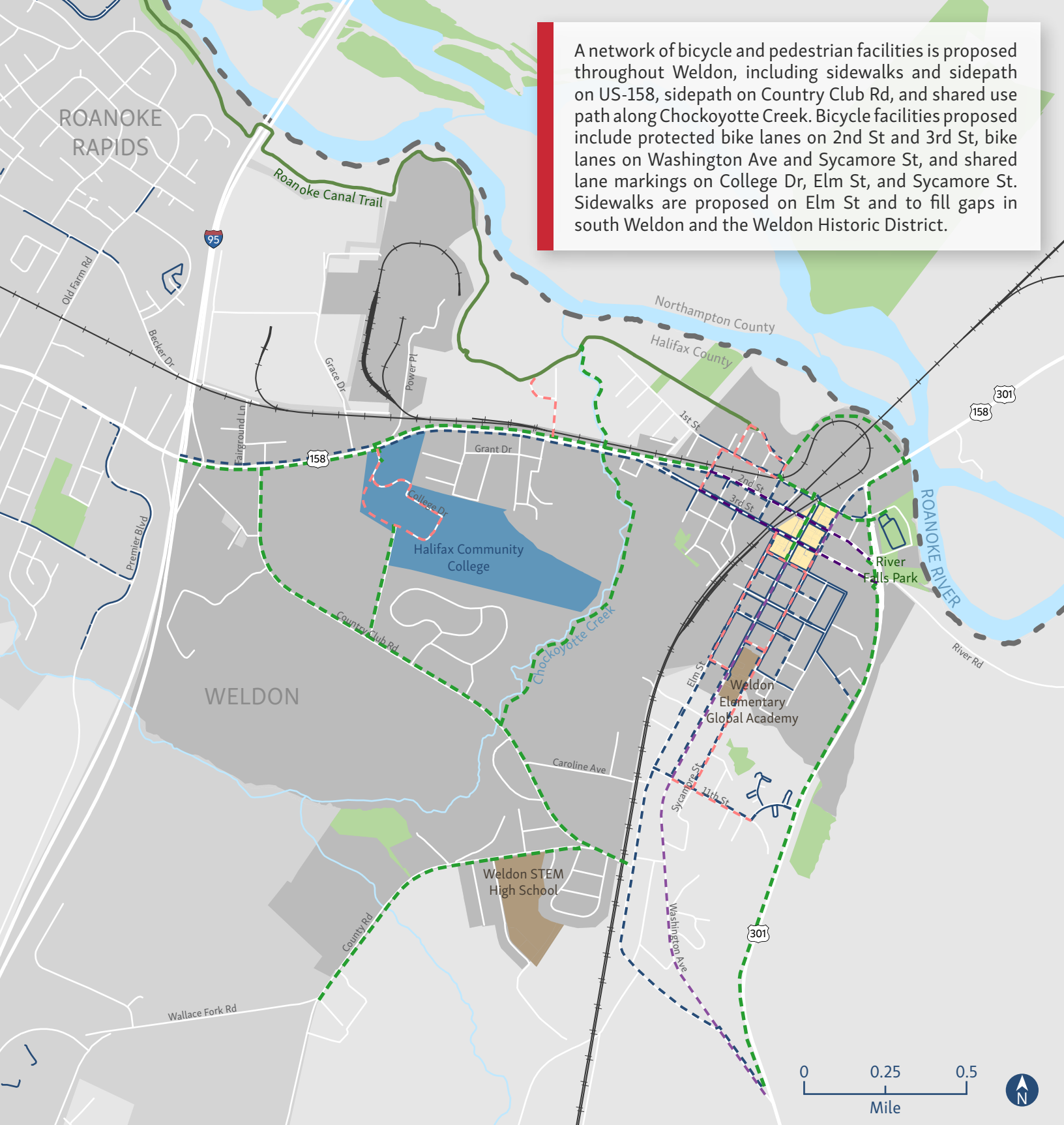
PROJECT	FROM	TO	TYPE	INTERSECTION + CROSSING TREATMENT
Sycamore St	E 8th St	E 11th St	Sidewalk	High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk
Sycamore St	E 3rd St	US-301	Sidewalk	High-Visibility Crosswalk
Maple St	E 9th St	E 7th St	Sidewalk	Crosswalk
Woodlawn Ave	Sunset Dr	End of road	Sidewalk	N/A
US-158, north side	HCC	Poplar St	Shared Use Path	Crosswalk, High-Visibility Crosswalk
US-158, south side	I-95	HCC	Shared Use Path	Crosswalk, High-Visibility Crosswalk
College Dr	Country Club Rd	HCC	Shared Use Path	High-Visibility Crosswalk
Chockoyotte Creek	Country Club Rd	Roanoke Canal Trail	Shared Use Path	N/A
Country Club Rd	US-158	Elm St	Shared Use Path	High-Visibility Crosswalk, Railroad Crossing
County Rd	Wallace Fork Rd	Country Club Rd	Shared Use Path	Crosswalk, High-Visibility Crosswalk
Roanoke Canal Trail	N Walnut St	River Falls Park	Shared Use Path	N/A
1st St	N Walnut St	River Falls Park	Shared Use Path	High-Visibility Crosswalk, Rectangular Rapid Flashing Beacon, Railroad Crossing
US-301	Washington Ave	Roanoke River	Shared Use Path	High-Visibility Crosswalk, Rectangular Rapid Flashing Beacon
Washington Ave	1st St	4th St	Shared Use Path	High-Visibility Crosswalk
College Dr	US-158	HCC	Shared Use Path	N/A
2nd St	Poplar St	US-301	Protected Bicycle Lane	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crossing, Rectangular Rapid Flashing Beacon, Through Bicycle Lane Marking, Railroad Crossing
3rd St	Poplar St	US-301	Protected Bicycle Lane	Crosswalk, High-Visibility Crosswalk, Rectangular Rapid Flashing Beacon, Through Bicycle Lane Marking, Railroad Crossing
Sycamore St	E 1st St	E 3rd St	Bicycle Lane	High-Visibility Crosswalk, Through Bicycle Lane Marking

PROJECT	FROM	TO	TYPE	INTERSECTION + CROSSING TREATMENT
Washington Ave	4th St	US-301	Bicycle Lane	Crosswalk, High-Visibility Crosswalk, Through Bicycle Lane Marking
College Dr	US-158	HCC	Shared Lane Markings	High-Visibility Crosswalk
Ponderosa Rd/ Aqueduct Rd	US-158	Roanoke Canal Trail	Shared Lane Markings	High-Visibility Crosswalk
Poplar St	W 1st St	W 3rd St	Shared Lane Markings	Crosswalk, Mid-Block Crosswalk, Rectangular Rapid Flashing Beacon
W 1st St	Poplar St	Walnut St	Shared Lane Markings	Crosswalk, Mid-Block Crosswalk, High-Visibility Crosswalk
Coward St	W 1st St	Roanoke Canal Trail	Shared Lane Markings	N/A
Water St/Mill St/N Walnut St	Roanoke Canal Trail	W 1st St	Shared Lane Markings	Crosswalk, High-Visibility Crosswalk
E 8th St	Washington Ave	Sycamore St	Shared Lane Markings	High-Visibility Crosswalk
W 9th St	Elm St	Washington St	Shared Lane Markings	High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk
4th St	Elm St	Sycamore St	Shared Lane Markings	Crosswalk, High-Visibility Crosswalk
Elm St	W 3rd St	W 9th St	Shared Lane Markings	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk
Sycamore St	E 3rd St	E 11th St	Shared Lane Markings	Crosswalk, High-Visibility Crosswalk, High-Visibility Mid-Block Crosswalk



Decorative signage at the US-301 entrance to Weldon

A network of bicycle and pedestrian facilities is proposed throughout Weldon, including sidewalks and sidepath on US-158, sidepath on Country Club Rd, and shared use path along Chockoyotte Creek. Bicycle facilities proposed include protected bike lanes on 2nd St and 3rd St, bike lanes on Washington Ave and Sycamore St, and shared lane markings on College Dr, Elm St, and Sycamore St. Sidewalks are proposed on Elm St and to fill gaps in south Weldon and the Weldon Historic District.



TOWN OF WELDON

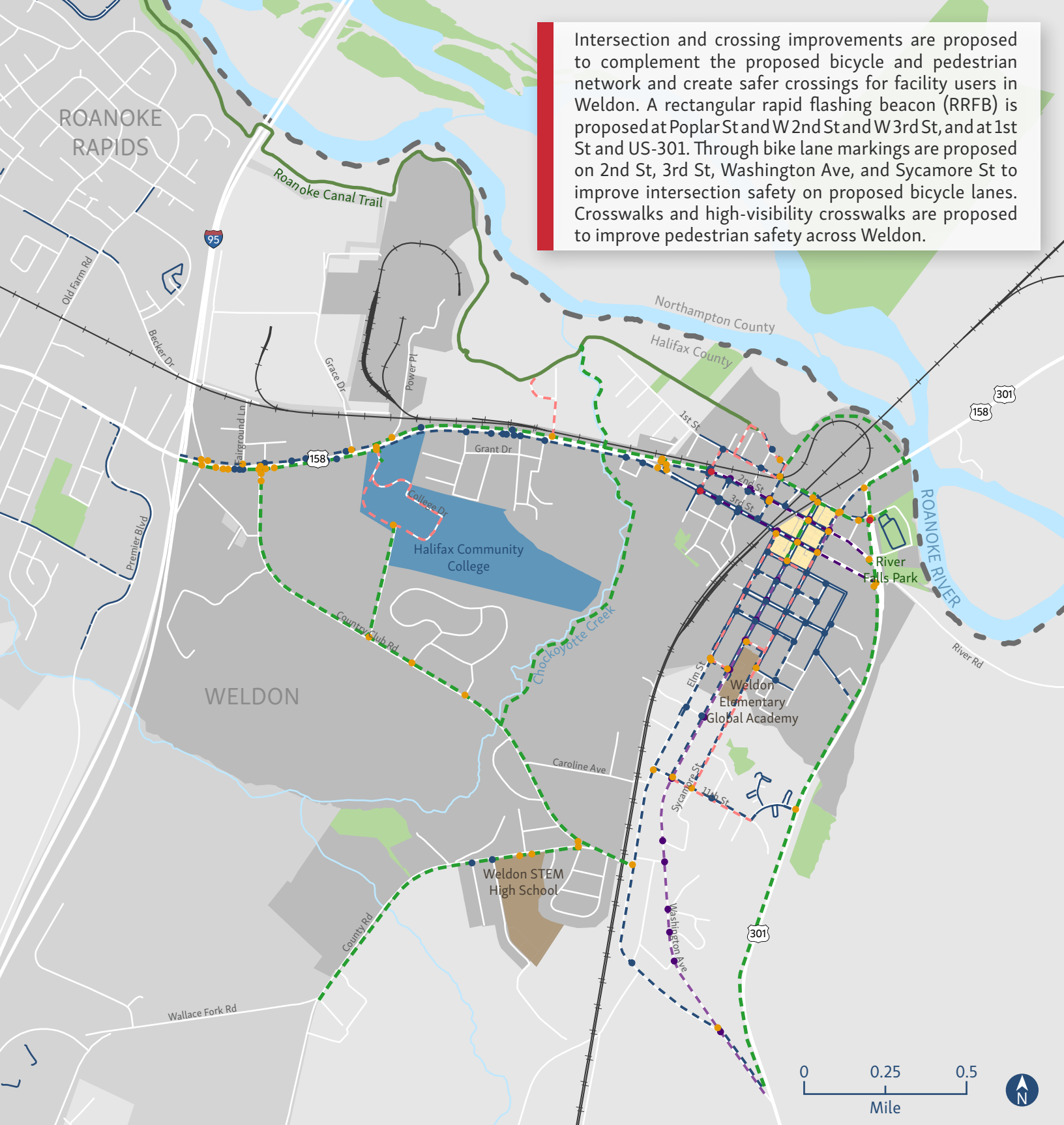
PROJECT ACCELERATION PLAN

Proposed Bicycle and Pedestrian Network

LEGEND ▶

- - - Proposed Shared Use Path
- + Railroad
- Existing Shared Use Path
- Roadway
- - - Proposed Sidewalk
- Downtown Weldon
- Existing Sidewalk
- School
- - - Proposed Protected Bike Lane
- College
- - - Proposed Bike Lane
- Park/Conserved Land
- - - Proposed Shared Lane Markings
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

Intersection and crossing improvements are proposed to complement the proposed bicycle and pedestrian network and create safer crossings for facility users in Weldon. A rectangular rapid flashing beacon (RRFB) is proposed at Poplar St and W 2nd St and W 3rd St, and at 1st St and US-301. Through bike lane markings are proposed on 2nd St, 3rd St, Washington Ave, and Sycamore St to improve intersection safety on proposed bicycle lanes. Crosswalks and high-visibility crosswalks are proposed to improve pedestrian safety across Weldon.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Proposed Intersection and Crossing Improvements

LEGEND ▶

- Proposed Shared Use Path
- Existing Shared Use Path
- Proposed Sidewalk
- Existing Sidewalk
- Proposed Protected Bike Lane
- Proposed Bike Lane
- Proposed Shared Lane Markings
- Proposed Crosswalk
- Proposed High-Visibility Crosswalk
- Proposed RRFB
- Proposed Through Bike Lane Markings
- + Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

PRIORITIZATION

METHODOLOGY + CRITERIA

Following the network recommendation phase, the project team reviewed each proposed project and scored them according to a set of eight criteria:

- Connects to Schools
- Connects to Parks and/or Recreation Centers
- Connects to Activity Centers
- Connects to Residential Areas
- Connects to Existing Bicycle or Pedestrian Facilities
- Improves Connections Across a Major Barrier
- Improves Areas with High Rates of Bicycle or Pedestrian Crashes
- Community Input

Prioritization criteria were selected based upon Town staff and Steering Committee feedback. Descriptions, ranking categories, and associated measurements and point scales are detailed in the following table.

CRITERIA	DESCRIPTION	RANK	MEASUREMENT	POINTS
Connects to Schools	Bicycle/Pedestrian corridor provides connections to schools or colleges.	High	Corridor provides a direct connection to a school or college.	10
		Medium	Corridor is within ¼ - ½ mile of a school or college.	5
		Low	Corridor does not connect to a school or college.	0
Connects to Parks and/or Recreation Centers	Bicycle/Pedestrian corridor provides connections to parks and/or recreation centers.	High	Corridor provides a direct connection to a park and/or recreation center.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Connects to Activity Centers: Future Mixed-Use, Central Business District, and/or General Commercial land uses	Bicycle/Pedestrian corridor provides connections to areas proposed as as Future Mixed-Use, Central Business District, and/or General Commercial land uses in the Town of Weldon Future Land Use Plan.	High	Corridor is within a ¼ mile of Future Mixed-Use, Central Business District, or General Commercial areas.	10
		Medium	Corridor is within a ½ mile of Future Mixed-Use, Central Business District, or General Commercial areas.	5
		Low	Corridor does not connect to an activity center.	0
Connects to Residential Areas	Bicycle/Pedestrian corridor provides connections to areas proposed as Medium Density Residential, Multi-Family, and/or Transitional land uses in the Town of Weldon Future Land Use Plan.	High	Corridor is within a ¼ mile of Medium Density, Multi-Family, or Transitional residential areas.	10
		Medium	Corridor is within a ½ mile of Medium Density, Multi-Family, or Transitional residential areas.	5
		Low	Corridor does not connect to a residential area.	0

CRITERIA	DESCRIPTION	RANK	MEASUREMENT	POINTS
Connects to Existing Bicycle or Pedestrian Facilities	Bicycle/Pedestrian corridor provides connections to existing bicycle or pedestrian facilities, including those of regional or statewide significance such as the Roanoke Canal Trail.	High	Corridor closes a gap between existing regional/statewide trails, such as the Roanoke Canal Trail.	10
		Medium	Corridor connects to existing bicycle or pedestrian facilities.	5
		Low	Corridor does not connect to existing bicycle or pedestrian facilities.	0
Provides Connections Across a Major Barrier	Bicycle/Pedestrian corridor provides connections across a major bicycle or pedestrian barrier, such as a railroad, major highway/interstate corridor, or river.	High	Corridor provides connections across a major barrier.	10
		Medium	Corridor connects to existing bicycle or pedestrian facilities that provide access across a major barrier.	5
		Low	Corridor does not provide access across a major barrier.	0
Improves Areas with High Rates of Bicycle or Pedestrian Crashes	Bicycle/Pedestrian corridor provides safety improvements in an area with a history of bicycle and pedestrian crashes.	High	Corridor has seen 4+ bicycle or pedestrian crashes.	10
		Medium	Corridor has seen 1-3 bicycle or pedestrian crashes.	5
		Low	Corridor has seen 0 bicycle or pedestrian crashes.	0
Community Input	Bicycle/Pedestrian corridor addresses community needs or concerns.	High	Town received 4+ public comments identifying needs or concerns along the corridor.	10
		Medium	Town received 1-3 public comments identifying needs or concerns along the corridor.	5
		Low	Town received 0 public comments identifying needs or concerns along the corridor.	0

PRIORITIZATION RESULTS

The results of the prioritization process are provided below. Several projects tied with the same score. From this list, the Town and Steering Committee decided to move forward with eight priority projects. Cut sheets are provided for these projects which are detailed later in this section.

RANK	PROJECT	FROM	TO	TYPE	SCORE
1 (tie)	US-158, south side	Halifax Community College (HCC)	Poplar St	Sidewalk	65
	Washington Ave	1st St	4th St	Shared Use Path	65
	2nd St	Poplar St	US-301	Protected Bike Lane	65
2 (tie)	1st St	Walnut St	River Falls Park	Shared Use Path	60
	1st St at Railroad Crossing	Washington Ave	Weldon Memorial Library	Sidewalk	60
	US-301	Washington Ave	Roanoke River	Shared Use Path	60
	Roanoke Canal Trail	Walnut St	US-301	Shared Use Path	60
	US-158, north side	HCC	Poplar St	Shared Use Path	60
	W 3rd St	W 2nd St	Elm St	Sidewalk	60
	Chockoyotte Creek	Country Club Rd	Roanoke Canal Trail	Shared Use Path	55
3 (tie)	US-158, south side	I-95	HCC	Shared Use Path	55
	Elm St/Old Halifax Rd	W 3rd St	Washington Ave	Sidewalk	55
	Washington Ave	9th St	11th St	Sidewalk	55
	Country Club Rd	US-158	Elm St	Shared Use Path	55
	3rd St	Poplar St	US-301	Protected Bike Lane	55
	Washington Ave	4th St	US-301	Bike Lane	55
	Sycamore St	E 3rd St	E 11th St	Shared Lane Markings	55
	College Dr	US-158	HCC	Shared Use Path	55
	US-158, north side	I-95	HCC	Sidewalk	55
	W 2nd St	S Walnut St	W 3rd St	Sidewalk	55
	College Dr	HCC	Country Club Rd	Shared Use Path	50
4 (tie)	S Walnut St	W 1st St	W 3rd St	Sidewalk	50
	Sycamore St	8th St	11th St	Sidewalk	50
	Sycamore St	E 3rd St	US-301	Sidewalk	50
	Sycamore St	E 1st St	E 3rd St	Bike Lane	50
	County Rd	Wallace Fork Rd	Country Club Rd	Shared Use Path	50

RANK	PROJECT	FROM	TO	TYPE	SCORE
5 (tie)	College Dr	US-158	HCC	Shared Lane Markings	45
	Water St/Mill St	Coward St	Walnut St	Sidewalk	45
	5th St	Elm St	Maple St	Sidewalk	45
	Water St/Mill St/ Walnut St	Coward St	W 1st St	Shared Lane Markings	45
	E 6th St	Sycamore St	Maple St	Sidewalk	45
	E 8th St	Washington Ave	Sycamore St	Sidewalk	45
	W 9th St	Elm St	Washington Ave	Sidewalk	45
	E 8th St	Washington Ave	Sycamore St	Shared Lane Markings	45
	W 9th St	Elm St	Washington Ave	Shared Lane Markings	45
	4th St	Elm St	Sycamore St	Shared Lane Markings	45
	11th St	Elm St	Dogwood Ln	Sidewalk	45
6 (tie)	Poplar St	W 1st St	W 3rd St	Sidewalk	40
	Elm St	W 3rd St	W 9th St	Shared Lane Markings	40
	7th St	Elm St	Maple St	Sidewalk	40
7 (tie)	Ponderosa Rd/ Aqueduct Rd	US-158	Roanoke Canal Trail	Shared Lane Markings	35
	W 1st St	Water St	Walnut St	Sidewalk	35
	Maple St	E 7th St	E 9th St	Sidewalk	35
	Woodlawn Ave	Sunset Dr	End of road	Sidewalk	35
	W 1st St	Poplar St	Walnut St	Shared Lane Markings	35
	Washington Ave	Elm St	US-301	Sidewalk	35
8 (tie)	Poplar St	W 1st St	W 3rd St	Shared Lane Markings	30
	Coward St	W 1st St	Water St	Shared Lane Markings	30
	Dogwood Dr	E 11th St	US-301	Sidewalk	30

PRIORITY PROJECTS

From the list of scored projects, eight were selected by Town staff for further study within this plan. On the following pages are cut sheets reviewing existing conditions, the proposed facility, and opportunities and constraints that may be associated with future implementation of the project. A project map, planning level cost estimate, and aerial or perspective rendering is provided with each project. The cut sheet projects represent projects that are high-priority and/or projects that could be completed in the near- to medium-term.

1. US-158 Sidepath and Sidewalks
2. Chockoyotte Creek Greenway
3. Roanoke Canal Trail
4. 2nd St Protected Bike Lane
5. Elm St Sidewalk Gaps
6. Country Club Blvd Sidepath
7. Washington Ave Bike Lane
8. Sycamore St Sidewalk Gaps

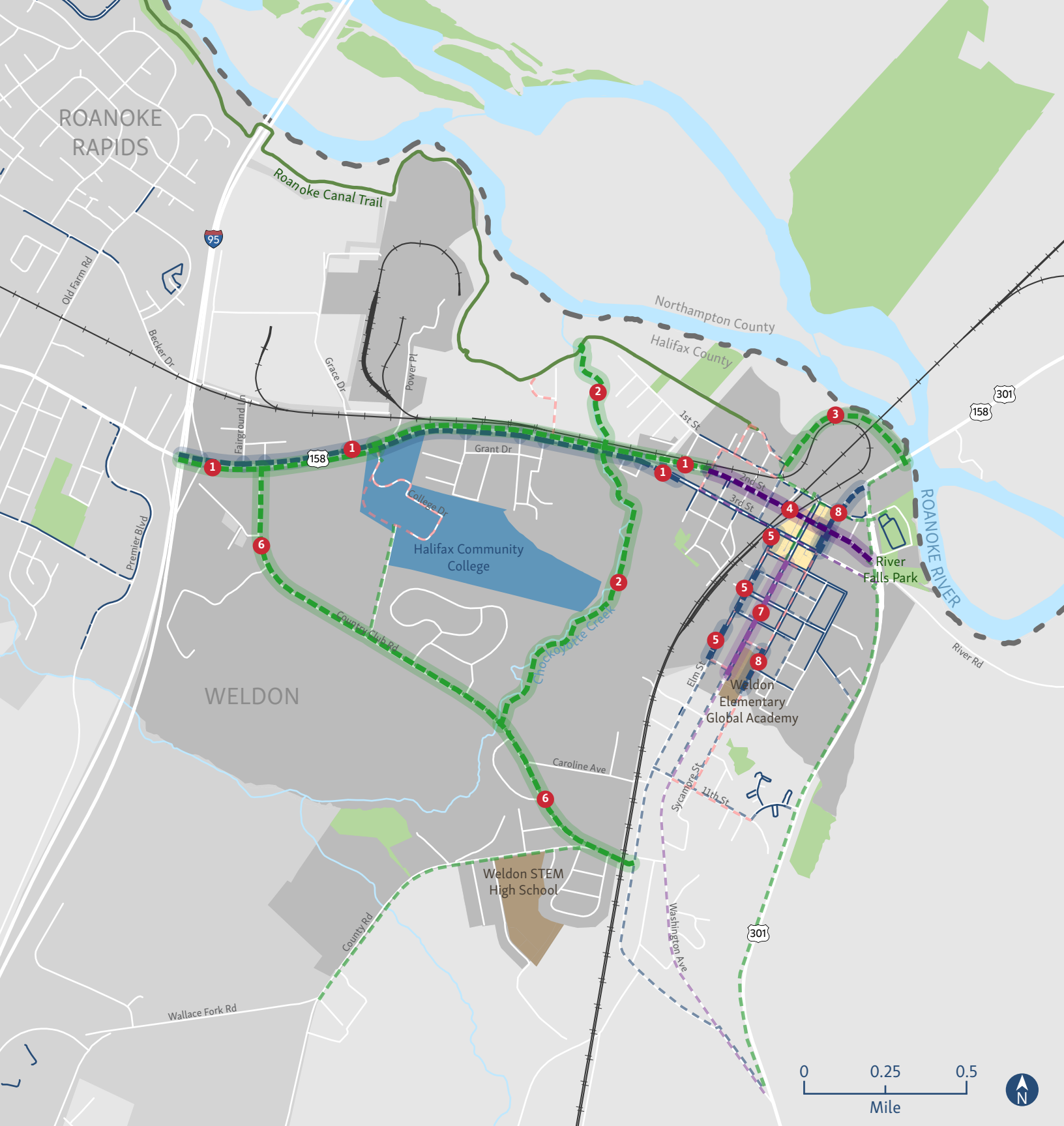
CUT SHEET CONSIDERATIONS:

Design + Cost Estimates

- Accessible curb ramps are required by the ADA at all crosswalks.
- Each rendering of the proposed facility is for illustrative and planning purposes only and does not reflect a final design based on an engineering analysis.
- Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

Typical Sections

Due to space constraints, some widths within the typical sections have been omitted. For additional information, please refer to the design considerations section for specific widths and details pertaining to each typical section.



TOWN OF WELDON

PROJECT ACCELERATION PLAN

Priority Bicycle and Pedestrian Projects

LEGEND ▶

- | | |
|-------------------------------|---------------------|
| Priority Project | Railroad |
| Priority Project ID # | Roadway |
| Proposed Shared Use Path | Downtown Weldon |
| Existing Shared Use Path | School |
| Proposed Sidewalk | College |
| Existing Sidewalk | Park/Conserved Land |
| Proposed Protected Bike Lane | Water |
| Proposed Bike Lane | Weldon Town Limits |
| Proposed Shared Lane Markings | Roanoke Rapids |
| | County Boundary |



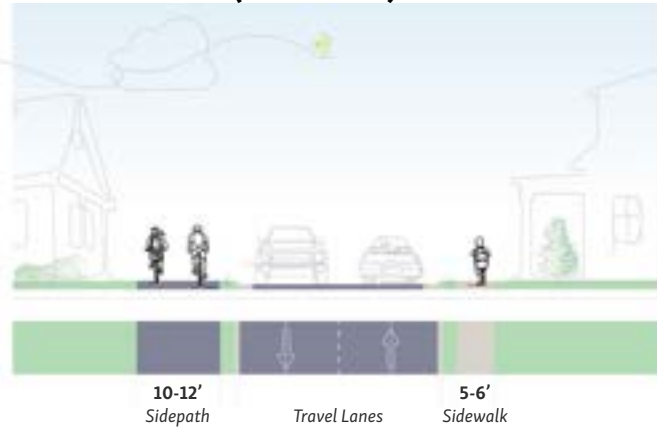
US-158 Sidepath and Sidewalks

US-158 is a priority transportation corridor that connects Weldon with Roanoke Rapids. Proposed multi-modal network recommendations include a sidepath along the south side between I-95 and College Dr and a sidepath along the north side between College Dr and Poplar St. Sidewalks are also recommended along the north side of US-158 between I-95 and College Dr and along the south side between College Dr and 3rd St. These facilities provide critical connections to Halifax Community College, several local businesses, and residential neighborhoods and will improve safety for bicyclists and pedestrians traveling along the corridor.

EXISTING CONDITIONS



TYPICAL SECTION (PROPOSED)



PROPOSED FACILITY RENDERING



PROJECT SNAPSHOT

LOCATION:

US-158 (both sides) from I-95 to Poplar St

TOTAL LENGTH:

3.1 Miles

FACILITY TYPE:

Sidepath and sidewalks

INTERSECTION TREATMENTS:

High-visibility crosswalk: W C Rivers Dr, Ponderosa Rd, Power Pl, Grace Dr, College Dr, Prince St, Country Club Rd, Fairground Ln, I-95, and select driveways

High-visibility mid-block crosswalk: near W C Rivers Dr

Crosswalks: at other streets and driveways

IMPLEMENTATION PARTNERS:

Town of Weldon, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, Halifax Community College

PLANNING LEVEL COST ESTIMATE:

Sidepath: \$4,606,419 // Sidewalk: \$1,793,005

Total Cost: \$6,399,424

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PRIORITIZATION FACTORS

Connects to Schools: **10; 10; 10; 10/10**

Connects to Parks and/or Recreation Centers: **5; 5; 5; 5/10**

Connects to Activity Centers: **10; 10; 10; 10/10**

Connects to Residential Areas: **10; 10; 10; 10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **0; 0; 0; 5/10**

Provides Connections Across a Major Barrier: **10; 10; 10; 10/10**

Improves Areas with High Rates of Bike/Ped Crashes: **5; 0; 0; 5/10**

Community Input: **10; 10; 10; 10/10**

TOTAL: 60; 55; 55; 65/80

DESIGN CONSIDERATIONS

Numerous driveways, intersecting streets, and slip lanes between I-95 and College Dr are safety hazards for pedestrians and bicyclists. To mitigate safety conflicts at street intersections and driveways along US-158, consider incorporating the following design strategies:

- Where possible, reduce driveway frequencies.
- Design or improve existing intersections to reduce driver speeds and heighten awareness of sidewalk users.
- Encourage lower speeds at sidewalk approaches by use of signage and traffic calming measures.
- Provide clear assignment of ROW with signs and markings. Consider using an R10-15 Right Turn Yield to Pedestrians sign at street crossings with right turn interactions.

ROW is constrained on the corridor and at the crossing of Chockoyotte Creek. A guardrail and downward slope present challenges for installing multimodal facilities adjacent to the roadway. A proposed greenway along Chockoyotte Creek could add a connection to or require a mid-block crossing of US-158.

ROW constraints due to narrow building setbacks are present along the south side of W 3rd St, and landscaping behind the curb may be impacted by sidewalk installation.

When crossing US-158 at Country Club Rd, College Dr, and W C Rivers Dr, pedestrian refuge islands should be considered in the design of the crosswalk to reduce crossing distances and enhance pedestrian safety.

Between Garris St and Brownell St, space is constrained on the south side of the roadway by utilities, signage, and parking. On the north side of the roadway, sidepath installation may require the relocation of utilities and billboards.

On the south side of the roadway between College Dr and Garris St, a small slope exists. Impacts to vegetation, drainage, and utilities may be necessary for sidewalk construction.

Install high-visibility crosswalks with pedestrian signal heads and signage at signalized intersections with I-95, Country Club Rd, Grace Dr, and College Dr to ensure visibility of the sidewalk crossing for all roadway users. High-visibility crosswalk markings are also recommended for unsignalized intersections with Fairground Ln, Power Pl, Ponderosa Rd, and W C Rivers Dr, as well as major businesses.

At College Dr, there is an opportunity to create a pedestrian link to a walking track and fitness equipment, which would require coordination with Halifax Community College. A proposed high-visibility crosswalk at College Dr may also impact the entrance and signage, requiring further coordination.

Project Segment Key

- 1 Sidepath (north side, east of HCC)
- 2 Sidewalk (north side, west of HCC)
- 3 Sidepath (south side, west of HCC)
- 4 Sidewalk (south side, east of HCC)



Chockoyotte Creek Greenway

The Chockoyotte Creek Greenway is proposed along the east side of Chockoyotte Creek from the Roanoke Canal Trail to Country Club Rd. The facility will serve as the central north-south greenway spine and expand recreational activities for the Town of Weldon. Enhancing the community’s multi-modal accessibility, the proposed greenway will also connect with the existing Roanoke Canal Trail and proposed sidepaths along US-158 and Country Club Rd.

EXISTING CONDITIONS



PROJECT SNAPSHOT

LOCATION:

Country Club Rd to Roanoke Canal Trail

TOTAL LENGTH:

1.4 Miles

FACILITY TYPE:

Shared-Use Path

INTERSECTION TREATMENTS:

N/A

IMPLEMENTATION PARTNERS:

Town of Weldon, Roanoke Canal Commission, Roanoke Canal Museum and Trail, Halifax County, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, CSX, Weldon in Action, Halifax Community College

PLANNING LEVEL COST ESTIMATE:

\$3,957,730

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

TYPICAL SECTION (PROPOSED)



10-12'
Shared-Use Path

PRIORITIZATION FACTORS

Connects to Schools: **5/10**

Connects to Parks and/or Recreation Centers: **0/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **10/10**

Provides Connections Across a Major Barrier: **10/10**

Improves Areas with High Rates of Bike/Ped Crashes: **0/10**

Community Input: **10/10**

TOTAL: 55/80

PROPOSED FACILITY RENDERING



DESIGN CONSIDERATIONS

The proposed sidepath and sidewalks along US-158 will provide connections between the greenway and the multi-modal transportation network and may serve as a temporary endpoint for the greenway corridor if implementation is phased. Additional analysis via a feasibility study may be required to determine the extent of available space beneath the existing roadway culvert and railroad bridge to accommodate a grade-separated crossing.

North of US-158, the banks of Chockoyotte Creek narrow and become more steep, rising approximately 40 feet on either side. This reduces the space available for greenway development and makes construction more challenging and costly. Connection to the Roanoke Canal Trail may require climbing this grade, potentially increasing construction costs and easement needs.

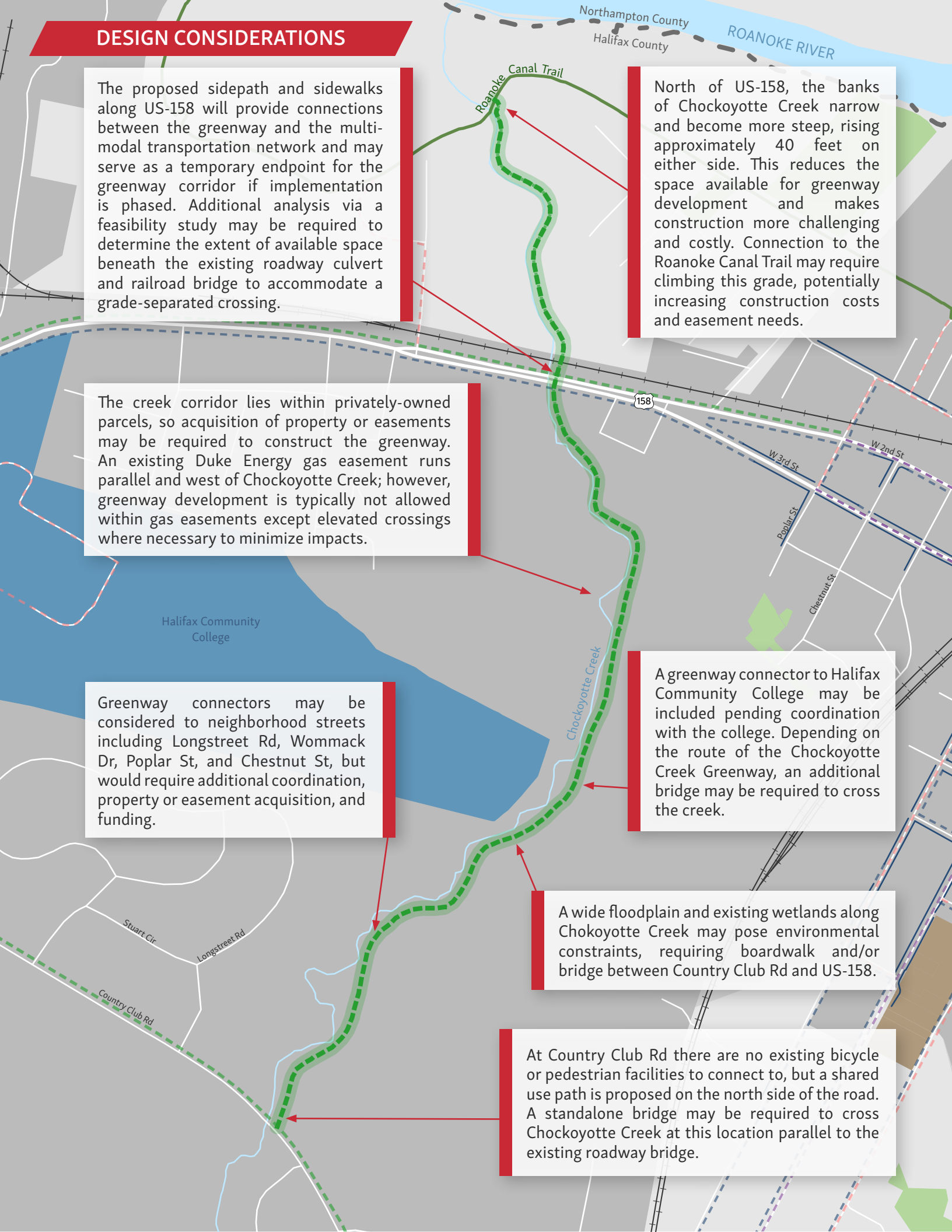
The creek corridor lies within privately-owned parcels, so acquisition of property or easements may be required to construct the greenway. An existing Duke Energy gas easement runs parallel and west of Chockoyotte Creek; however, greenway development is typically not allowed within gas easements except elevated crossings where necessary to minimize impacts.

Greenway connectors may be considered to neighborhood streets including Longstreet Rd, Wommack Dr, Poplar St, and Chestnut St, but would require additional coordination, property or easement acquisition, and funding.

A greenway connector to Halifax Community College may be included pending coordination with the college. Depending on the route of the Chockoyotte Creek Greenway, an additional bridge may be required to cross the creek.

A wide floodplain and existing wetlands along Chokoyotte Creek may pose environmental constraints, requiring boardwalk and/or bridge between Country Club Rd and US-158.

At Country Club Rd there are no existing bicycle or pedestrian facilities to connect to, but a shared use path is proposed on the north side of the road. A standalone bridge may be required to cross Chockoyotte Creek at this location parallel to the existing roadway bridge.





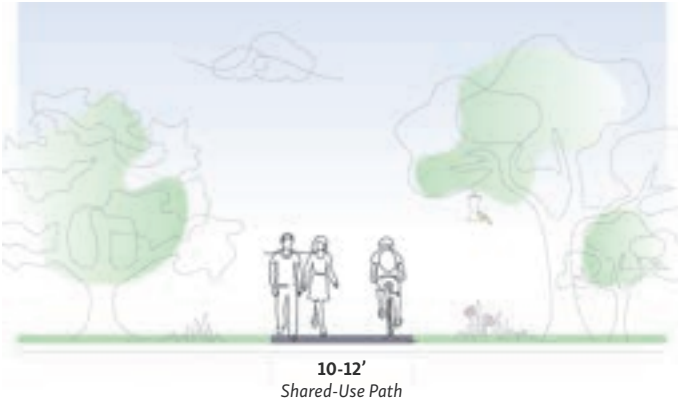
Roanoke Canal Trail

A new alignment of the Roanoke Canal Trail would provide access to the Roanoke River between N Walnut St and US-301, bypassing two at-grade railroad crossings on 1st St and creating a safe crossing of US-301. This project aims to provide a more scenic and accessible loop between the Roanoke Canal Trail, the Roanoke River, River Falls Park, and E 1st St.

EXISTING CONDITIONS



TYPICAL SECTION (PROPOSED)



PROPOSED FACILITY RENDERING



PROJECT SNAPSHOT

LOCATION:

N Walnut St to River Falls Park

TOTAL LENGTH:

0.5 Miles

FACILITY TYPE:

Shared-Use Path

INTERSECTION TREATMENTS:

N/A

IMPLEMENTATION PARTNERS:

Town of Weldon, Roanoke Canal Commission, Roanoke Canal Museum and Trail, Peanut Belt RPO, NC Wildlife Resources Commission, NCDOT Division 4, NCDOT IMD, NCDOT Rail Division, CSX, Weldon in Action

PLANNING LEVEL COST ESTIMATE:

\$1,413,995

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PRIORITIZATION FACTORS

Connects to Schools: **0/10**

Connects to Parks and/or Recreation Centers: **10/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **10/10**

Provides Connections Across a Major Barrier: **10/10**

Improves Areas with High Rates of Bike/Ped Crashes: **0/10**

Community Input: **10/10**

TOTAL: 60/80

DESIGN CONSIDERATIONS

Much of the area north of 1st St in Weldon falls within the 500-year floodplain, and areas between the CSX railroad loop and the Roanoke River fall within the base flood elevation, or 100-year floodplain, and floodway zones. Additionally, wetlands are prevalent west of the railroad loop. Development is limited in these areas and greenway construction may be more expensive due to requirements for boardwalk structure, environmental permitting, and flood studies.

The railroad corridor and adjacent parcels are owned by CSX. Extensive coordination with the railroad would be required to acquire an easement for greenway construction, or to purchase property. Based on current rail policy, CSX does not permit private or public parallel bicycle and pedestrian paths within the railroad's right-of-way.

An existing railroad bridge and a partially removed railroad bridge across the Roanoke River cross the proposed Roanoke Canal Trail. Coordination with CSX will be necessary to avoid impacts to the structures and ensure safety for trail users.

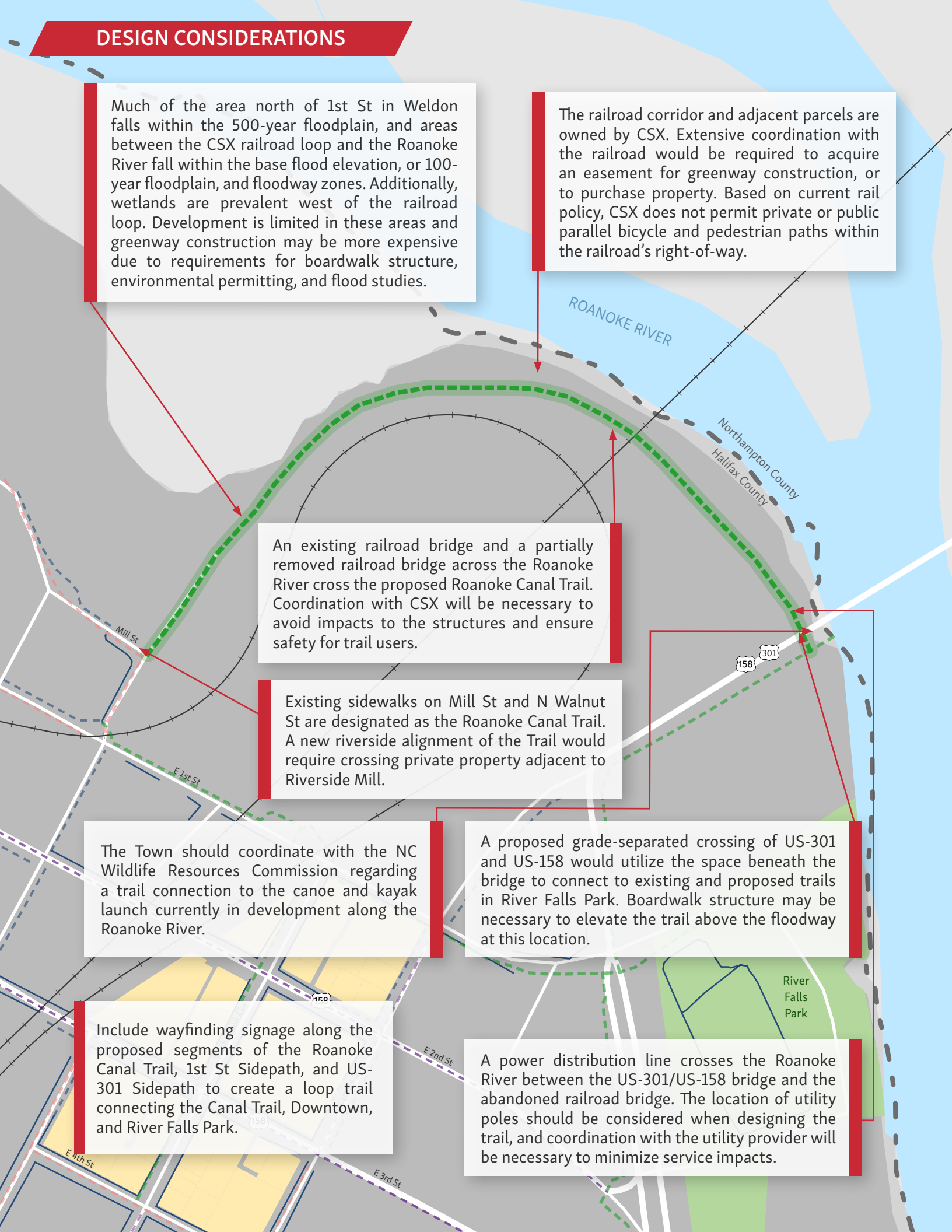
Existing sidewalks on Mill St and N Walnut St are designated as the Roanoke Canal Trail. A new riverside alignment of the Trail would require crossing private property adjacent to Riverside Mill.

The Town should coordinate with the NC Wildlife Resources Commission regarding a trail connection to the canoe and kayak launch currently in development along the Roanoke River.

A proposed grade-separated crossing of US-301 and US-158 would utilize the space beneath the bridge to connect to existing and proposed trails in River Falls Park. Boardwalk structure may be necessary to elevate the trail above the floodway at this location.

Include wayfinding signage along the proposed segments of the Roanoke Canal Trail, 1st St Sidepath, and US-301 Sidepath to create a loop trail connecting the Canal Trail, Downtown, and River Falls Park.

A power distribution line crosses the Roanoke River between the US-301/US-158 bridge and the abandoned railroad bridge. The location of utility poles should be considered when designing the trail, and coordination with the utility provider will be necessary to minimize service impacts.





2nd St Protected Bike Lane

A protected bicycle lane is proposed along 2nd St from Poplar St to US-301. 2nd St is a priority bicycle corridor as it provides connections between Downtown and adjacent residential neighborhoods in Weldon and would serve as a safe multi-modal connection between River Falls Park and proposed bicycle and pedestrian facilities along US-158. The proposed protected bicycle lane will also incorporate roadway improvements to improve safety for all roadway users.

EXISTING CONDITIONS



PROJECT SNAPSHOT

LOCATION:

2nd St between Poplar St and US 301

TOTAL LENGTH:

0.55 Miles

FACILITY TYPE:

Protected Bike Lane

INTERSECTION TREATMENTS:

Rectangular rapid flashing beacon (RRFB): Poplar St

Through bicycle lane markings: Intersections of W 2nd St and Poplar St, Chestnut St, Mulberry St, S Walnut St, Washington Ave, and Sycamore St

IMPLEMENTATION PARTNERS:

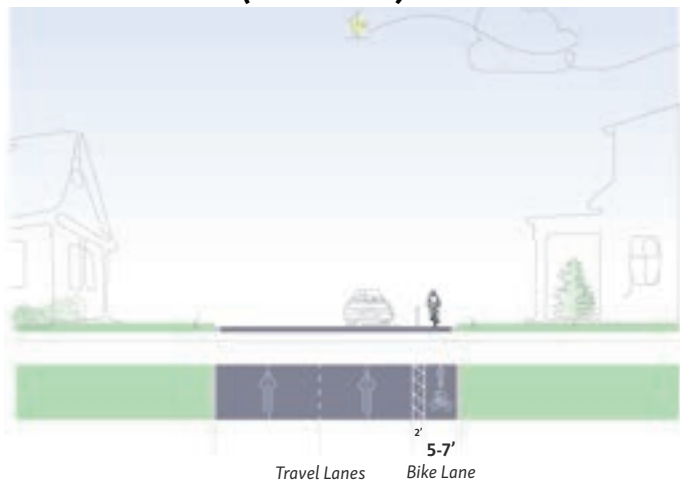
Town of Weldon, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, CSX

PLANNING LEVEL COST ESTIMATE:

\$787,172

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

TYPICAL SECTION (PROPOSED)



PRIORITIZATION FACTORS

Connects to Schools: **5/10**

Connects to Parks and/or Recreation Centers: **10/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **5/10**

Provides Connections Across a Major Barrier: **10/10**

Improves Areas with High Rates of Bike/Ped Crashes: **5/10**

Community Input: **10/10**

TOTAL: 65/80

PROPOSED FACILITY RENDERING



DESIGN CONSIDERATIONS

2nd St is approximately 45 feet wide, which encourages speeding and poses serious safety concerns for both bicyclists and pedestrians. There is ample available space within the roadway width to incorporate a 6-7 feet bicycle lane protected by a minimum 3-foot buffer, while retaining two vehicle travel lanes that are at 12+ feet in width to accommodate existing truck traffic.

Driveways and intersecting streets on either side of the roadway create many potential points of conflict between bicyclists and vehicles. Maintaining visibility is critical to ensure safety, and NACTO recommends the following strategies:

- If on-street parking is provided between the bike lane and travel lanes, prohibit parking within 30 ft of the crossing on either side.
- At side streets, provide a sight triangle of 20 ft to the bicycle lane. At driveway crossings, 10 ft is acceptable.
- Add yield lines, "Yield to Bicycles" signage, and colored pavement to increase visibility of the bicycle lane and establish priority of bicyclists over intersecting traffic.

A rectangular rapid flashing beacon (RRFB) is proposed at Poplar St to improve safety of bicyclists and pedestrians crossing W 2nd St. At this intersection, design should consider how users of the bicycle lane would transition to the proposed sidepath that continues west of Poplar St. To reduce the need to cross the roadway for bicyclists traveling west, consider placing the bicycle lane on the north or right side of the road.

At the railroad crossings between Elm St and Washington Ave, the travel lanes temporarily split to accommodate a bridge support. The roadway is narrower and the buffer width between the bicycle lane and the travel lane may need to be reduced. The at-grade rail crossing may require additional treatment. A crossing angle of between 60 and 120 degrees is desired between the bicycle lane and railroad tracks, and modifications to the signal and mast arm system may be necessary.

Currently, there is angled parking on the south side of 2nd St between Washington Ave and Sycamore St. When implementing a one-way protected bicycle lane, the existing parking spaces may serve as a buffer between the bicycle lane and travel lanes. This approach should be limited near intersections to ensure the visibility and safety of bicyclists.



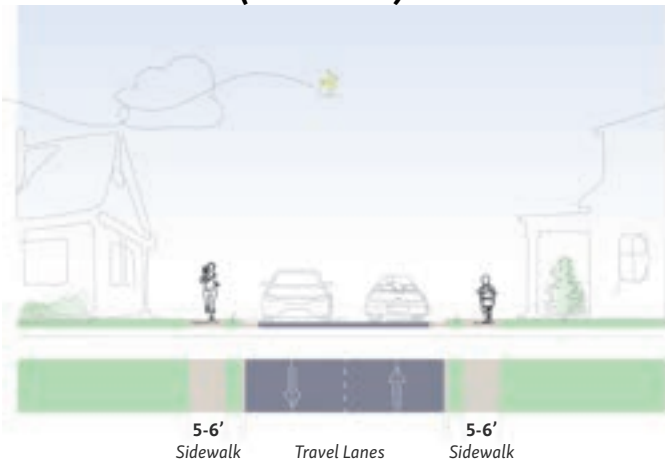
Elm St Sidewalk Gaps

A key priority for the Town of Weldon is to close sidewalk gaps in the Historic District, ensuring pedestrians have a safe and comfortable route to reach destinations in the neighborhood. To achieve this, sidewalk installation and crossing improvements are proposed in several key areas on Elm St, including on the east side between W 5th St and W 7th St, as well as between W 3rd St and W 4th St. Additionally, sidewalks and crossing improvements are proposed on the west side of Elm St between W 7th St and W 9th St to improve safety and accessibility in an area with a history of pedestrian crashes.

EXISTING CONDITIONS



TYPICAL SECTION (PROPOSED)



PROJECT SNAPSHOT

LOCATION:

Elm St (east side) between 3rd St and 4th St, and between 5th St and 7th St; Elm St (west side) between 7th St and 9th St

TOTAL LENGTH:

0.21 Miles

FACILITY TYPE:

Sidewalk

INTERSECTION TREATMENTS:

High-visibility mid-block crosswalk: W 9th St

High-visibility crosswalks: W 3rd St and W 9th St

Crosswalks: W 4th St, W 5th St, W 6th St, and W 7th St

IMPLEMENTATION PARTNERS:

Town of Weldon, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD

PLANNING LEVEL COST ESTIMATE:

\$225,947

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PROPOSED FACILITY RENDERING



PRIORITIZATION FACTORS

Connects to Schools: **5/10**

Connects to Parks and/or Recreation Centers: **5/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **5/10**

Provides Connections Across a Major Barrier: **5/10**

Improves Areas with High Rates of Bike/Ped Crashes: **10/10**

Community Input: **5/10**

TOTAL: 55/80

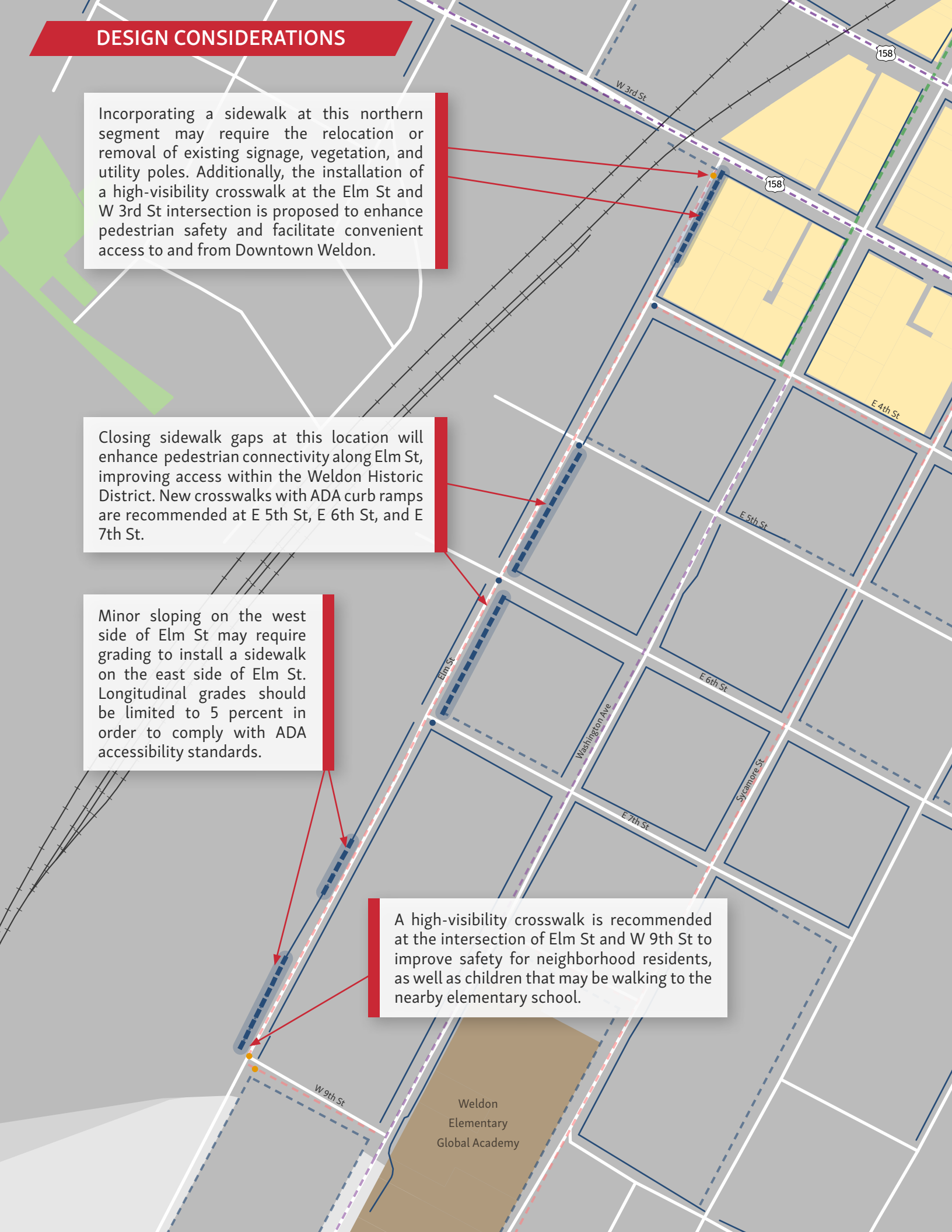
DESIGN CONSIDERATIONS

Incorporating a sidewalk at this northern segment may require the relocation or removal of existing signage, vegetation, and utility poles. Additionally, the installation of a high-visibility crosswalk at the Elm St and W 3rd St intersection is proposed to enhance pedestrian safety and facilitate convenient access to and from Downtown Weldon.

Closing sidewalk gaps at this location will enhance pedestrian connectivity along Elm St, improving access within the Weldon Historic District. New crosswalks with ADA curb ramps are recommended at E 5th St, E 6th St, and E 7th St.

Minor sloping on the west side of Elm St may require grading to install a sidewalk on the east side of Elm St. Longitudinal grades should be limited to 5 percent in order to comply with ADA accessibility standards.

A high-visibility crosswalk is recommended at the intersection of Elm St and W 9th St to improve safety for neighborhood residents, as well as children that may be walking to the nearby elementary school.





Country Club Rd Sidepath

A proposed sidepath along the north side of Country Club Rd aims to enhance the safety of both bicyclists and pedestrians in Weldon. This project will connect with the proposed shared-use paths on County Rd, College Dr, US-158, and Chockoyotte Creek, as well as planned sidewalks on Elm St. Furthermore, it will improve multimodal access to Halifax Community College, Chockoyotte Country Club, and businesses located near US-158.

EXISTING CONDITIONS



PROJECT SNAPSHOT

LOCATION:

Country Club Rd (north side) between US 158 and Elm St

TOTAL LENGTH:

1.75 Miles

FACILITY TYPE:

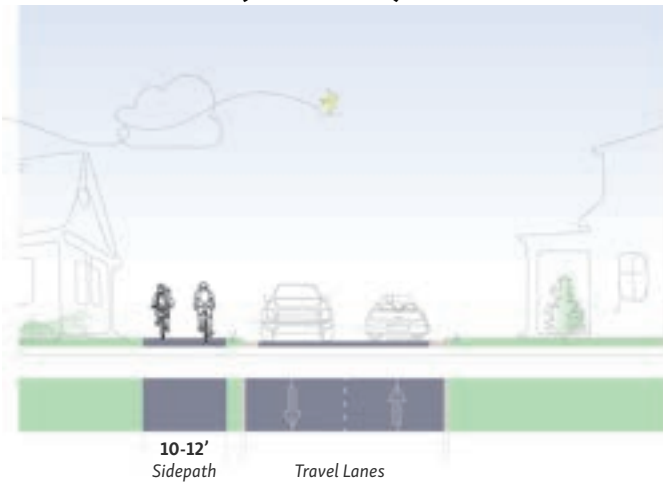
Shared-Use Path

INTERSECTION TREATMENTS:

High-visibility crosswalk: US-158, College Dr, Stuart Cir, County Rd, and BP gas station

High-visibility mid-block crosswalk: Elm St

TYPICAL SECTION (PROPOSED)



IMPLEMENTATION PARTNERS:

Town of Weldon, Halifax County, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, CSX

PLANNING LEVEL COST ESTIMATE:

\$4,941,696

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PRIORITIZATION FACTORS

Connects to Schools: **5/10**

Connects to Parks and/or Recreation Centers: **5/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **0/10**

Provides Connections Across a Major Barrier: **10/10**

Improves Areas with High Rates of Bike/Ped Crashes: **5/10**

Community Input: **10/10**

TOTAL: 55/80

PROPOSED FACILITY RENDERING



DESIGN CONSIDERATIONS

High-visibility crosswalks are proposed on all sides of the intersection with US-158, improving multimodal connectivity. Design should consider the addition of a pedestrian refuge on the west leg of the intersection to reduce crossing distances.

A pedestrian fatality occurred near the intersection of Hampton Farm Rd. When designing the sidepath, it is critical to take into account the visibility of sidepath users and maintain appropriate sight distances for drivers navigating the curve at this location.

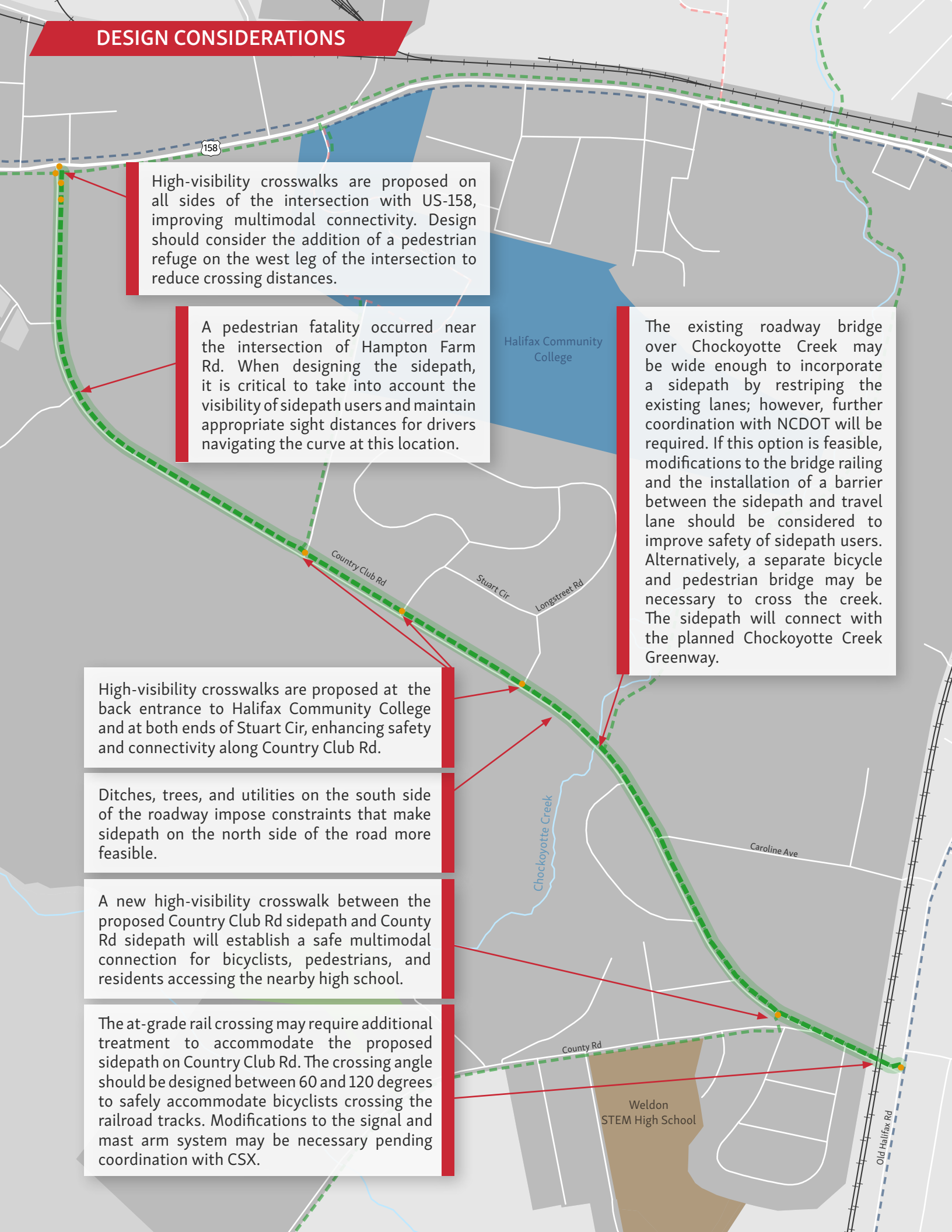
The existing roadway bridge over Chockoyotte Creek may be wide enough to incorporate a sidepath by restriping the existing lanes; however, further coordination with NCDOT will be required. If this option is feasible, modifications to the bridge railing and the installation of a barrier between the sidepath and travel lane should be considered to improve safety of sidepath users. Alternatively, a separate bicycle and pedestrian bridge may be necessary to cross the creek. The sidepath will connect with the planned Chockoyotte Creek Greenway.

High-visibility crosswalks are proposed at the back entrance to Halifax Community College and at both ends of Stuart Cir, enhancing safety and connectivity along Country Club Rd.

Ditches, trees, and utilities on the south side of the roadway impose constraints that make sidepath on the north side of the road more feasible.

A new high-visibility crosswalk between the proposed Country Club Rd sidepath and County Rd sidepath will establish a safe multimodal connection for bicyclists, pedestrians, and residents accessing the nearby high school.

The at-grade rail crossing may require additional treatment to accommodate the proposed sidepath on Country Club Rd. The crossing angle should be designed between 60 and 120 degrees to safely accommodate bicyclists crossing the railroad tracks. Modifications to the signal and mast arm system may be necessary pending coordination with CSX.





Washington Ave Bike Lane

A bicycle lane is proposed along Washington Ave between 4th St and 9th St. This facility will integrate with other planned bicycle facilities on 4th St, 8th St, and 9th St and a sidepath on Washington Ave north of 4th St. This project is designed to enhance bicyclist safety in Weldon and establish a vital link connecting residents of the Weldon Historic District with downtown and Weldon Elementary Global Academy.

EXISTING CONDITIONS



TYPICAL SECTION (PROPOSED)



5-6' Sidewalk 5-7' Bike Lane Travel Lanes 5-7' Bike Lane 5-6' Sidewalk

PROPOSED FACILITY RENDERING



PROJECT SNAPSHOT

LOCATION:

Washington Ave between 4th St and municipal boundary

TOTAL LENGTH:

0.37 Miles

FACILITY TYPE:

Bike Lane

INTERSECTION TREATMENTS:

High-visibility crosswalks: 4th St, 8th St, and W 9th St

Crosswalks: 4th St, 5th St, 6th St, and 7th St

Through bicycle lane markings: 4th St, 5th St, 6th St, 7th St, 8th St, and 9th St

IMPLEMENTATION PARTNERS:

Town of Weldon, Weldon City Schools, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD

PLANNING LEVEL COST ESTIMATE:

\$1,005,832

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PRIORITIZATION FACTORS

Connects to Schools: **10/10**

Connects to Parks and/or Recreation Centers: **5/10**

Connects to Activity Centers: **10/10**

Connects to Residential Areas: **10/10**

Connects to Existing Bicycle or Pedestrian Facilities: **5/10**

Provides Connections Across a Major Barrier: **5/10**

Improves Areas with High Rates of Bike/Ped Crashes: **0/10**

Community Input: **10/10**

TOTAL: 55/80

DESIGN CONSIDERATIONS

Consider bike through lane markings and intersection treatments that designate where the bike lane or bicyclists cross the unsignalized intersection or stage for entering the intersection, ensuring a seamless connection between the proposed sidepath along Washington Ave and the bike lanes at E 4th St. These markings may be supplemented with green-colored pavement to highlight where bicyclists cross vehicle paths through the intersection, in addition to supplemental signage to identify where the bicycle facility crosses the intersection. Consider adding additional stop signs to slow vehicular traffic.

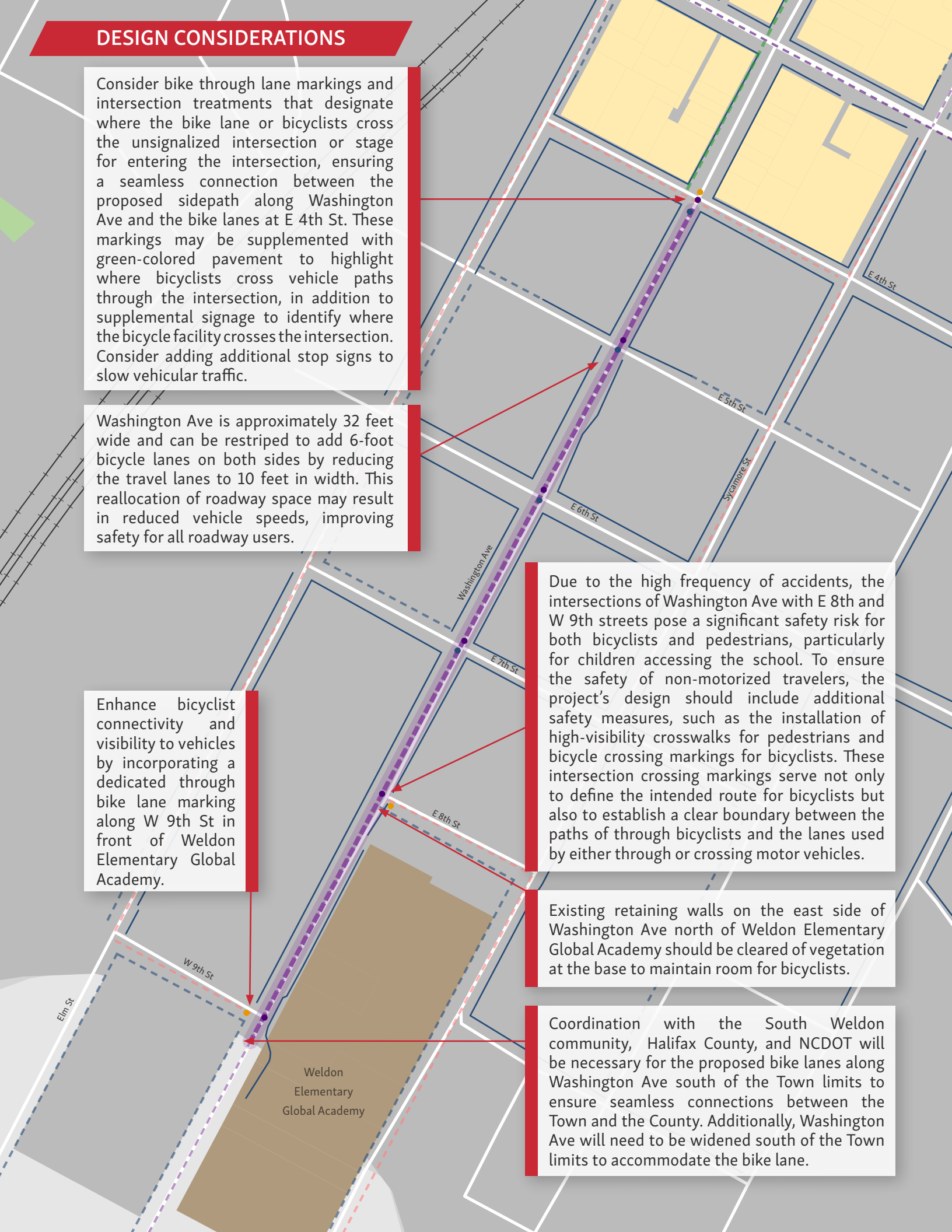
Washington Ave is approximately 32 feet wide and can be restriped to add 6-foot bicycle lanes on both sides by reducing the travel lanes to 10 feet in width. This reallocation of roadway space may result in reduced vehicle speeds, improving safety for all roadway users.

Enhance bicyclist connectivity and visibility to vehicles by incorporating a dedicated through bike lane marking along W 9th St in front of Weldon Elementary Global Academy.

Due to the high frequency of accidents, the intersections of Washington Ave with E 8th and W 9th streets pose a significant safety risk for both bicyclists and pedestrians, particularly for children accessing the school. To ensure the safety of non-motorized travelers, the project's design should include additional safety measures, such as the installation of high-visibility crosswalks for pedestrians and bicycle crossing markings for bicyclists. These intersection crossing markings serve not only to define the intended route for bicyclists but also to establish a clear boundary between the paths of through bicyclists and the lanes used by either through or crossing motor vehicles.

Existing retaining walls on the east side of Washington Ave north of Weldon Elementary Global Academy should be cleared of vegetation at the base to maintain room for bicyclists.

Coordination with the South Weldon community, Halifax County, and NCDOT will be necessary for the proposed bike lanes along Washington Ave south of the Town limits to ensure seamless connections between the Town and the County. Additionally, Washington Ave will need to be widened south of the Town limits to accommodate the bike lane.





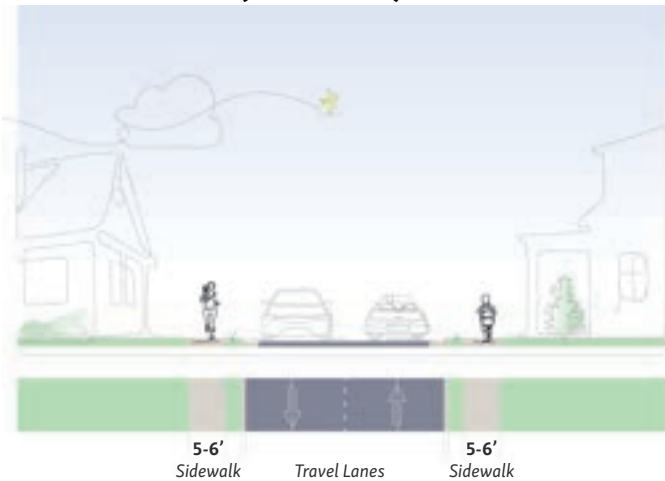
Sycamore St Sidewalk Gaps

Filling sidewalk gaps on Sycamore St is a priority project for Weldon to improve pedestrian safety and connectivity. Closing a gap on the west side of the street between 8th St and the municipal boundary would enhance connectivity to Weldon Elementary Global Academy, while sidewalks proposed between US-301 and 3rd St would fill gaps in the downtown pedestrian network and connect to the existing Roanoke Canal Trail on 1st St.

EXISTING CONDITIONS



TYPICAL SECTION (PROPOSED)



PROPOSED FACILITY RENDERING



PROJECT SNAPSHOT

LOCATION:

Sycamore St (west side) between 8th St and municipal boundary; Sycamore St between 3rd St and US-301

TOTAL LENGTH:

0.4 Miles

FACILITY TYPE:

Sidewalk

INTERSECTION TREATMENTS:

High-visibility mid-block crosswalk: E 9th St

High-visibility crosswalk: E 3rd St, E 2nd St, E 1st St, and US-301

IMPLEMENTATION PARTNERS:

Town of Weldon, Weldon City Schools, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD

PLANNING LEVEL COST ESTIMATE:

\$517,493

Planning level cost estimates were developed using the NCDOT Bicycle & Pedestrian Cost Estimator Tool, which determines facility cost estimates based on the following factors: project type, project length, facility width, construction market region, and existing roadway characteristics. The estimates are provided in 2019 dollars and were escalated to 2023 dollars. Estimates do not include costs of ROW acquisition and design.

PRIORITIZATION FACTORS

Connects to Schools: **5**; 10/10

Connects to Parks and/or Recreation Centers: **5**; 5/10

Connects to Activity Centers: **10**; 10/10

Connects to Residential Areas: **10**; 10/10

Connects to Existing Bicycle or Pedestrian Facilities: **5**; 5/10

Provides Connections Across a Major Barrier: **10**; 0/10

Improves Areas with High Rates of Bike/Ped Crashes: **0**; 5/10

Community Input: **5**; 5/10

TOTAL: 50; 50/80

DESIGN CONSIDERATIONS

Utility poles, fencing, and signage adjacent to the roadway limit the space available for sidewalk installation in downtown Weldon.

Sidewalk is proposed to connect to existing sidewalks on the north side of E 2nd St, and may require removal or relocation of existing utility poles and signage.

High-visibility crosswalks with pedestrian signal heads are recommended at each intersection of Sycamore St between E 1st St and E 3rd St in Downtown Weldon. In addition, a high-visibility crosswalk is recommended at US-301 to connect to the proposed Roanoke Canal Trail.

Adding sidewalks in a historic district can be challenging due to considerations such as space limitations, accessibility, costs, impacts to historic features, and balancing potential safety concerns. Installation of sidewalks may require coordination with the Weldon Historic Preservation Commission and state preservation agencies to minimize impacts to historic features.

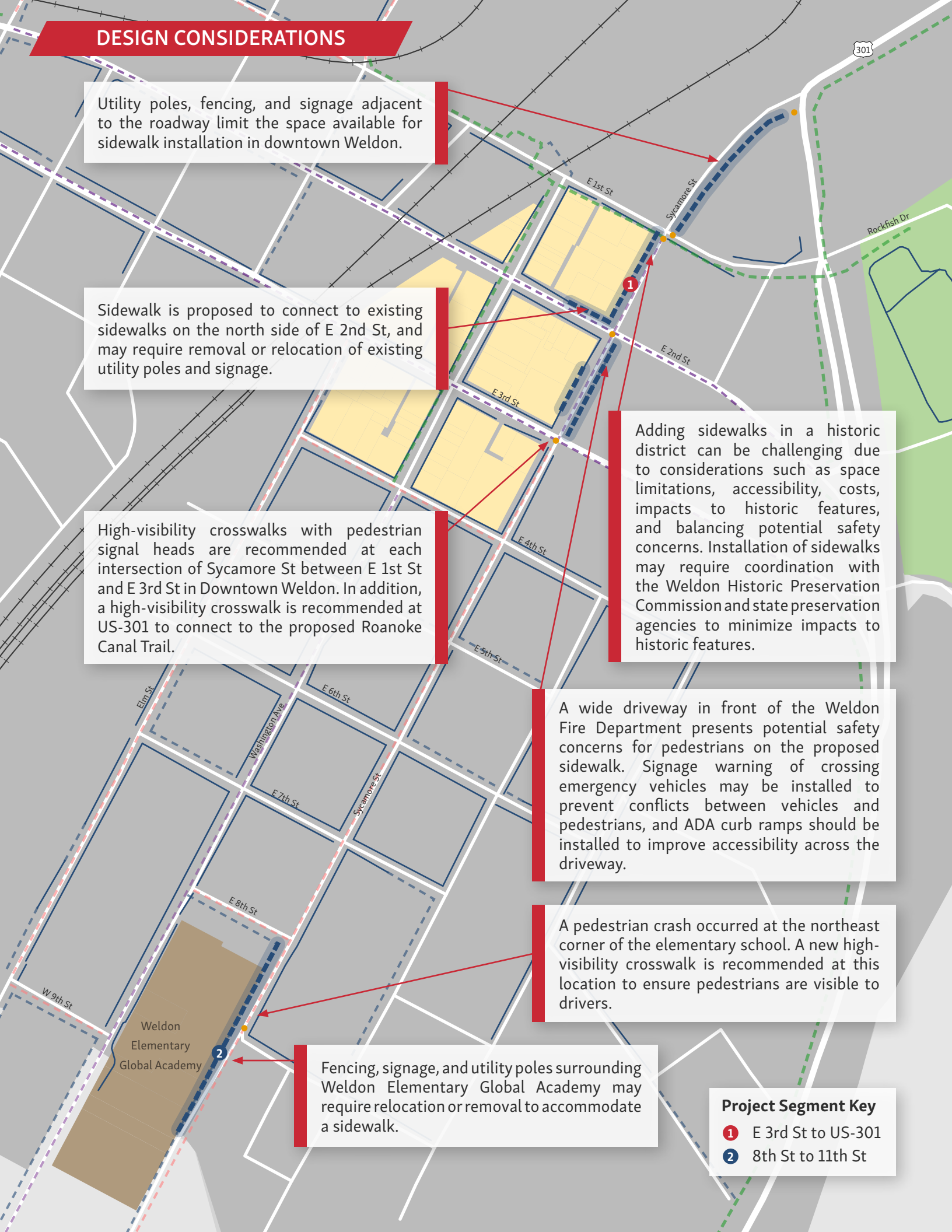
A wide driveway in front of the Weldon Fire Department presents potential safety concerns for pedestrians on the proposed sidewalk. Signage warning of crossing emergency vehicles may be installed to prevent conflicts between vehicles and pedestrians, and ADA curb ramps should be installed to improve accessibility across the driveway.

A pedestrian crash occurred at the northeast corner of the elementary school. A new high-visibility crosswalk is recommended at this location to ensure pedestrians are visible to drivers.

Fencing, signage, and utility poles surrounding Weldon Elementary Global Academy may require relocation or removal to accommodate a sidewalk.

Project Segment Key

- 1 E 3rd St to US-301
- 2 8th St to 11th St





05

Policy + Program Recommendations



OVERVIEW

Beyond infrastructure and amenity improvements, the bicycle and pedestrian experience is shaped by policies, regulations, and programs. The project team reviewed successfully implemented policies and programs from communities throughout the state and country to develop the recommendations outlined in this chapter of the plan. If adopted, these recommendations will help guide the development of safe, accessible, and attractive bicycle and pedestrian facilities recommended in **Chapter 4: Network Recommendations**, and to create a community culture that embraces and supports walking and bicycling as modes of transportation and recreation.

The following sections are covered in this chapter:

- Recommended Policies
- Recommended Programs

RECOMMENDED POLICIES

By implementing program and infrastructure recommendations alongside multimodal-friendly policies, the Town of Weldon can significantly enhance both bicyclist and pedestrian safety. These policies will not only improve safety but also ensure that multimodal needs are well-integrated into Town ordinances and codes. Below is a summary of recommended policies for the Town of Weldon.

POLICY RECOMMENDATION	DESCRIPTION
Define Active Transportation Facilities in the Subdivision Ordinance	The Town of Weldon should consider defining active transportation facilities (i.e., sidewalks, multi-use paths, bicycle facilities, and pavement markings) in their subdivision ordinance. Including a glossary of common active transportation terms will make it easier to understand the differences between facility types and ensure consistency in applying and creating additional standards for active transportation facilities.
Create Standard Design Guidelines for Active Transportation Facilities	The Town of Weldon should consider creating a standard set of design standards for sidewalk, multi-use path, and bicycle facilities to ensure consistent design within the community. At a minimum, facility design standards should include typical facility sections, materials selection, minimum and preferred widths of facilities, intersection treatments, and connectivity requirements.
Developer-Built Sidewalks and Shared Use Paths	The Town should consider updating its subdivision regulations to require developer-built shared use paths and sidewalks, where shown in officially adopted plans. Recognizing the vital role of private developers in the development of active transportation facilities, this step will promote the creation of pedestrian-friendly pathways and enhance active transportation options throughout the Town. A municipality may ask developers to construct planned sidewalks and shared use paths as a requirement to development in municipal limits. Planning staff should coordinate with developers to provide guidance on ordinance requirements and processes. In addition, they may also include specifications for multimodal design standards. Developers should be prepared to include active transportation facilities in future developments that provide connections to Weldon’s overall multimodal network.
Establish Wayfinding Standards	The Town of Weldon should consider establishing community-wide multimodal wayfinding standards to foster consistent placement, use, and design of wayfinding signage (including route confirmation, directional, and kiosk signage). Wayfinding standards should complement the existing Roanoke Canal Trail signage and standards to create a comprehensive multi-modal wayfinding system. Such a system will facilitate ease of navigation, enhance accessibility, and contribute to a more organized and user-friendly urban environment.
Establish an Active Transportation Advisory Committee	The Town of Weldon should consider establishing an Active Transportation Advisory Committee to guide the implementation of recommended multimodal infrastructure, programs, and policies outlined in the <i>Weldon Project Acceleration Plan</i> for the Town Board and staff. Weldon may also consider expanding the scope and role of an existing advisory committee, such as the Weldon Recreation Advisory Board, to accommodate active transportation issues.

POLICY RECOMMENDATION	DESCRIPTION
Adopt a Complete Streets Ordinance	The Town of Weldon should consider adopting a municipal Complete Streets Policy, similar to NCDOT’s statewide policy, to guide multimodal improvements for the local street network. NCDOT adopted its Complete Streets policy in 2019, which requires consideration of all modes of transportation in roadway improvement projects.
Adopt a Flood Damage Prevention Ordinance	<p>Considering recurrent flooding caused by the Roanoke River, the Town of Weldon should mandate the incorporation of nature-based solutions for infrastructure and stormwater management. This requirement should apply to all new roadway projects, upgrades, or reconstruction initiatives. By integrating nature-based solutions, such as green infrastructure, stormwater infiltration, conveyance, and storage improvements, the Town can effectively mitigate the adverse effects of stormwater and nuisance flooding. This proactive approach will promote resilience, protect the community, and contribute to sustainable development in the face of potential flood risks.</p> <p>More information on this subject can be found in FHWA’s Nature-Based Solutions for Coastal Highway Resilience: An Implementation Guide: https://www.fhwa.dot.gov/environment/sustainability/resilience/ongoing_and_current_research/green_infrastructure/implementation_guide/fhwahep19042.pdf</p>
Promote a Resilient Active Transportation Network Through Green Street Design Standards	<p>The Town of Weldon should consider developing a resilient active transportation network with green infrastructure and low impact development methods to mitigate or prevent localized flooding. The Environmental Protection Agency (EPA) highlights Green Streets which can help to protect the environment, improve community health, and contribute to placemaking. Green Street elements include permeable pavement, street trees, the use of recycled materials, native landscaping, energy-efficient street fixtures, and more. At a minimum, vegetative buffers should be provided on new sidewalk or multi-use path projects to increase drainage during flood events. Permeable pavement for active transportation facilities should also be considered to increase drainage from runoff.</p> <p>Follow this link for more information on Green Streets: https://www.epa.gov/G3/learn-about-green-streets</p>
Establish Dedicated Funding in the Capital Improvement Plan for Bicycle and Pedestrian Facilities	The Town of Weldon should consider a modest, dedicated local funding allocation for bicycle and pedestrian improvement programs and facilities in the Town’s budget to implement recommendations in this plan and to leverage other state and federal funding for these facilities.

POLICY RECOMMENDATION	DESCRIPTION
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Reduce Town-wide Speed Limit to 25mph	To enhance the safety of bicyclists using on-road bicycle facilities or shared street accommodations, the Town of Weldon should strongly consider reducing the Town-wide speed limit from 35 mph to 25 mph. The Town-wide speed limit applies to roadways without specified speed limits marked with signage. Lowering the Town-wide speed limit to 25 mph brings about a substantial reduction in the risk of injury for bicyclists involved in crashes with motorists. This proactive measure demonstrates the Town’s commitment to prioritizing the safety of vulnerable road users and fosters a more bike-friendly environment. By encouraging motorists to drive at slower speeds, the Town can create a conducive atmosphere for cyclists, promoting active transportation and contributing to an overall safer and more harmonious road system.
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Implement Signal Detection for Bicyclists at Signalized Intersections	To enhance bicycle safety and improve the efficiency of signalized intersections, the Town of Weldon should consider integrating standards for bicycle signal detection at actuated signals. By implementing this system, bicyclists can effectively notify signal controls of their intention to cross intersections, ensuring they receive appropriate green signal timings. This proactive approach not only promotes the safety of cyclists but also encourages active transportation, making the Town more bicycle-friendly. By accommodating the needs of bicyclists at signalized intersections in coordination with NCDOT, the Town demonstrates its commitment to fostering a sustainable and inclusive transportation system for all road users. Reference NACTO’s Urban Bikeway Design Guide for resources on bicycle signal detection.
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Adopt a Sidewalk Maintenance Policy	The Town of Weldon should develop and adopt a comprehensive sidewalk maintenance policy to ensure well-maintained sidewalks throughout the community. This policy should begin with the creation of a regularly updated sidewalk inventory, clearly defining the party responsible for maintenance. To ensure systematic upkeep, the policy should outline inspection frequency, track maintenance history, and establish measurable criteria for identifying necessary repairs or monitoring facilities. The criteria for determining maintenance needs may encompass various aspects such as identifying raised or sunken sections, cracks or openings, and vertical separations between sidewalk sections. Additionally, landscaping issues, such as overgrowth and obstructions hindering pedestrian movement, should also be addressed in the policy. To prioritize improvements effectively, the policy may establish a priority system based on factors such as usage rates and destinations served by the sidewalk facility. By adopting such a comprehensive sidewalk maintenance policy, the Town of Weldon can enhance pedestrian safety, improve accessibility, and cultivate a pedestrian-friendly environment that benefits all residents and visitors. More information on this subject can be found in FHWA’s A Guide for Maintaining Pedestrian Facilities for Enhanced Safety: https://highways.dot.gov/safety/pedestrian-bicyclist/guide-maintaining-pedestrian-facilities-enhanced-safety
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POLICY RECOMMENDATION**DESCRIPTION****Conduct an Accessibility Study/ADA Transition Plan of Existing Bicycle and Pedestrian Facilities**

The Town of Weldon should develop an Americans with Disabilities Act (ADA) Transition Plan to ensure full accessibility and compliance with the ADA requirements. Title II of the ADA mandates that public services and transportation, including sidewalks and multi-use paths, must be accessible to individuals with disabilities. While compliance is required for new constructions and alterations, the ADA also emphasizes the significance of accessibility in maintenance of existing infrastructure.

The Transition Plan should comprehensively identify the accessibility improvements necessary to bring all public services and transportation facilities in line with ADA standards. It should not only focus on new constructions but also address the existing infrastructure, ensuring that everyone, including individuals with disabilities, can access the town's amenities and services.

Furthermore, the plan should explore various funding options available for implementing these accessibility improvements. It should provide a clear and feasible timeline for the execution of the proposed changes, ensuring that the Town progresses towards full ADA compliance in a timely manner.

By conducting an ADA Transition Plan, the Town of Weldon can prioritize inclusivity, promote equal access for all residents and visitors, and work towards creating a welcoming and accommodating environment for everyone within the community.



A non-ADA compliant curb ramp in Weldon

RECOMMENDED PROGRAMS

Bicycle- and pedestrian-specific programs encourage and strengthen the culture for the use of multimodal facilities within a community. Investments in these programs, when coupled with infrastructure improvements, frequently lead to an increase in biking and walking among residents and visitors. Administering a series of programs to the community can help serve as an orientation to active transportation, and more specifically the use of sidewalks, shared use paths, and on-road bike facilities. A summary of multimodal programs and initiatives are provided below and are organized into the following program classifications that can help the Town select programs that are tailored to the needs of the community:

- Educational Programs
- Encouragement Programs
- Project Based Programs
- Evaluation Programs

EDUCATIONAL PROGRAMS

WATCH FOR ME NC

Watch for Me NC is a comprehensive statewide safety and awareness campaign administered by NCDOT geared toward bicycle and pedestrian safety, education, and enforcement. Combining multimedia with public engagement, Watch for Me NC promotes safety messages at local events, followed by targeted enforcement in areas with heightened risk of crashes involving bicyclists and pedestrians. A key component of the program includes training law enforcement officers on existing laws and how to enforce them.

<https://www.watchformenc.org/>

LET'S GO NC

Let's Go NC! A Pedestrian and Bicycle Safety Skills Program for Healthy, Active Children is an NCDOT sponsored, all-in-one package of lesson plans, materials, activities, and instructional videos for instructors to teach and encourage children to practice and develop safe pedestrian and bicycle behaviors while promoting healthy transportation choices and active lifestyles that can be carried into adulthood.

<https://connect.ncdot.gov/projects/BikePed/Pages/LetsGoNC.aspx>

SAFE ROUTES TO SCHOOL

Through the NC Safe Routes to School program, NCDOT works to make walking and riding a bicycle to school safe for all children. The program facilitates the planning, development, and implementation of programs and activities to improve safety near schools and increase walking and biking rates to school. The program includes initiatives such as Let's Go NC and the Safe Routes to School Non-Infrastructure Transportation Alternatives Program.

<https://connect.ncdot.gov/projects/BikePed/Pages/Non-Infrastructure-Alternatives-Program.aspx>

EAT SMART, MOVE MORE NC

Eat Smart, Move More NC is a statewide program that encourages healthy lifestyles through healthy food and physical activity in every part of people's lives. Walking is an important component of maintaining physical health and features prominently in the Eat Smart, Move More NC program. Resources, guides, and tips for using food and exercise to lose weight, prevent chronic disease and health problems, and make healthier food choices are among the materials offered by the program.

<https://www.eatsmartmovemorenc.com/>

ENVIRONMENTAL EDUCATION

Active transportation facilities such as greenways provide users with access to nature. Installing educational signage on the environmental benefits or existing conditions along a greenway could serve as environmental education programming for a community. One effective approach to foster environmental education along a greenway is by installing tree identification markers. Greenway cleanups and other volunteer events may also instill a sense of pride and support environmental stewardship for the land surrounding the greenway. According to the United States Environmental Protection Agency (USEPA), the following components should be incorporated into environmental education programming:

- Awareness and sensitivity to the environment and environmental challenges
- Knowledge and understanding of the environment and environmental challenges
- Attitudes of concern for the environment and motivation to improve or maintain environmental quality
- Skills to identify and help resolve environmental challenges
- Participation in activities that lead to the resolution of environmental challenges

<https://www.epa.gov/education/what-environmental-education>

ENCOURAGEMENT PROGRAMS

WALKING/BICYCLING MAP AND/OR MOBILE APP

Walking and bicycling network maps and mobile apps are developed to guide pedestrians or bicyclists along preferred routes with existing sidewalks, sidepaths, greenways, and bike routes. Maps and mobile apps are also helpful resources that assist new residents and visitors to safely navigate the transportation network.

WALK/BIKE TO SCHOOL DAY EVENTS

Walk/Bike to School Days are energizing events that remind adults and students alike of the health and environmental benefits of walking or bicycling to school. In numerous communities, such events result in increased walking or bicycling throughout the school year. This is often due to the event raising awareness about safety issues that require attention, or because it inspires children and families to adopt walking or biking as their preferred mode of school commute. National Walk & Roll to School Day occurs every October and Bike & Roll to School occurs every March.

<https://www.walkbiketoschool.org/>

WALK/BIKE TO WORK DAY EVENTS

Walk/Bike to Work Days are annual events that promote walking and bicycling as an option for commuting to work. Walk to Work Day is usually held every April. Walk to Work Day events typically include pit stops along popular walking routes to provide commuters with free breakfast, prizes, or education materials on active transportation. Bike to Work Day takes place in May every year. This day both encourages people to bike to work and raises awareness of cyclists as they commute to and from work each day. For many communities, these events lead to an increase in walking for transportation and recreation.

WALK WITH A DOC

Walk With a Doc is a program that promotes the health benefits of walking. Events are held year-round and organized by medical offices or medical schools. Walk With a Doc or Walk With a Future Doc events often consist of an educational presentation on a health topic followed by a community walk with coffee, healthy snacks, and blood pressure checks offered.

<https://walkwithadoc.org/>

THEMED WALKING/BIKING EVENTS

Walking or bicycling events encourage active transportation and present opportunities for residents to socialize over shared experiences. Local advocacy groups and/or the Town can promote walking or bicycling by hosting themed walking or bicycling events. Events can be centered around holidays, local festivals, or coincide with national events such as Earth Day and

National Trails Day. For many communities, these events lead to regularly scheduled walks and rides.

WALK-FRIENDLY COMMUNITY DESIGNATION

Walk Friendly Communities have shown a commitment to improving and sustaining walkability and pedestrian safety through comprehensive programs, plans, and policies. Communities apply to this program to receive recognition in the form of a Bronze, Silver, Gold, or Platinum designation. Before applying, the Town can take a self-assessment using the online assessment tool to collect all the Town's pedestrian-related data in one location which can help guide the discussion about areas for improvement. This data can also be used to apply for future grants. The Designation as a Walk Friendly Community sets the Town up as an example among peer cities and raises local awareness about the value of supporting a walkable environment.

<https://www.walkfriendly.org/>

BICYCLE FRIENDLY COMMUNITY DESIGNATION

The Bicycle Friendly Community (BFC) program provides a roadmap to improving conditions for bicycling and guidance to help make a community's vision for a better, bikeable community a reality. The BFC program is sponsored by the League of American Bicyclists. Applications for the programs are accepted twice a year. The program provides a checklist for current and aspiring BFCs that includes the following action items:

- Adopt the BFC Action Plan Charter
- Review BFC resources, facts and questions, and information online.
- Form an Active Transportation Advisory Committee
- Conduct a bike parking inventory or audit in your community
- Encourage local employers to apply for the Bicycle Friendly Business designation
- Host a League Cycling Instructor (LCI) seminar or sponsor one or more local residents, bike advocates, and/or municipal employees to attend an existing LCI seminar elsewhere.
- Create or improve the inventory of your community's bike infrastructure
- Conduct a bike count to start to benchmark and measure trip ridership in your community
- Send someone to attend the National Bike Summit
- Organize a local bicycling event or series of events

<https://www.bikeleague.org/community>

WAYFINDING SIGNAGE

Wayfinding systems consist of comprehensive signage and/or pavement markings to guide bicyclists and pedestrians to their destinations along preferred routes. Signs are typically placed at decision points along the routes – typically at the intersection of two or more bikeways and greenways and at other key locations leading to and along designated routes. There are three types of wayfinding signs: confirmation signs, turn signs, and decision signs. Pavement markings can also be installed to help reinforce routes and directional signage and to provide route branding benefits. Signage should be simple and easily understood while a user is in motion.

BICYCLE PARKING

Bicycle parking is a critical strategy for promoting bicycling for transportation and recreation. The presence of convenient, user-friendly, and secure bicycle parking not only motivates individuals to replace some car trips with bicycle trips but also legitimizes bicycling as a viable transportation mode. By offering parking opportunities on par with motorized modes, it encourages more people to opt for bicycles as their preferred means of transportation. Guidance on bike parking, including site planning for short and long-term parking, rack selection, and placement can be found in the Association of Pedestrian and Bicycle Professionals (APBP) Essentials of Bike Parking.

https://www.apbp.org/assets/docs/EssentialsofBikeParking_FINAL.pdf

BICYCLE REPAIR STATIONS

Bicycle repair stations include all the tools necessary to perform basic bike repairs and maintenance, from changing a flat tire to adjusting brakes. The tools are securely attached to the stand with cables with tamper-proof fasteners, and an air pump can be attached to the base of the repair station. Bicycle repair stations should be placed in areas highly trafficked by bicyclists, such as parks and recreation centers, Downtown, trailheads, and employment and shopping centers.

PROJECT BASED PROGRAMS

POP-UP DEMONSTRATION EVENTS / PILOT PROJECTS

Pop-up demonstrations or pilot projects for proposed multimodal facilities, such as plazas, parklets, and pop-up bike lanes create an action-based approach to transforming the local bicycle and pedestrian network by using short-term, low-cost, and scalable interventions to propel long-term change. Demonstrations or pilot projects can be led by the Town or local advocates and residents to experiment with and gather input on potential street design improvements.

<http://tacticalurbanismguide.com/about/>

OPEN STREETS EVENTS

Open Streets events open local streets to people by temporarily closing them to car traffic. This program provides resources and guidance to communities on opening their streets while also advocating for people-friendly and walkable cities. Communities that implement Open Streets programs can create a place for people of all ages and abilities to gather and enhance their health and mobility. Open Streets events take place at different times of the year in many communities.

<https://openstreetsproject.org/>

PAINT THE PAVEMENT PROGRAM

Paint the Pavement programs allow mural painting on public streets to encourage community building and public art as a means to transform roadway public space into neighborhood assets. Street murals implemented as part of the program also serve as an important traffic calming solutions in residential neighborhoods. Paint the Pavement programs are sponsored by governmental agencies, but mural design and painting are often led by volunteers.

SHARED STREETS PROGRAM

Shared streets provide pedestrians with the right-of-way and remove the formal distinctions between spaces dedicated to pedestrians, cyclists, and motorized vehicles. During the COVID-19 pandemic, shared streets pilot programs were initiated by municipalities to help people find safe, healthy ways to enjoy outdoor activities while supporting social distancing. Most programs implement soft street closures on designated residential streets to allow for residents to safely walk and bike in their neighborhood.

EVALUATION PROGRAMS

311 SERVICE / SEE-CLICK-FIX

311 service is a governmental program that allows citizens to report maintenance issues, such as street debris in bike lanes, potholes, damaged sidewalks, or malfunctioning traffic signals. Similarly, See-Click-Fix is an online platform and mobile app that allows residents to report maintenance issues via interactive map to precisely pinpoint and describe the issue. Municipalities can partner with See-Click-Fix as their local 311 provider.

FACILITY INVENTORY + MAINTENANCE PROGRAM

Facility inventory and maintenance programs are tools for communities to maintain up-to-date lists and maps of active transportation facilities such as sidewalks, sidepaths, bike lanes, and greenways. This can serve as a guide for residents and visitors seeking to walk or bike in the community. A facility inventory can also help staff identify and prioritize segments of pedestrian or bicycle infrastructure in need of maintenance and rehabilitation.

PEDESTRIAN/BICYCLE COUNT PROGRAM

Pedestrian or bicycle count programs provide non-motorized travel information for sidewalks and shared use paths. Count programs assist municipalities to understand existing active transportation traffic and plan for future non-motorized infrastructure needs. Agencies who show clear evidence of use are more likely to receive funding for projects. NCDOT sponsors the North Carolina Non-Motorized Volume Data Program, which is managed by NC State University.

<https://itre.ncsu.edu/focus/bike-ped/nc-nmvd/>



See-Click-Fix allows citizens to report facilities in need of maintenance



06

Implementat



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OVERVIEW

Recommendations outlined in this Plan propose significant active transportation investments that will positively impact the way in which residents, employees, and visitors travel throughout the Town of Weldon. Successful implementation of this Plan will require a coordinated and consistent effort with a wide range of community partners.

This chapter outlines partner roles and action steps to guide key agencies and stakeholders in the funding, design, and construction of bicycle and pedestrian network recommendations as well as the implementation of recommended policies and programs. Action steps prioritize implementation strategies over a 10-year planning horizon. Funding resources available to the Town of Weldon for implementation are outlined in **Appendix A: Funding Resources**.

The following sections are covered in this chapter:

- Key Partners in Implementation
- Action Plan
- Maintenance Guidance
- Project Development Process

KEY PARTNERS IN IMPLEMENTATION

Achieving success in the development of the *Project Acceleration Plan* relies on collaboration with community partners and stakeholders at the local, regional, and state levels. Implementation will require both individual and coordinated efforts by all project stakeholders. Key roles in the implementation of this plan are outlined below.

The following partners were identified:

- **Town of Weldon**
 - » Board of Commissioners
 - » Planning Board
 - » Town Departments
 - Planning & Development
 - Parks & Recreation
 - Public Works
 - » Additional Committees & Commissions
 - Recreation Advisory Committee
 - Small Town Main Street Committee
 - Weldon Historic Preservation Commission
 - » Weldon City Schools

- **Regional + County Stakeholders**
 - » Roanoke Canal Commission
 - » Roanoke Canal Museum and Trail
 - » Halifax County
 - » Peanut Belt Rural Planning Organization (RPO)

- **North Carolina Wildlife Resources Commission (NCWRC)**

- **North Carolina Department of Transportation (NCDOT)**
 - » Division 4
 - » Integrated Mobility Division (IMD)
 - » Rail Division

- **CSX**

- **Local Residents, Businesses, + Community Groups**
 - » BikeWalkNC
 - » Halifax Community College
 - » Weldon in Action
 - » Halifax County Business Horizons
 - » Roanoke Valley Chamber of Commerce

- **Private Developers**

TOWN OF WELDON

BOARD OF COMMISSIONERS

The Weldon Board of Commissioners, which includes the Mayor, establishes policies, while the Town Administrator is responsible for implementing and administering these policies. The Town Board of Commissioners holds ultimate responsibility for adopting this plan, establishing recommended policies, and supporting the necessary action steps for plan implementation. The Board of Commissioners has a broader legislative role and can accept, reject, or modify the recommendations of the Planning Board. By adopting the Plan, the Board of Commissioners expresses their commitment to expanding bicycle and pedestrian infrastructure and programs in Weldon, as well as their intent to support the efforts of other key partners, including Town departments and NCDOT.

PLANNING BOARD

Members of a planning board are typically appointed, and they often include individuals with expertise in land use planning, architecture, engineering, or related fields. Their role is to provide technical advice and recommendations on land use and development issues. The Planning Board is also responsible for adopting this Plan, laying the groundwork for recommended policies, and providing essential support for the action steps necessary to bring the plan's recommendations to life. They also express their intent to support the efforts of other key partners, such as Town departments and NCDOT.

TOWN COMMITTEES

The proposed Town of Weldon Active Transportation Advisory Committee (ATAC), in conjunction with the existing Weldon Recreation Advisory Board, Small Town Main Street Committee, and the Weldon Historic Preservation Commission, can collectively steer the implementation of the recommended bicycle and pedestrian infrastructure, programs, and policies outlined in this Plan. The establishment of an ATAC introduces a system of checks and balances, ensuring accountability for elected officials and staff. It offers valuable guidance on matters related to bicycle and pedestrian concerns while also affording residents the opportunity to provide input and receive responses from Town staff.

In addition, members of the Recreation Advisory Board possess the potential to provide insights into various aspects of recreational policies, facilities, finances, programs, long-term planning, and land acquisition or disposition. Should the ATAC not be established, the role of the existing Recreation Advisory Board would need to be expanded to encompass recommendations pertaining to active transportation facilities and programs, further aligning with the broader scope of this Plan.

TOWN DEPARTMENTS

Various town departments in Weldon take the lead or provide support for the development of active transportation projects. When leading a project, municipal staff assume the central role of coordinating project development, engaging the community, shaping policies, devising funding strategies, and overseeing maintenance.

Town departments involved in the implementation of the *Project Acceleration Plan* encompass the Planning & Development, Parks & Recreation, and Public Works departments. The Planning Department offers guidance to citizens, staff, and elected officials on policies related to land use, housing, transportation, zoning, and growth management. The Public Works Department is responsible for the maintenance and repair of Town-owned roadways, buildings, water and sewer systems, and grounds. Finally, the Parks & Recreation Department manages the planning, design, and maintenance of both existing and future parks, while also providing recreation services and programs.

REGIONAL + COUNTY STAKEHOLDERS

Other governmental organizations that have jurisdictional authority or administer services in Weldon play a key role in project implementation by working with lead agencies to advance shared goals of improving multi-modal connectivity and expanding travel choices in the region. Key agency partners include Halifax County and the Peanut Belt Rural Planning Organization (RPO).

Halifax County regulates zoning and development within unincorporated areas of the county, develops long-range plans for transportation and land use, and coordinates with municipalities and state agencies on the implementation of parks, recreation, and transportation projects.

The Peanut Belt RPO, in cooperation with the NC Department of Transportation, develops long-range local and regional multi-modal transportation plans, provides a forum for public participation in the transportation planning process, develops and prioritizes suggestions for projects that the organization believes should be included in the State's Transportation Improvement Program (STIP), and provides transportation-related information to local governments and other interested organizations and persons.

The Roanoke Canal Trail and Museum, in conjunction with the Roanoke Canal Commission, form a dynamic partnership committed to enhancing and extending the existing Roanoke Canal Trail. The museum offers an immersive experience through its exhibits, delving into the rich history of the river, canal, and the surrounding area. The Roanoke Canal Commission was established with the primary mission of securing property rights and preserving the original navigation canal, along with its associated architectural treasures. This Commission not only acquires the necessary land but also initiates preservation and restoration efforts. The Commission's broader vision is to foster development and safeguard the natural beauty and historical significance of the area, which remains an integral part of the legacy of the old Roanoke Navigation Company Canal.

NORTH CAROLINA WILDLIFE RESOURCES COMMISSION (NCWRC)

The NCWRC, a state government agency, was created by the General Assembly in 1947 to conserve and sustain the state's fish and wildlife resources through research, management, use, and public input. The NCWRC holds the responsibility of enforcing North Carolina's laws governing fishing, hunting, trapping, and boating. Its mission is to conserve North Carolina's wildlife resources and their habitats and provide programs and opportunities that allow hunters, anglers, boaters and other outdoor enthusiasts to enjoy wildlife-associated recreation.

The Weldon Riverfront Enhancement Project is a comprehensive initiative aimed at transforming the River Falls Park in Weldon. It encompasses the construction of boardwalks both upriver and downriver from the NC Wildlife Resources Boat Landing, strategically positioned at the heart of the park. The Weldon Riverfront Enhancement Project will be implemented within the existing River Falls Park, utilizing property owned or leased by the Town of Weldon. To ensure the seamless integration of trail connections to this project, partnering with the NCWRC emerges as a crucial step. Collaborating with the NCWRC would not only enhance the project's overall success but also facilitate a coordinated approach, aligning the Town's vision for the launch with conservation practices and regulatory considerations associated with the river environment.

NORTH CAROLINA DEPARTMENT OF TRANSPORTATION (NCDOT)

NCDOT allocates federal and state funding and establishes policies for transportation improvements in communities across North Carolina. Every two years, NCDOT develops the State Transportation Improvement Program (STIP), which identifies projects that will receive funding during a 10-year period. NCDOT policies, such as Complete Streets provide guidance and oversight for permitting and implementing active transportation projects. The Complete Streets Policy, adopted in August 2019, requires NCDOT to consider and incorporate multimodal facilities in the design and improvement of the state's transportation projects.

Given its role as the primary state agency responsible for funding allocation, policy guidance, and approval of activities along NCDOT-maintained roadway corridors, NCDOT assumes a pivotal role in the execution of the *Project Acceleration Plan*. Notably, NCDOT's Divisions actively engaged in project development encompass Division 4 and the Integrated Mobility Division. In addition, NCDOT IMD may be influential in helping Weldon City Schools administer a Safe Routes to School Program to develop encouragement and safety programs for students walking and biking to school.

NCDOT's Rail Division oversees rail infrastructure across the state, operates Amtrak service on the Piedmont and Carolinian routes, and is a partner in achieving safety at rail crossings. The Rail Division plays a key role in creating safe bicycle and pedestrian crossings of railroads and will be critical to the implementation to connect Weldon to the Amtrak network with the construction of a planned passenger rail station in the future.

CSX

CSX is the owner of the railroad tracks and the corresponding right-of-way that traverse Weldon, serving as a vital conduit for both freight and passenger transportation. The railroad assumes a role in fostering secure mobility throughout its infrastructure in the Town. To uphold a commitment to safety, CSX upkeeps crossing systems, ensuring the well-being of its operations and the individuals who depend on safe passage across the tracks. Furthermore, CSX is a crucial partner in the development of secure railroad crossings that accommodate bicyclists and pedestrians.

LOCAL RESIDENTS, BUSINESSES + COMMUNITY GROUPS

Residents, businesses, and community groups that promote walking and biking as viable forms of transportation serve a key role in advocating for project and program investment. Community members and groups generate support for projects by raising awareness amongst the public, advocating to elected officials to prioritize funding for active transportation, and fostering collaboration amongst jurisdictional partners. Key organizations that may also support implementation of the *Project Acceleration Plan* include BikeWalk NC, Halifax Community College, Weldon in Action, Halifax County Business Horizons, and the Roanoke Valley Chamber of Commerce.

BikeWalk NC, a membership-based statewide advocacy organization, is dedicated to advancing non-motorized transportation options for North Carolina residents and visitors. Its network encompasses a diverse range of stakeholders, including local shop owners, enthusiastic cycling clubs, dedicated transportation experts, and many others, contributing to a holistic outreach throughout the state to support its vision of promoting active transportation as a viable mode of travel.

Halifax Community College, a publicly accessible institution situated in Weldon, serves as a pivotal community hub due to its central location and the capacity to draw substantial crowds. This highlights the significance of investing in bicycle and pedestrian accommodations to enhance accessibility to and from this vital community resource.

Weldon in Action is a coalition comprising both current and former residents of Weldon, united in their shared goal of revitalizing the Town. Its members are committed to the development and promotion of the Roanoke Canal Trail, with a strong focus on shaping a future that encourages biking and walking throughout the Town.

Halifax County Business Horizons and the Roanoke Valley Chamber of Commerce play a pivotal role in driving economic growth, bolstering local businesses, and cultivating a positive business environment within the Town of Weldon. By endorsing and championing the bicycle and pedestrian projects outlined in this plan, especially those located in Downtown Weldon, these groups not only add to the Town's charm but also draw in visitors and deliver direct advantages to businesses.

PRIVATE DEVELOPERS

Private developers can have a significant impact on the development of bicycle and pedestrian facilities within a community. They are often required, by municipal ordinances such as the Unified Development Ordinance (UDO), to construct planned sidewalks, sidepaths, greenways, and on-street bicycle facilities within the municipal limits. It is essential for the Town Planning staff to collaborate with developers, offering guidance on upcoming requirements and processes, as well as the design standards for both bicycle and pedestrian facilities. Developers should be prepared to include these facilities in future projects, which will contribute to the expansion of Weldon's active transportation network.

ACTION PLAN

The following Action Plan outlines strategies to implement prioritized bicycle and pedestrian infrastructure, policies, and programs in Weldon based on a 10-year implementation plan. The Town should apply the following steps to implement recommendations identified in this plan. Estimated timeframe durations for the steps are categorized into short-term, medium-term, long-term, and perpetual steps. These categories are described below.

STATUS DESCRIPTION	EXPLANATION
Short-Term	The action item could be initiated in the near future within 2-4 years.
Medium-Term	The action item could be initiated within the next 4-8 years.
Long-Term	The action item could be initiated within the next 8+ years.
Perpetual	The action item does not have an end date. It may be currently under progress and will continue to be implemented in perpetuity of this plan.

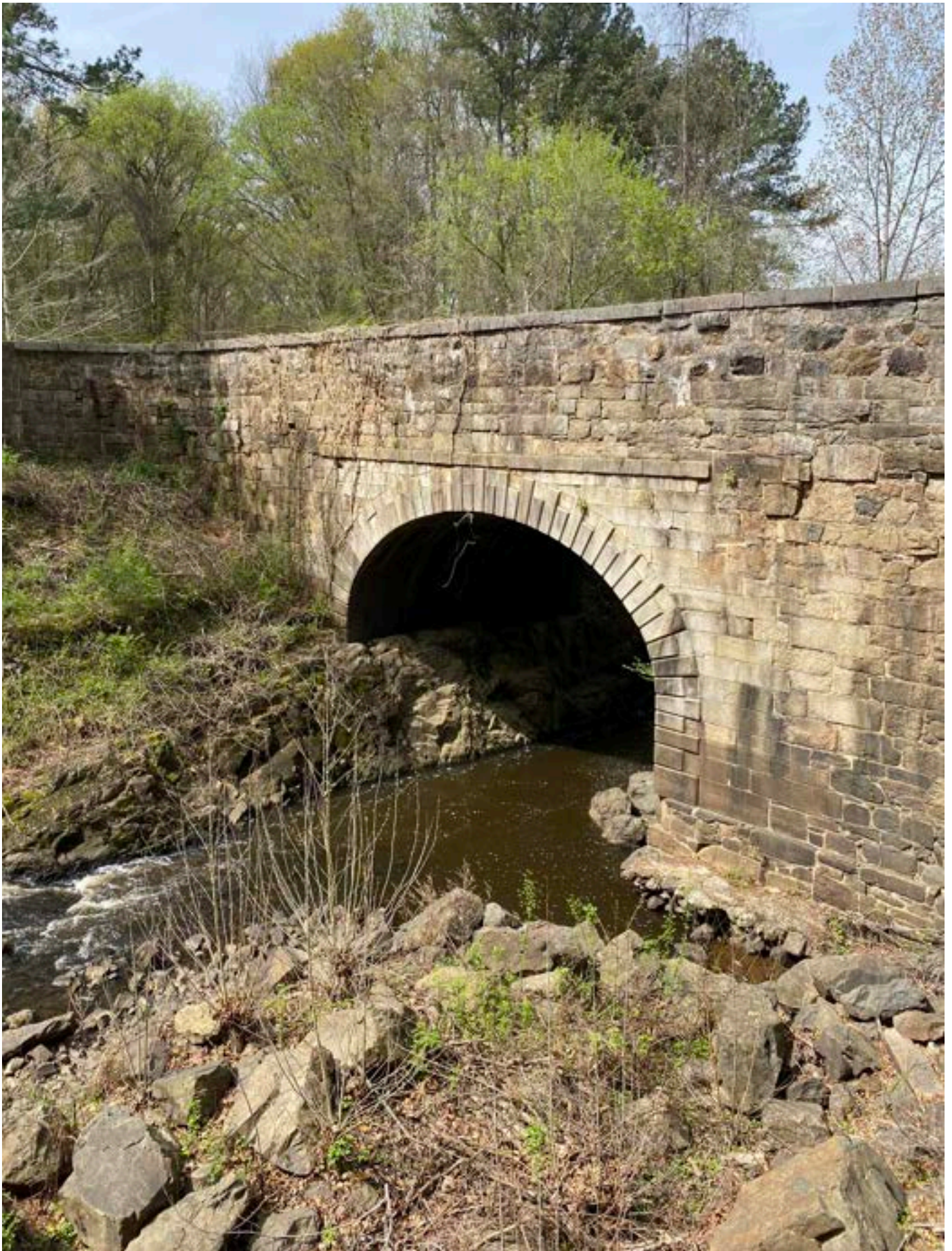
ACTION	DESCRIPTION	LEAD	SUPPORT	TIMELINE
Adopt the <i>Project Acceleration Plan</i> as the Town of Weldon’s bicycle and pedestrian plan.	Establish the <i>Project Acceleration Plan</i> as the guiding planning document for both bicycle and pedestrian transportation.	Planning Board	Town Departments, Steering Committee	Short-Term
Coordinate with the Peanut Belt RPO on the Halifax County Comprehensive Transportation Plan (CTP) to include recommendations from the <i>Project Acceleration Plan</i>.	The NCDOT Complete Streets Policy requires recommendations from locally adopted plans to be incorporated into the regional CTP for bicycle and pedestrian facilities to be evaluated as part of NCDOT roadway projects.	Peanut Belt RPO, Planning Board	NCDOT Transportation Planning Division, NCDOT IMD, Town Planning & Development Department	Short-Term
Establish an Active Transportation Advisory Committee (ATAC) or expand the Recreation Advisory Committee to guide plan implementation.	Establish an ATAC or expand the role of the Recreation Advisory Committee to guide Town staff in the implementation of recommended infrastructure, programs, and policies.	Planning Board	Town Staff, Steering Committee, Town Parks & Recreation Department	Short-Term
Designate Town staff to lead implementation of plan recommendations.	The Town Administrator and Department Directors of Planning & Development, Public Works, and Parks & Recreation should identify their staff leads and duties for plan implementation. Staff leads should be selected to guide project development, complete streets, data collection, programs, and maintenance.	Town Administrator Town Department Directors	Town Staff	Short-Term

ACTION	DESCRIPTION	LEAD	SUPPORT	TIMELINE
Provide staff resources and training related to bicycle and pedestrian infrastructure and programming.	Provide resources and funding for staff involved in plan implementation to attend webinars, trainings, and conferences on active transportation planning, design, and programming Invest in guidance materials available from FHWA, NACTO, AASHTO, and NCDOT.	Town Administrator, Town Department Directors	Town Staff	Short-Term
Establish wayfinding standards for bicycle and pedestrian facilities.	Establish townwide standards for consistent placement, use, and design of wayfinding signage to create a comprehensive multimodal wayfinding system in the Town.	Town Planning & Development Department, Planning Board	Town Staff	Short-Term to Medium-Term
Adopt a municipal Complete Streets ordinance.	Adopt a municipal Complete Streets Policy, like NCDOT’s statewide policy, to guide multimodal improvements for the local street network.	Planning Board	Town Planning & Development Department, Town Staff	Short-Term
Adopt a flood damage prevention ordinance.	Adopt a flood damage ordinance to promote environmental resilience, land use planning, recreational opportunities, and community engagement in flood-prone areas within the Town.	Planning Board	Town Planning & Development Department, Town Staff	Short-Term
Define standards and develop design guidance for active transportation facilities in the Subdivision Ordinance.	Amend the Subdivision Ordinance to define active transportation facilities (i.e., sidewalks and multi-use paths) and include design guidance.	Planning Board, Town Staff	Town Planning & Development Department	Short-Term
Ensure that bicycle and pedestrian infrastructure recommendations are incorporated into new developments by requiring developer-built shared use paths and sidewalks, where shown in officially adopted plans.	The Planning & Development Department should continue to coordinate closely with developers during the development review process on the inclusion of multimodal facilities in new developments where required.	Town Planning & Development Department	Planning Board, Town Departments	Perpetual

ACTION	DESCRIPTION	LEAD	SUPPORT	TIMELINE
Develop funding strategies for bicycle and pedestrian facility implementation.	Review funding resources provided in Appendix A and coordinate with Town staff, Peanut Belt RPO, Halifax County, and NCDOT IMD to determine funding strategies for plan recommendations. Consider prioritizing dedicated funding for multimodal facilities in the Town budget.	Town Administrator, Town Staff	Planning Board, Peanut Belt RPO, NCDOT IMD, Halifax County	Perpetual
Coordinate with NCDOT IMD to participate in the Bicycle and Pedestrian Count Program.	Coordinate with NCDOT to be involved in the NC State Institute for Transportation Research and Education (ITRE) Non-Motorized Volume Data Program to collect bicycle and pedestrian counts along multimodal facilities.	Town Parks & Recreation Department, Town Public Works Department	NCDOT IMD, NCDOT Division 4	Short-Term to Medium-Term
Coordinate with NCDOT, Amtrak, and CSX on the development of passenger rail service to Weldon.	Coordinate with NCDOT, Amtrak, and CSX on the development of passenger rail service in Weldon to ensure multimodal connectivity between transit and bicycle and pedestrian facilities.	Town Planning & Development Department, NCDOT Rail Division	Peanut Belt RPO, NCDOT IMD, Amtrak, CSX, Town Staff, Board of Commissioners	Perpetual
Coordinate with implementation partners on the development of priority projects.	Coordinate with NCDOT, regional partners, and adjacent jurisdictions (Roanoke Rapids) on the development of priority projects. Consider conducting feasibility studies for projects that need further study.	Town Departments, Town Staff	Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, Halifax County, Northampton County, and Roanoke Rapids	Short-Term to Medium-Term
Coordinate with Town Staff on the development of the 1st St and Washington Ave Streetscape improvements as the project develops following design.	The proposed 1st St sidepath will extend the Roanoke Canal Trail from the current trailhead to River Falls Park via 1st St. Coordinate with Town Staff to ensure a seamless connection between the facilities, alignment with historic district guidelines, and consideration of streetscape improvements for bicycle and pedestrian projects recommended in this plan.	Town Departments, Town Staff	Weldon Historic Preservation Commission and state preservation agencies, Weldon in Action, Roanoke Canal Commission, Roanoke Canal Museum & Trail, NCDOT Division 4, NCDOT IMD	Short-Term to Medium-Term

ACTION	DESCRIPTION	LEAD	SUPPORT	TIMELINE
Coordinate with NCDOT Division 4 on planned Strategic Transportation Improvement Plan (STIP) projects to include bicycle and pedestrian facilities.	Coordinate with NCDOT Division 5 and IMD on future State Transportation Improvement Program (STIP) projects to incorporate bicycle and pedestrian facilities and crossing treatments in roadway enhancements (i.e., Signal detection for bicyclists at signalized intersections, crosswalks, new multiuse facilities).	Town Staff, NCDOT Division 4	NCDOT IMD, Peanut Belt RPO	Perpetual
Coordinate with NCDOT Division 4 on their roadway resurfacing schedule to implement improvements.	Coordinate with NCDOT Division 4 on proposed Highway Maintenance Improvement Program (HMIP) projects to incorporate intersection improvements such as high-visibility crosswalks in resurfacing projects.	Town Public Works Department	Town Departments and Staff, NCDOT IMD	Perpetual
Coordinate with implementation partners on the proposed extension of the Roanoke Canal Trail along the Roanoke River.	Coordinate with the NCDOT Rail Division, CSX, the Roanoke Canal Commission, Roanoke Canal Trail & Museum, and Weldon in Action on the proposed extension of the Roanoke Canal Trail along the Roanoke River between S Walnut St and River Falls Park.	Town Staff	Town Departments, Roanoke Canal Commission, Roanoke Canal Museum and Trail, Peanut Belt RPO, NCDOT Division 4, NCDOT IMD, NCDOT Rail Division, CSX, Weldon in Action	Short-Term to Medium-Term
Coordinate with NCWRC on trail connections to the new kayak and canoe launch.	The Town should coordinate with the NC Wildlife Resources Commission regarding a trail connection to the canoe and kayak launch currently in development along the Roanoke River (Weldon Riverfront Enhancement Project).	Town Staff	Town Departments, Roanoke Canal Commission, Roanoke Canal Museum and Trail, Peanut Belt RPO, NCWRC, NCDOT Division 4, NCDOT IMD, NCDOT Rail Division, CSX	Short-Term

ACTION	DESCRIPTION	LEAD	SUPPORT	TIMELINE
Coordinate with Weldon City Schools on local Safe Routes to School programming.	Coordinate with Weldon City School staff to create a Safe Routes to School programming in Weldon schools.	Town Staff	Weldon City Schools, NCDOT IMD	Perpetual
Develop a Bicycle and Pedestrian Facility Operations and Maintenance Plan.	Develop a bicycle and pedestrian facility maintenance plan to define maintenance responsibilities, budget, and staff resources required to maintain facilities.	Town Public Works Department, Town Parks & Recreation Department	Town Administrator, Planning Board	Short-Term
Prioritize funding for an accessibility study/ADA Transition Plan of existing bicycle and pedestrian facilities.	Develop an ADA Transition Plan to ensure full accessibility and compliance with the ADA requirements.	Town Staff, Planning Board	Town Administrator, Planning Board	Short-Term
Prioritize funding for feasibility studies for the priority projects highlighted in the Project Acceleration Plan.	Secure funding for priority projects recommended in this plan.	Town Staff, Planning Board	NCDOT IMD	Perpetual
Reduce the Town-wide speed limit to 25 MPH.	Amend Chapter 72.25 of the Code of Ordinances to reduce the Town-wide speed limit from 35 mph to 25 mph along roadways without specified speed limits marked with signage.	Planning Board, Board of Commissioners	Town Staff	Short-Term



The Roanoke Canal Trail aqueduct over Chockoyotte Creek

MAINTENANCE GUIDANCE

Bicycle and pedestrian facilities are critical components of a multimodal transportation network, but they require regular maintenance and repair like any other infrastructure. Sidewalk maintenance and accessibility is a particular concern in Weldon, and any plan for a multimodal transportation network must also consider strategies for upkeep. The following section contains guidance on common maintenance issues for bicycle and pedestrian facilities, as well as strategies and best practices for addressing them.

COMMON MAINTENANCE ISSUES

Sidewalks are durable structures designed to withstand heavy use for several decades. However, over time, they can be affected by normal wear and tear and environmental conditions. In Downtown Weldon and the Weldon Historic District, numerous sidewalk segments are currently in need of repair due to abovementioned factors. Ignoring these maintenance concerns can lead to safety hazards and discourage pedestrians from walking to and from destinations. To address sidewalk maintenance needs effectively, the implementation of a regularly updated maintenance policy is crucial.

The proposed maintenance policy should include a comprehensive inventory of all sidewalks, clearly identifying the agency responsible for maintaining each section and noting their current condition. This approach will help the town prioritize repairs and allocate resources accordingly.

Sidewalk maintenance issues commonly fall into two categories: surfacing problems and structural problems. Surfacing problems, such as cracking or crumbling resulting from subpar installation or materials, might initially appear less severe but can worsen significantly over time. Structural problems often require repairing entire sections of the sidewalk. To facilitate the management of these issues, the following table outlines some common types of sidewalk problems and potential repair methods:

SIDEWALK CHALLENGE	DESCRIPTION	POTENTIAL REPAIR
Uplift, Tilt, or Settlement	Uplift, tilting, or settlement of sidewalks can occur due to thermal expansion or non-uniform subgrade compaction.	Grinding and horizontal cutting, wedging
Tensile Shrinkage	Tensile shrinkage of sidewalks can occur when the underlying soil shrinks due to decreased moisture content.	Mud jacking, sidewalk replacement, wedging
Sagging	Sagging of sidewalks can occur when the edge subsides at a greater rate than the center.	Crack sealing, grinding and horizontal cutting
Heaving	Heaving of sidewalks can occur when soils under the center swell, or due to tree roots.	Wedging
Spalling	Spalling of sidewalks can occur when a section of the sidewalk surface is broken out.	Patching, sidewalk replacement
Tree Root Damage	Sidewalks can be damaged by the growth of tree roots from trees near the sidewalk.	Grinding and horizontal cutting, wedging

By proactively implementing a maintenance policy and promptly addressing sidewalk issues, the Town of Weldon can enhance safety, encourage pedestrian activity, and project an image of a well-cared-for community. This approach contributes to a more inviting and pedestrian-friendly environment that benefits both residents and visitors.

ADA ACCESSIBILITY

Sidewalks in Weldon's Downtown and Historic District have ADA accessibility issues that make walking in the area challenging and pose safety hazards for individuals with disabilities. ADA adherence is not required when conducting regular maintenance and repair of pedestrian facilities but is required for any alterations or new construction. An alteration is found because of changes to the facility that could affect access, circulation, or use by individuals with disabilities, and often this threshold is met when changes to the adjacent roadway are made in conjunction with sidewalk replacement.

Downtown Weldon has significant accessibility concerns, notably related to traverse slope and overgrowth on sidewalks. Additionally, sections of Washington Ave present accessibility challenges as the sidewalk is at a different grade from the roadway, necessitating the use of steps for access from street parking. Additionally, utilities obstruct the pedestrian passageway, and several curb ramps do not meet ADA accessibility. Curb ramps are currently being replaced by NCDOT at 23 intersections in Weldon, concentrated south and west of downtown. In Downtown Weldon and other locations with pedestrian crossings, there are no pedestrian signals present, and some crosswalks are faded. Installing pedestrian signals with audible warnings and lead pedestrian intervals is critical to reduce the risk of pedestrian crashes, especially where crossing roadways with high-volume or high-speed traffic. Where faded or missing, crosswalks should be restriped to ensure visibility and safety of users.

If sidewalks deteriorate to the point where passage becomes hazardous, closure should be considered to reduce risk of pedestrian falls or injuries until maintenance can be performed. A detour should always be identified and marked along with a closure to provide alternative routes for pedestrians, but with the knowledge that pedestrians will often choose a faster, more efficient route even if it is less safe. Current sidewalk closures in downtown Weldon force pedestrians to walk around the block or risk their safety by stepping into the roadway to bypass the closure.

SEASONAL MAINTENANCE

Vegetation Removal + Control

Neglected sidewalks with minimal foot traffic can succumb to overgrowth. This commonly manifests as grass or moss sprouting within cracks and expansion joints or encroaching from the sidewalk's edges. Additionally, nearby trees might extend branches that obstruct the path for pedestrians. Accessibility guidelines prescribe trimming branches back if they protrude more than four inches into the pedestrian path or are less than 80 inches above the walkway surface. Root growth beneath the sidewalk can compromise its integrity, leading to problems like uplift or heaving. It is essential to address these issues promptly to maintain safe and accessible sidewalks.

To rectify these neglected sidewalks, several strategies can be employed, including weeding, branch trimming, and edge trimming. While these tasks may be time-consuming, they are relatively straightforward maintenance activities that can effectively restore the sidewalks to a safe and well-kept condition. Weldon subdivision regulations require a four-foot vegetative strip between the sidewalk and the curb, which is the responsibility of adjacent property owners to mow and maintain, but the Town should also incorporate overgrowth monitoring into a regular inspection routine.

Sweeping

Debris can accumulate on sidewalks, multi-use paths, and bicycle facilities if not carefully maintained. Many communities conduct street sweeping which may help clear on-street facilities, but sidewalk sweeping is less commonly maintained at a communitywide level, and many municipalities do not have equipment specialized for sidewalk sweeping. The responsibility for clearing leaves, dirt, sand, and other debris often falls on property owners or a business improvement district in a downtown or commercial area. Areas of persistent debris or drainage settlement should be incorporated into regular facility inventory inspections to identify sweeping needs or repairs which may address the scenario. Some types of debris pose a particular safety hazard, such as broken glass, and may require immediate attention. Often debris accumulation does not follow a predictable pattern and relies on reports from community members. Thus, it is essential to establish an effective user reporting system that allows community members to share maintenance concerns with government agencies responsible for their upkeep.



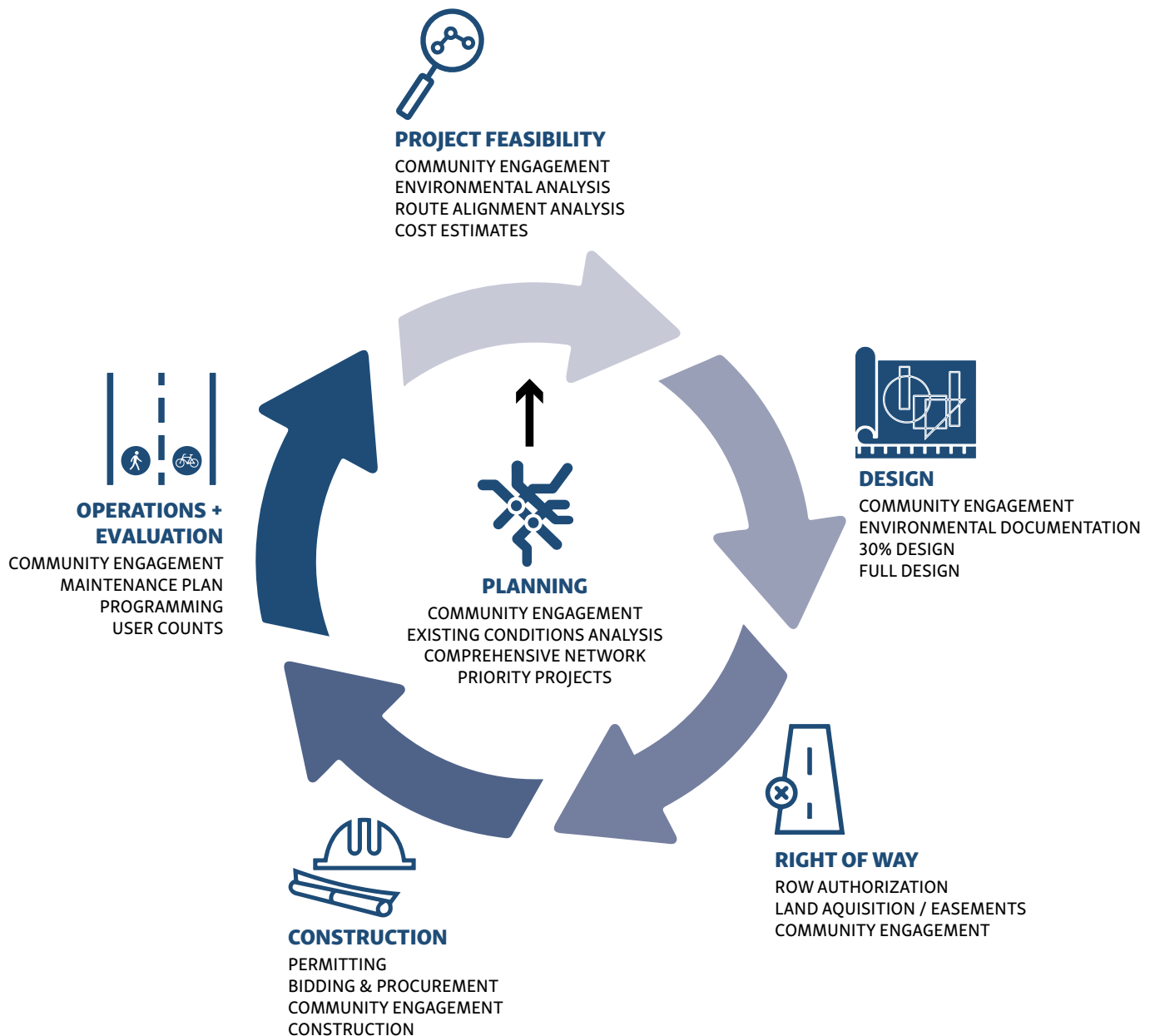
Snow and ice removal should be completed in a timely manner

Snow and Ice Removal

While winter weather is not typically a major concern in Weldon, the accumulation of snow and ice on bicycle and pedestrian facilities can make safe and accessible passage difficult and increase risk of injury. Additionally, roads are often plowed first following snowfall, pushing snow onto sidewalks, blocking crosswalks, and clogging drains. The clearing of sidewalks within the public right-of-way should be undertaken in a reasonable timeframe, often within the first 24 to 48 hours. Snow clearing policies may impose a specific time of day by which snow must be cleared from sidewalks or may prioritize the clearing of sidewalks in critical areas such as business districts, school zones, or transit stops. Clearing strategies may also vary depending on weather conditions; for example, communities in warmer climates may be able to rely on warmer temperatures following snowfall to melt snow and ice. Other communities use salt in advance of snowfall to help prevent ice from forming or spread sand to help maintain traction and mobility. Identifying and communicating responsibility for the clearing of snow and ice is another important component of a sidewalk maintenance program. Many communities require property owners to remove snow and ice from sidewalks and curb ramps adjacent to their property, allowing municipal crews to focus on other areas; however, this practice is not always effective as property owners may be physically unable to perform the work or may reside elsewhere. Ensuring the responsibility of sidewalk maintenance is clearly communicated is key to a successful snow and ice removal program, as well as providing enforcement when removal is not undertaken as required.

PROJECT DEVELOPMENT PROCESS

While the *Project Acceleration Plan* provides an overall framework for development of active transportation facilities, it is only the first step in a larger process. As a living document, recommendations and priorities outlined in the Plan may evolve with changing development pressures, funding opportunities, and community growth trends. Further, Weldon may need to conduct feasibility studies to understand the environmental conditions, routing challenges, and costs of priority corridors. This detailed analysis allows design and ROW acquisition to be finalized, which is followed by construction. Operational and programming plans are developed once the facility is constructed to determine maintenance and evaluation needs. The diagram below illustrates the typical planning, design, and construction process for active transportation facilities with key phases and individual tasks outlined.





Appendices



APPENDIX A:
Funding Resources

APPENDIX B:
Design Resources

APPENDIX C:
Additional Community
Engagement

APPENDIX D:
Cost Estimates

APPENDIX A: FUNDING RESOURCES

The Bipartisan Infrastructure Law (BIL) authorized \$1.2 trillion for transportation and infrastructure spending, with \$550 billion of that figure going toward new investments and programs. Additionally, the Inflation Reduction Act (IRA) provides \$700 billion in incentives, grants, and loans to support new infrastructure investments in the areas of clean energy, transportation, and the environment. These historic investments in infrastructure provide new and expanded opportunities for the planning, design, and construction of bicycle and pedestrian facilities in Weldon. The following Federal, state, local, and private funding programs have been instrumental in the successful development of bicycle and pedestrian networks in North Carolina communities. This section includes information on funding programs known at the time of publication; funding amounts, cycles, and programs may change over time. All discretionary grant programs are reimbursement-based unless otherwise stated.

FEDERAL FUNDING

North Carolina communities have partnered with Federal agencies to build multi-use paths, greenways, sidewalks, bike lanes, and improve crossings. The Federal government provides significant funding for active transportation facilities, which it distributes to municipalities through state agencies and Metropolitan Planning Organizations (MPO), Rural Planning Organizations (RPOs) as well as through discretionary grant programs.

The BIL authorizes transportation funding for highway, transit, rail, bicycle and pedestrian, and safety programs and infrastructure through fiscal year (FY) 2026. Federal Highway Administration (FHWA) administers BIL funding for surface transportation projects. FHWA distributes funding to the North Carolina Department of Transportation (NCDOT) and directly to MPOs through the Locally Administered Projects Program (LAPP). Communities wishing to access Federal funding must submit their candidate projects to their MPO or RPO, who then enter them into the NCDOT's Strategic Transportation Investment (STI) Mobility Formula. This formula ranks projects and identifies those for funding in the State Transportation Improvement Program (STIP). These funds require a 20% match from the municipality. Federal transportation funds for bicycle and pedestrian projects are primarily distributed through four programs: Transportation Alternatives (TA), Congestion Mitigation & Air Quality (CMAQ), Recreational Trails Program, (RTP), and Highway Safety Improvement Program (HSIP).

State and MPO Administered Federal Funding

Transportation funding, apportioned by Congress, using enabling legislation such as the former Fixing America's Surface Transportation Act (FAST Act) and current BIL, goes from the United States Department of Transportation (USDOT) and its departments to State DOTs and MPOs. Federal funding often follows a formula, which provides USDOT with a blueprint for distribution of funding amongst the states. States and MPOs must distribute allocated funds.

Transportation Alternatives (TA)

Transportation Alternatives (TA) provides Federal funds for community-based projects that expand travel choices and enhance the transportation experience by integrating modes and improving the cultural, historic, and environmental aspects of our transportation infrastructure. In North Carolina, NCDOT administers TA funds and allocates funding to Program-eligible projects through STI. NCDOT has created a bicycle and pedestrian scoping guidance document for local governments that have received TA funding. The Bike/Ped Project Scoping Guidance for Local Governments ([link below](#)) provides an overview of the four scoping tools used for locally managed, Federally-funded transportation projects in North Carolina. The document provides guidance on the project delivery process, scoping, identifying project risks, and project cost estimation.

- Total Funding: \$7.2 billion (FY22-26)
- Application Deadline/Cycle: Varies based on MPO/RPO administering funding
- Project Awards: minimum \$100,000
- Match Requirements: 20%
- Eligible Applicants: MPOs, RPOs, local governments, nonprofits, tribal governments, regional transportation authorities, transit agencies
- Eligible Projects: Construction/planning/design of on-road or off-road trails for bicyclists and pedestrians; infrastructure projects for improving non-driver access to public transportation and enhanced mobility; community improvement activities; refurbishment of historic transportation facilities such as the conversion and

use of abandoned railroad corridors for trails; recreational trails program; environmental mitigation; streetscape improvements; safe routes to school projects.

https://www.fhwa.dot.gov/environment/transportation_alternatives/

<https://connect.ncdot.gov/projects/BikePed/Documents/BikePed%20Project%20Scoping%20Guidance%20for%20Local%20Governments.pdf>

Recreational Trails Program (RTP)

The BIL reauthorized the RTP through FY22-26 as a set aside from the Transportation-Alternatives Set-Aside under the Surface Transportation Block Grant. The program funds state agencies to develop and maintain recreational trails and trail-related facilities for both non-motorized and motorized recreational trail uses. The allocation of the total RTP budget to each state is based on a formula, mandated by law, which apportions half of the total funds equally among all states with the other half distributed in proportion to the estimated amount of non-highway recreational fuel use in each state. In North Carolina, NCDOT's Division of Parks and Recreation and State Trails Program manages these funds with a goal of helping citizens, organizations, and agencies plan, develop and manage all types of trails ranging from greenways and trails for hiking, biking, and horseback riding to river trails and off-highway vehicle trails.

- Total Funding: \$1.49 million (FY23)
- Application Deadline/Cycle: Annual – February
- Project Awards: \$10,000-\$100,000
- Match Requirement: 25%
- Eligible Applicants: Local Governments, government agencies, nonprofit agencies
- Eligible Activities: New trail/greenway construction or renovation; trailhead or trail markers; purchase of tools to construct/renovate greenway; land acquisition for trails; planning, legal, environmental, and permitting costs (up to 10% of grant amount)

<https://trails.nc.gov/trail-grants/apply-grant>

Highway Safety Improvement Program (HSIP)

The purpose of the North Carolina Highway Safety Improvement Program (HSIP) is to provide a continuous and systematic procedure that identifies and reviews specific traffic safety concerns throughout the state. The goal of the HSIP process is to reduce the number of traffic crashes, injuries, and fatalities by reducing the potential for these incidents on public roadways. USDOT provides this core Federal aid program through annual allocations, which vary from year to year. States must spend at least 15% of HSIP funds on biking and walking safety when bicyclist and pedestrian fatalities are 15% or more of traffic fatalities. NCDOT uses a set of criteria called safety warrants to identify locations that need improvements. NCDOT then evaluates these locations, designated as potentially hazardous locations, using crash analyses, field investigations, and other tools to develop safety recommendations and implement countermeasures.

<https://safety.fhwa.dot.gov/hsip/reports/pdf/2020/nc.pdf>

<https://connect.ncdot.gov/resources/safety/Pages/NC-Highway-Safety-Program-and-Projects.aspx>

https://www.nhtsa.gov/sites/nhtsa.gov/files/2023-10/NC_FY24HSP-tag.pdf

North Carolina Land & Water Fund (NCLWF)

Created by the General Assembly in 1996, the NCLWF, formerly known as Clean Water Management Trust Fund, aims to conserve North Carolina's streams, rivers, and open space. This Environmental Protection Agency (EPA) funding, distributed through the North Carolina Department of Natural and Cultural Resources (NCDOT), land acquisition, stream restoration, stormwater, and planning projects that protect and conserve riparian buffers to provide environmental protection and establish a network of greenways for environmental, educational, or recreational uses. The NCLWF also funds mini grants of up to \$25,000 for donated property or the value of the conservation donation to pay transaction costs associated with the donation of property or a permanent conservation agreement. All NCLWF funded projects must sign a permanent conservation agreement.

- Application Deadline/Cycle: February - Annual
- Project Awards: up to \$500,000
- Match Requirements: 50%
- Eligible Applicants: North Carolina counties and incorporated municipalities

- Eligible Projects: Stream restoration, land acquisition, stormwater, and planning projects

<https://nclwf.nc.gov/apply>

Community Development Block Grant (CDBG)

The Community Development Block Grant Program, authorized by Title 1 of the Housing and Community Development Act of 1974, provides annual grants on a formula basis to states, cities, and counties to develop viable urban communities by providing decent housing, suitable living environments, and expanding economic opportunities for low- and moderate-income persons. CDGB funds are allocated at the Federal level by HUD and at the state level by the NC Department of Commerce. All municipalities are eligible to receive State CDBG funds except for entitlement communities, which receive funds directly from HUD.

https://www.hud.gov/program_offices/comm_planning/communitydevelopment

Carbon Reduction Program

The BIL establishes the Carbon Reduction Program (CRP), which provides funds for projects designed to reduce transportation emissions, defined as carbon dioxide (CO₂) emissions from on-road highway sources. Federal funds for the Carbon Reduction Program flow to NCDOT then through the MPOs/RPOs through a competitive call for projects. A state may transfer up to 50% of CRP funds to any other apportionment of the state including National Highway Performance, Program, Surface Transportation Block Grant Program, Highway Safety Improvement Program, Congestion Mitigation and Air Quality Improvement (CMAQ) Program, National Highway Freight Program, and Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT) Formula Program. Title 23 of US code requires each state to apportion 65% of CRP funds based on population and can choose how to obligate the remaining 35%.

- Total Funding: \$6.4 billion (FY22-26)
- Application Deadline/Cycle: March – Annual
- Project Awards: minimum \$100,000
- Match Requirements: 20%
- Eligible Applicants: Municipalities, county governments, and public transit agencies
- Eligible Projects: projects that reduce transportation emissions, including transportation alternatives such as the planning, design, and construction of on-road and off-road trail facilities

https://www.fhwa.dot.gov/bipartisan-infrastructure-law/crp_fact_sheet.cfm

https://eccog.org/wp-content/uploads/2023/02/Carbon-Reduction-Program_RPO_Presentation-CRP-Only.pdf

Discretionary Grants

The discretionary grant programs outlined in this section can provide millions of dollars towards the planning, design, and construction costs of bicycle and pedestrian facilities in Weldon. These funding programs are competitive and require project sponsors to submit forms, project narratives, and benefit-cost analyses (for construction projects); and encourage the submittal of supplemental materials and letters of support. The Federal funding through BIL emphasizes the importance of project alignment with USDOT priorities. Any Federal discretionary grant applications should consider how the project or components of the project align with the following USDOT priorities and strategic goals:

- **Equity:** USDOT aims to reduce inequities across transportation systems and the communities they affect, and to support and engage people and communities to promote safe, affordable, accessible, and multimodal access to opportunities and services while reducing transportation-related disparities, adverse community impacts, and health effects. Justice40 embodies this commitment to equity. Justice40 mandates that 40% of the overall benefits of certain Federal investments, including climate change and clean transportation, flow to disadvantaged communities that are marginalized, underserved, or overburdened by pollution.
- **Safety:** One of USDOT’s strategic goals for FY 22 to FY 26 is to make the transportation system safer for all people and advance a future without transportation-related serious injuries and fatalities. The National Roadway Safety Strategy (NRSS) outlines a comprehensive approach that communities can take to significantly reduce injuries and deaths on roadways, using a Safe System Approach that supports safer roads, safer speeds, safer people, safer vehicles, and post-crash care.
- **Climate and Sustainability:** Climate change presents a significant and growing risk to transportation infrastructure. USDOT is working to tackle the climate crisis by ensuring that transportation plays a significant

role in the solution. This includes substantially reducing greenhouse gas emissions and transportation-related pollution and building more resilient and sustainable transportation systems to benefit and protect communities.

- **Innovation and Transformation:** USDOT is investing in purpose-driven research and innovation to meet the challenges of the present and modernize a transportation system of the future that serves everyone today and in the decades to come.
- **Economic Strength and Global Competitiveness:** This priority aims to grow an inclusive and sustainable economy, invest in the transportation system to provide American workers and businesses reliable and efficient access to resources, markets, and good-paying jobs. One of the priorities of the Federal government is the promotion of diversity and inclusion in the workforce. Executive Order 14035 on Diversity, Equity, Inclusion, and Accessibility (DEIA) in the Federal Workforce provided agencies with information, resources, and a methodology to assess the state of DEIA and give a framework to promote DEIA practices.
- **Rural Opportunities to Use Transportation for Economic Success (ROUTES):** This initiative prioritizes the needs of rural America by supporting rural transportation policy and equitable access for communities that face challenges related to safety, mobility, and economic development, addressing disparities in rural transportation infrastructure by developing user-friendly tools and information, aggregating DOT resources, and providing direct technical assistance to better connect rural communities with funding, financing, and outreach resources available.

Reconnecting Communities and Neighborhoods (RCN)

In FY23, USDOT combined the Reconnecting Communities Pilot (RCP) and Neighborhood Access and Equity (NAE) programs into a single Notice of Funding Opportunity (NOFO) called Reconnecting Communities and Neighborhoods (RCN). This funding seeks to advance community-centered connection transportation projects, with a priority for projects that benefit disadvantaged communities, by improving access to daily needs such as jobs, education, health care, food, nature and recreation; fostering equitable development and restoration; and reconnecting communities by removing, retrofitting, or mitigating highways or other transportation facilities that create barriers to community connectivity, including to mobility, access, or economic development.

Project Awards:

- Total Funding \$1.785 billion (FY22-26):
 - » Capital Construction: \$1.15 billion (\$148 million in RCP and \$1 billion in NAE funds)
 - » Community Planning: \$185 million (\$50 million in RCP and \$135 million in NAE funds)
 - » Regional Partnerships Challenge: \$450 million (NAE funds)
- Application Deadline/Cycle: September - Annual
- Project Awards:
 - » RCP:
 - Planning: Up to \$2 million
 - Construction: Minimum of \$5 million
 - » NAE:
 - Planning: No min/max
 - Construction: No min/max
 - Regional Partnerships Challenge: No min/max – 3-5 projects will be awarded
- Match Requirements:
 - » RCP-Community Planning Grants: 20%
 - » RCP-Capital Construction Grants: 50% (other Federal funds may be used to bring the total Federal share up to a maximum of 80% of the total cost of the project)
 - » NAE-Community Planning, Capital Construction, and Regional Partnerships Challenge Grants: 20% (projects in disadvantaged or underserved community do not require a local match)
- Eligible Applicants:
 - » RCP Planning: State, local government, Tribal government, MPO, nonprofit organization
 - » RCP Capital Construction: Owner of facility; partnership between facility owner and applicant eligible for

RCP planning grants.

- » NAE Grants: state/local government, tribal government, MPO, nonprofit organization, special purpose district
- Eligible Projects: Facilities that divide or burden community; planning studies to assess feasibility of mitigating existing dividing facility or build capacity in disadvantaged area; pre-construction and construction activities for mitigating a burdening/dividing facility; improving complete streets/ multi-use trails/ regional greenways.
- Merit Criteria: Equity and Environmental Justice; Access; Facility Suitability; Community Engagement, and Community-based Stewardship, Management, and Partnerships; Equitable Development; Climate and Environment; and Workforce Development and Economic Opportunity.

<https://www.transportation.gov/grants/rcnprogram>

Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation Program (PROTECT)

The BIL established the Promoting Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT) Grant program to plan for and strengthen surface transportation to be more resilient to natural hazards, including climate change, sea level rise, flooding, extreme weather events, and other natural disasters to address the climate crisis through both formula and discretionary grant programs. USDOT seeks to fund projects through the discretionary program that: are grounded in the best available scientific understanding of climate change risks, impacts, and vulnerabilities; support the continued operation or rapid recovery of crucial local, regional, or national surface transportation facilities; utilize innovative and collaborative approaches to risk reduction, including the use nature-based solutions such as conservation, restoration, or construction of riparian and streambed treatments, marshes, wetlands, native vegetation, stormwater bioswales, breakwaters, reefs, dunes, parks, urban forests, and shade trees. The PROTECT discretionary program offers two types of awards: Planning Grants and Competitive Resilience Improvement Grants.

- Total Funding: \$848 million (FY 22 and 23)
- Application Deadline/Cycle: August – Annual
- Project Awards: minimum of \$500,000
- Match Requirements: N/A
- Eligible Applicants: States (or a political subdivision of a state, MPOs, local governments, special purpose districts or public authorities with a transportation function, Tribal governments, and Federal land management agencies (FLMAs), when applying jointly with states.
- Eligible Projects: highway, transit, and certain port projects that include resilience planning, strengthening, and protecting evacuation routes, enabling communities to address vulnerabilities, and increasing the resilience of surface transportation infrastructure from the impacts of sea level rise, flooding, wildfires, extreme weather events, and other natural disasters.
- Merit Criteria:
 - » Planning Grants: Program Alignment; Schedule and Budget; Public Engagement, Partnerships and Collaboration; Innovation
 - » Resilience Grants: Vulnerability and Risk; Criticality to Community’ Design Elements; Public Engagement, Partnerships and Collaboration; Equity and Justice⁴⁰; Climate Change and Sustainability; Schedule and Budget; Innovation

<https://www.transportation.gov/rural/grant-toolkit/promoting-resilient-operations-transformative-efficient-and-cost-saving>

Rebuilding American Infrastructure with Sustainability and Equity (RAISE)

This program, formerly known as TIGER and BUILD, and now as Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grants, awards funding to invest in road, rail, transit, and port projects that promise to achieve national objectives and have a significant local or regional impact. Since the eligibility requirements of RAISE allow project sponsors at the State and local levels to obtain funding for multi-modal, multi-jurisdictional projects that are more difficult to support through traditional DOT programs. Historically, annual appropriations largely funded this program, however the BIL included \$7.5 billion in supplemental funding between FY 22 and 26. Since the program’s inception, USDOT has awarded over \$14 billion to projects in all 50 states and several US territories.

- Total Funding: \$2.3 billion (FY 23) with set asides for Areas of Persistent Poverty and Historically Disadvantaged Communities
- Application Deadline/Cycle: February - Annual
- Project Awards:
 - » Planning Projects: No minimum
 - » Capital Projects: Minimum \$1 million (rural); minimum \$5 million (urban)
- Match Requirement: 20%– Federal share may increase in rural areas, Areas of Persistent Poverty, and Historically Disadvantaged Communities
- Eligible Applicants: State/local governments; multijurisdictional entity or group of entities; tribal government; transit agency.
- Eligible Projects: Capital projects (highway, intermodal, surface transportation, infrastructure, culvert replacement or stormwater runoff) and Planning (preparation for eligible surface transportation capital projects)
- Merit Criteria: Safety; Environmental Sustainability; Quality of Life; Mobility and Community Connectivity; Economic Competitiveness and Opportunity; State of Good Repair; Partnership and Collaboration; Innovation

<https://www.transportation.gov/RAISEgrants/about>

Active Transportation Infrastructure Investment Program (ATIIP)

Among the new discretionary grants created through the BIL is the Active Transportation Infrastructure Investment Program (ATIIP). This program will support the development of walking and biking infrastructure that connects to accessible, affordable, and safe active transportation networks, allowing people to reach destinations within a community and travel between communities. The BIL authorized \$200 million annually for the ATIIP, and of that \$45 million was allocated by the FY23 Omnibus Appropriations Act for the first round of funding. USDOT has not yet released the NOFO for ATIIP.

- Total Funding: \$45 million (FY23)
- Application Deadline/Cycle: TBD – Annual
- Project Awards: at least \$100,000 for Planning & Design; at least \$15 million for Construction grants
- Match Requirements: 20% or more
- Eligible Applicants: Local/regional government, state, tribe
- Eligible Projects: Projects that support active transportation networks connecting destinations.
- Merit Criteria: TBD

<https://www.railstotrails.org/policy/funding/active-transportation-infrastructure-investment-program/>

Bridge Investment Program (BIP)

This new funding source established through BIL awards funding to projects that improve bridge and culvert condition, safety, efficiency, and reliability. This funding source focuses on existing bridges to reduce the overall number of bridges in poor condition.

- Total Funding (FY23-26): \$9.62 billion
- Application Deadline/Cycle: November 27, 2023 – Annual
- Project Awards:
 - » Planning: no minimum/maximum
 - » Bridge: minimum of \$2.5 million
 - » Large Bridge: \$50 to \$100 million (at least \$100,000 for Planning & Design; at least \$15 million for Construction)
- Match Requirements: 20% (Bridge Projects); 50% (Large Bridge Projects)
- Eligible Applicants: Local/regional government, state, tribe
- Eligible Projects: Projects that support active transportation networks connecting destinations.
- Merit Criteria: State of Good Repair; Safety and Mobility; Economic Competitiveness and Opportunity; Climate Change, Sustainability, Resiliency, and the Environment; Equity and Quality of Life; and Innovation

<https://www.fhwa.dot.gov/bridge/bip/>

Safe Streets and Roads For All (SS4A) Grant Program

The BIL established this program, which funds regional, local, and tribal initiatives to prevent roadway deaths and serious injuries. The program supports the development of a comprehensive safety action plan (Action Plan) that identifies the most significant roadway safety concerns in a community and the implementation of projects and strategies to address roadway safety issues. Action Plans are the foundation of the SS4A grant program. SS4A requires an eligible Action Plan be in place before applying to implement projects and strategies. Applicants can either receive Planning or Demonstration Grants (to complete a safety action plan) or Implementation Grants (to implement projects/strategies identified in an Action Plan).

- Total Funding: \$5 billion (FY22-26)
- Application Deadline/Cycle: Annual – Fall
- Project Awards: \$100,000-\$10,000,000 (Planning & Demonstration Grants) and \$2,500,000-\$25,000,000 (Implementation Grants)
- Match Requirements: 20%
- Eligible Applicants: Counties, cities, towns, transit agencies, MPOs, tribal governments
- Eligible Projects:
 - » Planning and Demonstration Grants: Develop a comprehensive Action Plan; conduct supplemental safety planning to enhance an Action Plan; conduct demonstration activities to inform the development of, or an update to, an Action Plan
 - » Implementation Grants: Fund projects and strategies identified in an Action Plan; planning, design, and development activities for projects and strategies identified in Action Plan; must have an existing Action Plan to receive Implementation Grant funding.

A future SS4A Planning and Demonstration Grant and subsequent implementation grant could provide funding for a more in-depth analysis and strategy to solve safety issues along the corridor and implement countermeasures and other improvements.

<https://www.transportation.gov/grants/SS4A>

Rural and Tribal Assistance Pilot Program

The BIL created the Rural and Tribal Assistance Pilot Program, which makes \$10 million available over five years to provide states, local governments, and tribal governments in rural areas with funding to support early-stage development assistance for rural and tribal infrastructure projects. This program awards grants for either the hiring of staff or the procurement of expert firms to provide financial, technical, and legal assistance; assistance with development-phase activities; and information regarding innovative financing best practices and case studies. The first NOFO makes \$3.4 million available for the first two fiscal years to eligible applicants on a first-come, first-served basis.

- Total Funding: \$10 million (FY22-26)
- Application Deadline/Cycle: September – Annual
- Project Awards: up to \$360,000
- Match Requirements: N/A
- Eligible Applicants: State, local government, Federally recognized tribe
- Eligible Projects:
 - » Financial services, such as revenue forecasting and economic assessments
 - » Technical services, such as feasibility studies, environmental review and permitting, preliminary engineering and design, funding application assistance, and public engagement
 - » Legal services, such as statutory and regulatory framework analysis and procurement support
- Merit Criteria: Appropriateness of services requested; Viability of grant funds requested

<https://www.transportation.gov/buildamerica/RuralandTribalGrants>

National Endowment for the Arts Our Town Program

Our Town is the National Endowment for the Arts' creative placemaking grants program. Through project-based funding, the NEA supports projects that integrate arts, culture, and design activities into efforts that strengthen

communities by advancing local economic, physical, and/or social outcomes. These projects require a partnership between a local government entity and nonprofit organization, one of which must be a cultural organization; and should engage in partnership with other sectors (such as agriculture and food, economic development, education and youth, environment and energy, health, housing, public safety, transportation, and workforce development). Cost share/matching grants range from \$25,000 to \$150,000, with a minimum cost share/match equal to the grant amount.

<https://www.arts.gov/grants/our-town>

TECHNICAL ASSISTANCE

Building Blocks for Sustainable Communities

Local land use decisions, such as transportation options, housing type and location, stormwater management, and issues of equity, all have direct impacts on the health and environment of our communities. Founded in 2011, the Building Blocks for Sustainable Communities program collaborates with local communities across the US including tribes and territories, to develop smart growth solutions and strategies in ways that benefit human health and the environment. The program uses an inclusive and locally-led process that strengthens local capacity, facilitates partnerships, and creates a path forward to achieve community-identified goals.

EPA staff and EPA-hired consultant teams deliver the Building Blocks for Sustainable Communities technical assistance program. Each technical assistance project spans a period of roughly eight months, including inclusive public engagement through a two-day workshop, the involvement of relevant decision-makers and potential public and private sector partners, and a report outlining the workshop process and specific next steps generated that the community could take to achieve its goals. The workshop focuses on the issues and priorities determined by the community.

The application for Building Blocks consists of a two-page letter of interest that outlines the issue(s) the community wishes to address. The most recent call for letters closed in 2020.

<https://www.epa.gov/smartgrowth/building-blocks-sustainable-communities>

Greening America's Communities

Greening America's Communities (formerly known as Greening America's Capitals) is an EPA program to help cities and towns develop an implementable vision of environmentally friendly neighborhoods that incorporate innovative green infrastructure and other sustainable design strategies. EPA provides design assistance to help support sustainable communities that protect the environment, economy, and public health and to inspire local and state leaders to expand this work elsewhere.

EPA funds a team of designers to visit each community to produce schematic designs and exciting illustrations intended to catalyze or complement a larger planning process for the pilot neighborhood. Additionally, these pilots are often the testing ground for citywide actions, such as changes to local codes and ordinances to better support environmentally sustainable growth and green infrastructure. The design team and EPA, along with partners from other Federal agencies, also help city staff develop specific implementation strategies.

<https://www.epa.gov/smartgrowth/greening-americas-communities>

Rivers, Trails, and Conservation Assistance Program (RTCA)

The National Parks Service (NPS) Rivers, Trails, and Conservation Assistance (RTCA) Program supports community-led natural resource conservation and outdoor recreation projects across the nation. Although RTCA is not a traditional funding program, NPS staff provide planning, design and technical expertise for trails and outdoor recreation projects. Depending on the scale of the project, RTCA can invest up to four years of planning and project development assistance. Eligible entities include community groups, nonprofit organizations, tribes, and government agencies. Technical assistance services include:

- Define project vision and goals.
- Set priorities and build consensus.
- Inventory and map community resources.
- Identify funding strategies.
- Identify and analyze key issues and opportunities.
- Design community outreach, participation, and partnerships plans.

- Create project management and strategic action plans.
- Develop concept plans for trails, parks, and natural areas.

<https://www.nps.gov/orgs/rtca/index.htm>

STATE FUNDING

North Carolina communities have partnered with state agencies to build multi-use paths, greenways, sidewalks, bike lanes and improve crossings. NCDOT, North Carolina Department of Natural and Cultural Resources (NCDNCR), and North Carolina Department of Commerce (NCDOC) are the primary state agencies that fund bicycle and pedestrian planning, infrastructure, and programs. The North Carolina Department of Health and Human Services (DHHS), North Carolina Department of Environmental Quality (NCDEQ), and the North Carolina Department of Agriculture administer discretionary grant programs focusing on public health and community development, when funding is available.

Discretionary state grants promote alignment with both statewide goals and initiatives. While these are similar to Federal priorities and strategies, they are distinct to North Carolina and include the following:

- North Carolina Executive Order 80: North Carolina’s Commitment to Address Climate Change and Transition to a Clean Energy Economy: <https://governor.nc.gov/documents/files/executive-order-no-80-north-carolinas-commitment-address-climate-change-and-transition-clean-energy/open>
- North Carolina Executive Order 246: North Carolina’s Transformation to a Clean, Equitable Economy: <https://governor.nc.gov/executive-order-no-246/open>
- NC Moves 2050 Plan: <https://www.ncdot.gov/initiatives-policies/Transportation/nc-2050-plan/ncmoves2050/Pages/default.aspx>
- Great Trails State Plan: <https://www.ncdot.gov/divisions/integrated-mobility/multimodal-planning/great-trails-state/Pages/default.aspx>
- Statewide Transportation Improvements Program (STIP): <https://connect.ncdot.gov/projects/planning/pages/state-transportation-improvement-program.aspx>
- Strategic Highway Safety Plan (FY 24-FY 26): <https://www.nhtsa.gov/document/north-carolina-fy2024-2026-highway-safety-plan>
- NC Clean Transportation Plan: <https://www.ncdot.gov/initiatives-policies/environmental/climate-change/Pages/ncctp-executive-summary-final-report.aspx>

Great Trails Program Fund

The Great Trails State Program is a new program, authorized in September 2023 with the passage of the State Budget. The budget provided \$12.5 million each year for two years, distributed through a competitive grant program for new trail development and the extension of existing trails, including paved trails, greenways, natural surface trails for biking, hiking, walking, equestrian use, and paddling. NCDNCR will administer the program. The goal of this funding is to accelerate the completion of significant trail projects statewide.

- Total Funding: \$25 million
- Application Deadline/Cycle: Annual – TBD
- Project Awards: up to \$500,000
- Match Requirements: 25% flexible match (including cash, in-kind services, or donation of assets)
- Eligible Applicants: Municipalities/counties, regional governments, public authorities, nonprofit organizations
- Eligible Projects: Planning; design; environmental assessment or permitting and review; land or easement acquisition; trail construction; trail structures (bridges); trail amenities like trailhead parking, and signage; maintenance; matching funds for other Federal or non-state grants

Great Trails State Program funding could be used as a match for any Federal funding Weldon applies for, or to fund activities like environmental review and property acquisition. This would demonstrate commitment to the project on a local and state level, strengthening applications for competitive Federal funding.

<https://greattrailsstatecoalition.org/latest/#:~:text=The%20North%20Carolina%20General%20Assembly%27s,outdoor%20recreation%20in%20American%20history>

Complete the Trails Fund (CTF)

In 2021, legislation created the Complete the Trails Fund (CTF), which provided \$29.5 million for the 14 State Trails in NC, of which the Roanoke River State Trail is one. The 2023-2025 North Carolina Budget added \$5 million to the CTF. Non-profit partners must apply for and receive CTF funding to comply with program requirements. Eligible activities for these funds include land acquisition, capacity building, trail development, and small community grants. NCDOT administers this funding source.

<https://trails.nc.gov/state-trails/nc-complete-trails-program>

Parks and Recreation Trust Fund (PARTF)

Since 1994, the North Carolina parks and Recreation Trust Fund (PARTF) awards matching grants to local governments for parks, public beach access, and improvements to state parks. The program helps local governments reach their park and public access goals and improve the quality of life in their communities. Funding for PARTF is allocated annually, and the Parks and Recreation Authority, a citizen board appointed by the Governor, President Pro Tempore of NC Senate, and Speaker of the NC House of Representatives select grant recipients and allocate the funding. The projects must be located on a single site, and the applicant must own or have at least a 25-year signed lease or easement for the property where the PARTF facility will be located.

- Total Funding: \$8.5 million (FY24)
- Application Deadline: Annual – May
- Project Awards: up to \$500,000
- Match Requirements: 50%
- Eligible Applicants: North Carolina counties and incorporated municipalities. Public authorities, as defined by GS 159-7, are also eligible if they are authorized by N.C. general statutes to acquire land and develop recreation facilities for public use
- Eligible projects: Land acquisition; construction or renovation of facilities for a linear or non-linear public park; must be located on a single site and applicant must own or have at least a 25-year signed lease/easement for the property.

<https://www.ncparks.gov/about-us/grants/parks-and-recreation-trust-fund>

Powell Bill Funds

The primary purpose of Powell Bill funds is to resurface municipal streets, but municipalities can also use this program to plan, construct, and maintain bike paths, greenways, or sidewalks. Each municipality receives annual funding based on a formula set by NC General Statute 136-41.1 – 136-41.4 with 75% of the funds based on population and 25% based on the number of municipality-maintained street miles. Eligible uses of Powell Bill funds can include maintaining, repairing, constructing, reconstructing, or widening of any street or public thoroughfare including bridges, drainage, and curb and gutter.

<https://www.ncampo.org/documents/NCAMPO/Presentations2015/Toolkit-330/OverviewPowellBillProgram.pdf>

Strategic Transportation Investments (STI)

The Strategic Transportation Investments law, passed in 2013, establishes the Strategic Mobility Formula, which allocates available funding based on data-driven scoring and local input. NCDOT, working collaboratively with MPOs and RPOs, uses the Strategic Mobility Formula to develop the State Transportation Improvement Program (STIP), which identifies projects that will receive funding during a 10-year period. The STIP is state and federally mandated and updated by NCDOT every two years. The Strategic Mobility Formula groups projects in three categories: Division Needs, Regional Impact, and Statewide Mobility.

FUNDING CATEGORY	FUNDING DISTRIBUTION	OVERVIEW
Division Needs	30%	NCDOT's 14 transportation divisions share funding in this category equally. Project scores are based 50% on data and 50% on rankings by MPOs and RPOs and the NCDOT Divisions.
Regional Impact	30%	Projects on this level compete within regions made up of two NCDOT Divisions with funding based on population. Project scores are based 70% on data and 30% on rankings by MPOs and RPOs and the NCDOT Divisions.
Statewide Mobility	40%	Projects in this category are of statewide significance and are based 100% on data.

NCDOT programs independent bicycle and pedestrian projects in the Division Needs category. Eligible bicycle and pedestrian projects submitted for prioritization must be included in a locally adopted plan and have a minimum project cost of \$100,000. Eligible activities include right-of-way acquisition, design, and construction. Additionally, the Statewide Transportation Investments (STI) law prohibits the use of state funding for bicycle and pedestrian projects, requiring municipalities to provide the 20% match for Federally-funded projects.

Bicycle and Pedestrian STI Prioritization: Qualitative Scoring

Local input points represent 50% of the scoring for bicycle and pedestrian projects. MPOs and RPOs assign 25% of local input points, which are determined by municipal and county project priorities and public comment. NCDOT Division Engineers assign the remaining 50% of the local input points.

Bicycle and Pedestrian STI Prioritization: Quantitative Scoring

CRITERIA	MEASURE	DIVISION NEEDS (50%)
Safety	(Number of crashes x 40%) + (Crash severity x 20%) + (Safety risk x 20%) + (Safety benefit x 20%)	20%
Accessibility / Connectivity	Points of Interest pts + Connection pts + Route pts	15%
Demand / Density	# of households and employees per square mile near project	10%
Cost Effectiveness	(Safety + Accessibility / Connectivity + Demand / Density) / Cost to NCDOT	5%

Project Bundling

Project sponsors can bundle multiple bicycle and pedestrian projects to better compete with other projects submitted in the Division Needs category. NCDOT allows bundled projects across various geographies and project types. Projects do not have to be contiguous or related, and projects can be within a single municipality or across multiple jurisdictions. Bundled projects must be under one project manager, which must be a TAP eligible entity.

LOCAL FUNDING

While Federal funding can support high-cost projects, local funding is often ideal for supporting the delivery of specific components of greenway projects. Municipalities often plan for funding of bicycle and pedestrian facilities through their Capital Improvement Program (CIP). Local funding is often necessary to supplement Federal and state funding and can come in various forms.

Bonds

Municipalities and counties can propose bonds to protect open space corridors and build greenway networks, as has been seen in Wake County, City of Raleigh, City of Wilmington, Town of Chapel Hill, and City of Greenville. Successful bond campaigns require a well-defined plan with specific projects supported by the community. A community's public affairs department should organize bond campaigns and ensure coordination across all internal departments. Public outreach during the campaign is essential to educate residents about the benefits of infrastructure investment and to understand which projects garner the highest community support.

Developer Built Trails/In-Lieu Fees

North Carolina communities can require developers to dedicate land for greenways and recreational areas, open space, streets, and sidewalks through local land use and development ordinances. In-lieu fees are one-time fees that developers pay to municipalities instead of dedicating land for these purposes. These fees can then be used to fund larger capital and operational improvements. For example, the Town of Cary built its first greenway 40 years ago, now has over 80 miles of greenway trails, and requires developers to set aside important open space providing trail connectivity, wildlife habitat corridors, and water quality protection. Cary requires developers to dedicate land or make payment in-lieu of public park and/or greenway development to serve the recreational needs of residents.

Impact Fees

Impact fees represent financial payments made to a local government by a developer to fund certain off-site capital improvements needed to accommodate future growth. In North Carolina, local governments can impose impact fees on developers to offset the budgetary impact a new development places on public roads, schools, parks, recreational facilities, and water and sewerage. Many North Carolina communities impose impact fees for transportation, parks and recreation, and open space facility needs. For example, the City of Durham imposes transportation impact fees to fund for a portion of the costs for new streets and sidewalks, paving, grading, resurfacing, and widening of existing streets, traffic control signals and markings, lighting, and crosswalks. Durham uses development fees for open space and parks and recreation for the acquisition of park land and the provision of facilities, including athletic fields, parks, playgrounds, courts, recreation centers, shelters, stadiums, arenas, swimming pools, lighting, trail construction, and bike paths.

Public/Private Partnerships

Public private partnerships (P3s) are long-term contractual agreements between a public agency and a private entity to design, build, finance, operate and/or maintain infrastructure projects. This allows for greater private participation in the delivery of projects and allows private partners to assume risks for design, construction, finance, and long-term operation. P3s give public entities access to private capital, reduce costs to public entities, and can accelerate project delivery, encourage innovation, and allow partners to manage projects more efficiently. However, they require substantial up-front administrative costs and procurement may involve difficult financial and legal issues. P3s may not deliver the best value as compared to traditional or other alternative delivery methods.

Partnerships engender a spirit of cooperation, civic pride, and community participation. The key to the involvement of private partners is to make a compelling argument for their participation. For example, specific segments of a greenway may make critical connections to employment centers or potential partners' place of business, which would incentivize private participation in its design and construction. Furthermore, signage at trail heads or interpretive signage along greenway systems can incentivize private participation through name recognition for corporate partners. One example of a public/private partnership in North Carolina that has leveraged funding for trail construction is in Greensboro. The City of Greensboro is leading North Carolina in leveraging public-private partnerships to complete their Downtown Greenway Loop. Through the Action Greensboro Foundation, the project has raised over \$10 million in private funds by working with foundations and private givers. This money leverages over \$21 million in local and Federal funds.

Capital Improvement Program (CIP)

A Capital Improvement Program (CIP) is one element in a municipality's long-term planning process. It is a bridge between the municipality's Comprehensive Plan and short-term planning for infrastructure and operations. A Capital Improvement Program analyzes major facility and equipment needs, establishes priorities, estimates fiscal resources, and schedules the development of funded projects. For example, the City of Raleigh funds parks, greenways, and active transportation facilities through the city's Capital Improvement Program. The Parks, Recreation and Cultural Resources Department's CIP primary sources of funding come from Parks and Recreation Bonds, Facility Fees, General Fund (Tax Base), grants, and donations.

Municipal Service Districts (MSD)

Municipal Service Districts provide an equitable method for funding special improvements to public right-of-way areas because property owners share in the cost. For example, the Town of Morrisville uses Municipal Service Districts in several neighborhoods to perform pavement, curb and gutter, and sidewalk enhancements and repairs on the public streets throughout neighborhoods in the MSD.

PRIVATE FUNDING

Golden LEAF Foundation

The Golden LEAF Foundation is a nonprofit organization established in 1999 to receive a portion of North Carolina's funding received from the 1998 Master Settlement Agreement with cigarette manufacturers. Golden LEAF works to increase economic opportunity in North Carolina's rural and tobacco-dependent communities through leadership in grantmaking, collaboration, innovation, and stewardship as an independent and perpetual foundation. Golden LEAF's grantmaking focuses on the following priorities: Job creation and economic investment; workforce preparedness; agriculture; and community competitiveness, capacity, and vitality. Golden LEAF has two standard programs open to eligible entities seeking grants: Open Grants Program and Economic Catalyst Program. These programs complement other ongoing initiatives of the Foundation, such as the Community-Based Grants Initiative.

- **Open Grants Program:** The Open Grants Program process is open to all governmental entities and 501(c)(3) organizations that propose projects in Golden LEAF's priority areas. This program is for economic development projects aligned with the Golden LEAF priority areas. Most awards in the Open Grants Program will be for \$200,000 or less.
- **Economic Catalyst Program:** The Economic Catalyst process is open to governmental entities and 501(c)(3) organizations with projects that will create jobs at risk without Golden LEAF funding. Grants include funds for public infrastructure, job training, upfit for buildings owned by governmental or nonprofit entities, or equipment acquisition where the building or equipment will be leased or sold at fair-market value to a company creating jobs. Grants are available only for projects that include a specific company's commitment to create full-time jobs in NC.
- **Community-Based Grants Initiative:** Each year, the Golden LEAF Foundation invites organizations from counties from a different Prosperity Zone to participate in the Community-Based Grant Initiative (CBGI). The process is competitive, but organizations from all counties within the Prosperity Zone will have an opportunity to apply. The CBGI identifies projects with the potential to have a significant impact. It is a focused process with grants targeted toward investments in the building blocks of economic growth. Projects must address economic development, agriculture, workforce preparedness, infrastructure, and capital costs necessary to create health care jobs. County managers serve a key role in the process. Each county manager will submit a slate of up to four projects for consideration. Applicants must be 501(c)(3) organizations or governmental entities, such as county and municipal governments, community colleges, or universities. Golden Leaf limits awards to no more than three projects per county and will total no more than \$1.5 million per county.

Golden LEAF funding can assist with funding infrastructure costs for projects in Weldon that connect to employment opportunities.

<https://www.goldenleaf.org/>

National Association of Realtors Placemaking Grants

The National Association of Realtors (NAR) funds placemaking and smart growth grants to make communities better places to live by transforming unused or underutilized sites into welcoming destinations accessible to everyone in a community.

- **Smart Growth Grants:** Smart Growth grants can fund visioning sessions, community workshops, and placemaking visioning for meaningful transportation projects and issues. These community planning activities would align with the intent of the *Project Acceleration Plan*.
- **Placemaking Grants:** Placemaking Grants fund the creation of new, outdoor public spaces and destinations in a community. This program funds amenities such as street furniture, paint, signage, materials, landscaping, murals, site preparation, and artist fees. A state or local REALTOR® association must submit applications, and grants provide up to \$5,000 per award.

<https://realtorparty.realtor/community-outreach/>

People for Bikes Community Grant Program

The People For Bikes Community Grant Program supports bicycle infrastructure projects and targeted advocacy initiatives that make it easier and safer for people of all ages and abilities to ride. People For Bikes accepts grant applications from non-profit organizations with a focus on bicycling, active transportation, or community development; city or county agencies or departments, and state or Federal agencies working locally. People For Bikes focuses most grant funds on bicycle infrastructure projects, such as bike paths, lanes, trails and bridges, mountain bike facilities, bike parks and pump tracks, BMX facilities, and end-of-trip facilities such as bike racks, bike parking, bike repair stations and bike storage. Advocacy projects funded through the program include open street events and campaigns to increase investment in bicycle infrastructure. People For Bikes accepts requests for funding up to \$10,000. People For Bikes does not require a specific percentage match, but they will not consider requests in which the grant funding would amount to 50% or more of the project budget.

<https://www.peopleforbikes.org/grant-guidelines>

Two for the Trails (Athletic Brewing Company)

Athletic Brewing Company provides funding to protect and restore trails, waterways, beaches, parks, and urban areas in need of maintenance.

- Total Funding: \$2 million annually
- Grant Deadline/Cycle: Summer – Annual
- Project Awards: up to \$50,000
- Match Requirements: 20%
- Eligible Applicants: Any registered LLC with an environmental cleanup project.
- Eligible Projects: Projects that restore trails and outdoor recreation facilities.

<https://athleticbrewing.com/pages/two-for-the-trails-application>

AARP Community Challenge

The AARP Community Challenge provides small grants to fund quick-action projects that can help communities become more livable for people of all ages. AARP accepts applications for projects that improve public spaces, housing, transportation, civic engagement, coronavirus recovery, diversity, and inclusion, and more. Project types include those that provide permanent physical improvements in the community, temporary demonstrations that lead to long-term change, and innovative programming or services. The program is open to 501(C)(3), 501(C)(4) and 501(c)(6) nonprofits and government entities. Grants can range from several hundred dollars for smaller, short-term activities to several thousand or tens of thousands of dollars for larger projects.

- Total Funding: \$3.6 million
- Grant Deadline/Cycle: March – Annual
- Project Awards: \$500 to \$50,000
- Match Requirements: N/A
- Eligible Applicants: Governments and nonprofit organizations
- Eligible Projects: Projects that improve public spaces, transportation, and inclusion that provide permanent physical improvements in the community and innovative programming or services.

Additionally, AARP has introduced a Capacity Building Microgrant program, which provides \$2,500 to either conduct a walk audit to enhance pedestrian safety and walkability or to start/grow a community garden. This funding opportunity is better suited for incremental improvements after the construction of a greenway project.

<https://www.aarp.org/livable-communities/community-challenge/>

APPENDIX B: DESIGN RESOURCES

Below are several design resources that can be used to inform design decisions for bicycle and pedestrian facilities. Organizations such as Federal Highway Administration (FHWA), American Association of State Highway and Transportation Officials (AASHTO), National Association of City Transportation Officials (NACTO), and North Carolina Department of Transportation (NCDOT) offer general guidelines and project-specific tools to help professionals make design decisions. These guidelines promote flexibility to ensure context-sensitive applications.

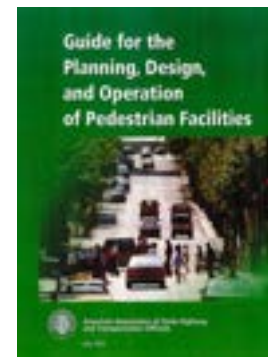
AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES

The AASHTO Guide for the Development of Bicycle Facilities is the authoritative national standard for bikeway design. The document provides guidance to designers and planners by referencing a recommended range of design values and describing alternative design approaches. The guide provides information on how to accommodate bicycle travel and operations in most environments. Sufficient flexibility is permitted to encourage designs that are sensitive to local context and incorporate the needs of bicyclists, pedestrians, and motorists. https://nacto.org/wp-content/uploads/2015/04/AASHTO_Bicycle-Facilities-Guide_2012-toc.pdf



AASHTO GUIDE FOR THE PLANNING, DESIGN AND OPERATION OF PEDESTRIAN FACILITIES

The AASHTO Guide for the Planning, Design, and Operation of Pedestrian Facilities provides guidance for pedestrian facilities along streets and highways. The primary audiences for this manual are planners, roadway designers, and transportation engineers, whom make decisions on a daily basis that affect pedestrians. The guide focuses on identifying effective measures for accommodating pedestrians on public rights-of-way, and it recognizes the effect that land use planning and site design have on pedestrian mobility and addresses these topics as well. [https://onlinepubs.trb.org/onlinepubs/nchrp/docs/NCHRP20-07\(263\)_FR.pdf](https://onlinepubs.trb.org/onlinepubs/nchrp/docs/NCHRP20-07(263)_FR.pdf)



MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)

The Manual on Uniform Traffic Control Devices for Streets and Highways, or MUTCD defines the standards used by road managers nationwide to install and maintain traffic control devices on all public streets, highways, bikeways, and private roads open to public travel. The MUTCD is published by the Federal Highway Administration (FHWA) and is a compilation of national standards for all traffic control devices, including road markings, roadway signs, and traffic signals. <https://mutcd.fhwa.dot.gov/>



NCDOT ROADWAY DESIGN GUIDE

The North Carolina Department of Transportation (NCDOT) Roadway Design Guide defines standards for roadways owned and maintained by NCDOT, including typical sections for roadways. Typical sections establish design elements that emphasize safety, mobility, complete streets, and accessibility for multiple modes of travel. Typical sections also provide guidelines for comprehensive transportation planning, project planning, and project design activities. <https://connect.ncdot.gov/projects/Roadway/pages/roadway-design-manual.aspx>



NCDOT COMPLETE STREETS IMPLEMENTATION GUIDANCE

The North Carolina Department of Transportation (NCDOT) Complete Streets Implementation Guide is designed to assist NCDOT staff engineers, project managers and designers in implementing the Complete Streets Policy adopted by the Board of Transportation in August 2019. This document provides comprehensive guidance for incorporating a complete streets approach into NCDOT's planning, programming, design, and maintenance processes.

<https://connect.ncdot.gov/projects/BikePed/Pages/Complete-Streets.aspx>



NACTO URBAN STREETS DESIGN GUIDE

The Urban Street Design Guide charts the principles and practices of the nation's foremost engineers, planners, and designers working in cities today. A blueprint for designing 21st century streets, the guide unveils the toolbox and the tactics cities use to make streets safer, more livable, and more economically vibrant. The Guide outlines both a clear vision for complete streets and a basic road map for how to bring them to fruition.

<https://nacto.org/publication/urban-street-design-guide/>



NACTO URBAN BIKEWAY DESIGN GUIDE

The NACTO Urban Bikeway Design Guide provides cities with state-of-the-practice solutions that can help create complete streets that are safe and enjoyable for bicyclists. Design treatments included in the guide offer required, recommended, and optional design elements to address the complexity of individual streetscape situations. In August 2013, the FHWA issued a memorandum officially supporting the use of this document. All of the NACTO Urban Bikeway Design Guide treatments are in use internationally and in many cities around the US.

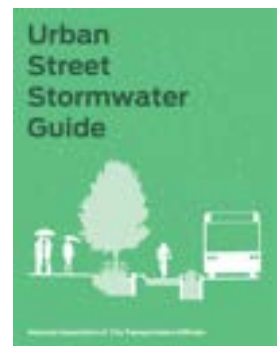
<https://nacto.org/publication/urban-bikeway-design-guide/>



NACTO URBAN STREET STORMWATER GUIDE

The Urban Street Stormwater Guide advances the discussion about how to design and construct sustainable streets. The guide provides cities with national best practices for sustainable stormwater management in the public right-of-way, including core principles about the purpose of streets, strategies for building inter-departmental partnerships around sustainable infrastructure, technical design details for siting and building bioretention facilities, and a visual language for communicating the benefits of such projects. The guide sheds light on effective policy and programmatic approaches to starting and scaling up green infrastructure, provides insight on innovative street design strategies, and proposes a framework for measuring performance of streets comprehensively.

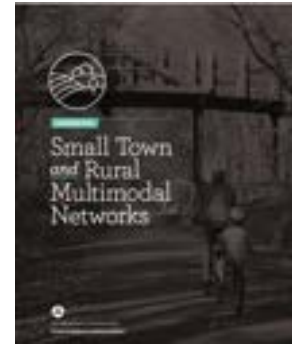
<https://nacto.org/publication/urban-street-stormwater-guide/>



FHWA SMALL TOWN & RURAL MULTIMODAL NETWORKS

The Federal Highway Administration (FHWA) Small Town and Rural Multimodal Networks applies existing national design guidelines in a rural setting and highlights small town and rural case studies. It addresses challenges that are specific to rural areas and focuses on opportunities to make improvements despite the geographic, fiscal, and other challenges that many rural communities face. It also includes several design concepts applicable to National Scenic and Historic Trails.

https://www.fhwa.dot.gov/environment/bicycle_pedestrian/publications/small_towns/



FHWA BIKEWAY SELECTION GUIDE

A resource to help transportation practitioners consider the trade-offs relating to the selection of bikeway types. The document builds upon other FHWA resources that promote design flexibility and support connected, safe, and comfortable bicycle networks. The Bikeway Selection Guide outlines a process for identifying the desired bikeway type and assessing and refining potential options based on real-world conditions and decision-making factors. This process is intended to accelerate the delivery of high-quality multimodal projects that improve safety for everyone and meet the transportation needs of people of all ages and abilities.

<https://highways.dot.gov/sites/fhwa.dot.gov/files/2022-07/fhwasa18077.pdf>



FHWA SEPARATED BIKE LANE PLANNING AND DESIGN GUIDE

The Separated Bike Lane Planning and Design Guide outlines planning considerations for separated bike lanes and provides a menu of design options covering typical one and two-way scenarios. It highlights different options for providing separation, while also documenting intersection treatments and mid-block design considerations for driveways, transit stops, accessible parking, and loading zones. Case studies within the guide highlight best practices and lessons learned.

https://nacto.org/wp-content/uploads/2016/05/2-4_FHWA-Separated-Bike-Lane-Guide-ch-5_2014.pdf



PENNSYLVANIA TRAIL DESIGN & DEVELOPMENT PRINCIPLES: GUIDELINE FOR SUSTAINABLE, NON-MOTORIZED TRAILS

This document compiles the best practices and guidelines for the planning, design, construction, and management of trails. Techniques are presented for developing trails that create desirable and enjoyable experiences for trail users. These techniques employ sustainable design elements and construction practices that allow the trail to make use of natural systems so that the trail remains both physically and environmentally sustainable.

<https://www.americantrails.org/resources/pennsylvania-trail-design-development-principles>



BEST PRACTICES IN TRAIL MAINTENANCE: A MANUAL BY THE OHIO RIVER GREENWAY

This manual is intended for practical use by trail maintenance managers. It will also be useful for policy makers who are tasked with anticipating and planning for maintenance budget and personnel needs. The recommendations included in this manual were chosen, in part, to facilitate widespread adoption by other trail operators. They are cost-effective solutions that require minimal technological or financial commitments. It also provides guidance for trail construction and design where a maintenance issue can best be resolved by constructing new trail segments.

<https://www.railstotrails.org/resourcehandler.ashx?name=best-practices-in-trail-maintenance-a-manual-by-the-ohio-river-greenway&id=21221&fileName=Best%20Practices%20in%20Trail%20Maintenance.pdf>



FHWA ACHIEVING MULTIMODAL NETWORKS: APPLYING DESIGN FLEXIBILITY AND REDUCING CONFLICTS

This publication is resource for practitioners seeking to build multimodal transportation networks. It highlights ways that planners and designers can apply the design flexibility found in current national design guidance to address common roadway design challenges and barriers. It focuses on reducing multimodal conflicts and achieving connected networks so that walking and bicycling are safe, comfortable, and attractive options for people of all ages and abilities.

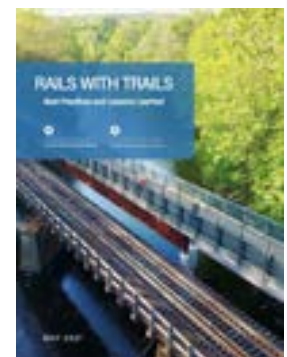
https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/



RAILS TO TRAILS CONSERVANCY (RTC) RAIL WITH TRAILS: BEST PRACTICES AND LESSONS LEARNED

This updated USDOT Rails-with-Trails: Lessons Learned report documents how the state of the practice, perspectives, and context for rails-with-trails have evolved since the first report in 2002 and includes updated effective practices. Best practices are based on extensive research into existing and planned rails with-trails that involved interviews with railroad officials and trail managers; a literature review of previous rail-with-trail studies; a review of trail planning guidance documents; and input from various railroad and trail professionals.

https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/



ADA STANDARDS FOR ACCESSIBLE DESIGN

This guide explains requirements in the current editions of the Americans with Disabilities Act (ADA) Standards issued by the Department of Justice (DOJ) and the Department of Transportation (DOT). It provides the scoping and technical requirements for new construction and alterations resulting from the adoption of revised 2010 Standards in the final rules for Title II and Title III.

https://www.ada.gov/2010ADASTandards_index.htm



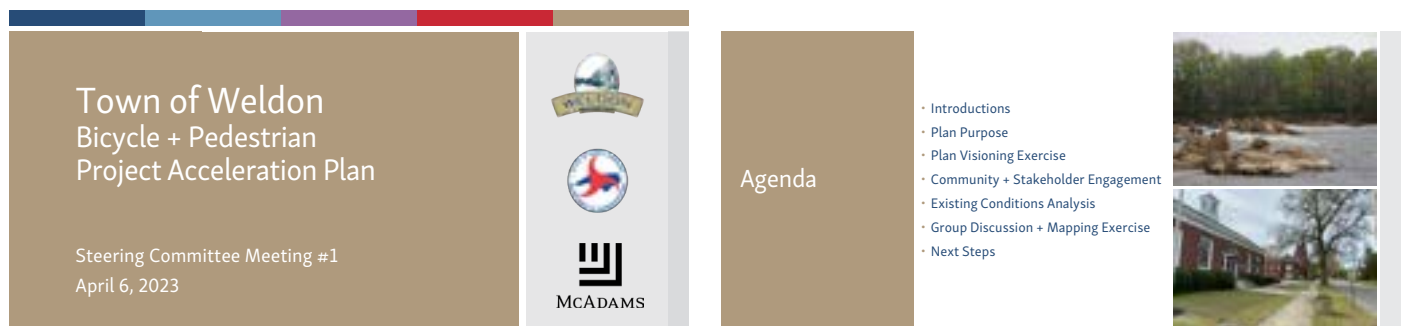
APPENDIX C: ADDITIONAL COMMUNITY ENGAGEMENT

Included on the following pages are materials from community engagement efforts throughout the duration of the project.

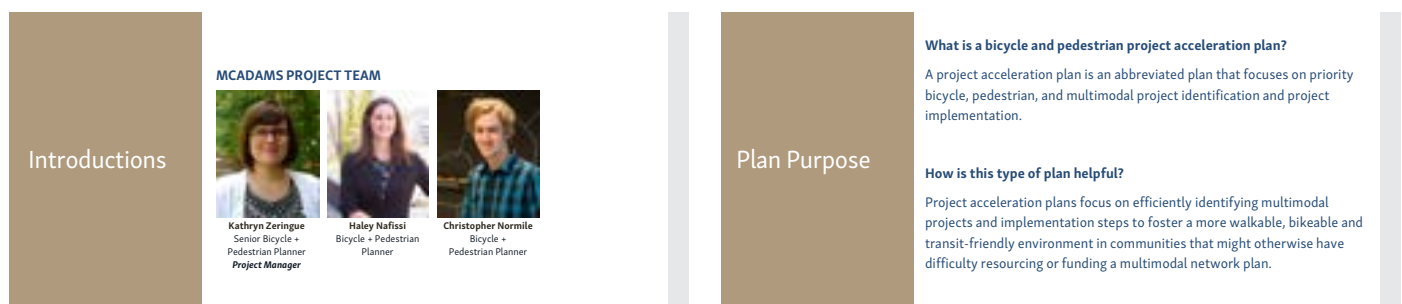
Attachments:

- Steering Committee Meeting #1
- Steering Committee Meeting #2
- Steering Committee Meeting #3
- Public Meeting
- Community Survey

STEERING COMMITTEE MEETING #1



The slide features a title bar with a color gradient. The main content area is divided into three sections. The left section is a brown box containing the text: "Town of Weldon Bicycle + Pedestrian Project Acceleration Plan", "Steering Committee Meeting #1", and "April 6, 2023". The middle section is a white box with logos for Weldon, a red star, and MCADAMS. The right section is a brown box with the word "Agenda" and a list of items: "Introductions", "Plan Purpose", "Plan Visioning Exercise", "Community + Stakeholder Engagement", "Existing Conditions Analysis", "Group Discussion + Mapping Exercise", and "Next Steps". To the right of the agenda list are two small images: a river with rocks and a brick building.



The slide is divided into two main sections. The left section is a brown box with the word "Introductions". The right section is a white box with the following content: "MCADAMS PROJECT TEAM" followed by three headshots of team members: Kathryn Zeringue (Senior Bicycle + Pedestrian Planner, Project Manager), Haley Nafissi (Bicycle + Pedestrian Planner), and Christopher Normile (Bicycle + Pedestrian Planner). Below the headshots is a brown box with the text "Plan Purpose". To the right of this box is a white box containing the text: "What is a bicycle and pedestrian project acceleration plan?" followed by a definition: "A project acceleration plan is an abbreviated plan that focuses on priority bicycle, pedestrian, and multimodal project identification and project implementation." Below this is the text: "How is this type of plan helpful?" followed by a paragraph: "Project acceleration plans focus on efficiently identifying multimodal projects and implementation steps to foster a more walkable, bikeable and transit-friendly environment in communities that might otherwise have difficulty resourcing or funding a multimodal network plan."

Plan Purpose

Where do people want to bike and walk in Weldon?

Identify key locations people want and/or need to reach by biking and walking.

How do we connect to these places?

Identify corridors and appropriate facility types to connect key locations.

How do we build Weldon's bicycle + pedestrian network?

Identify priority projects, implementation strategies, and funding opportunities.

How do we build a bicycle + pedestrian friendly community in Weldon?

Identify policies and programs to promote biking and walking.

Bicycle + Pedestrian Facility Types

Greenway

A greenway or shared use path provides an area separate from motorized traffic for bicyclists, pedestrians, wheelchair users, skaters, joggers, etc.

Greenways offer network connectivity opportunities beyond the roadway network, as they are often located along streams, in utility corridors, and parks.



Bicycle + Pedestrian Facility Types

Sidepath

A sidepath is a bidirectional shared use path adjacent and parallel to a roadway.

Sidepaths offer a low-stress experience for bicyclists and pedestrians along network routes with high-speed or high-volume traffic.



Bicycle + Pedestrian Facility Types

Sidewalk

A sidewalk is a bi-directional path that provides a dedicated travel area for pedestrians in the transportation network.

Sidewalks are physically separated from the roadway by a curb or unpaved buffer space.



Bicycle + Pedestrian Facility Types

Separated Bicycle Lane

A separated bicycle lane is a facility for exclusive use by bicyclists that is located within or directly adjacent to the roadway and is physically separated from motor vehicle traffic by a buffered space with a vertical separation element.

Separated bicycle lanes can be designed to accommodate one-way or bi-directional bicycle travel.



Bicycle + Pedestrian Facility Types

Buffered Bicycle Lane / Bicycle Lane

A buffered bicycle lane designates travel space for bicyclists in the roadway through use of pavement markings and a 1.5-4' buffer separating the bicycle lane from the adjacent travel lane.

A bicycle lane designates a travel space for bicyclists in roadway in the absence of more separated facilities.



Bicycle + Pedestrian Facility Types

Bicycle Boulevard / Shared Lane Markings

A bicycle boulevard is a low-stress, shared street accommodation to offer priority for bicyclists operating within a roadway shared with motor vehicle traffic.

Bicycle boulevards use a combination of pavement markings (shared lane markings), signage, and optional traffic calming measures to allow bicyclists to comfortably travel along a roadway.



Plan Visioning Exercise



Discussion:
"What does success look like for Weldon's Project Acceleration Plan?"

Plan Visioning Exercise

1. **Click the link in the chat:**
<https://app.conceptboard.com/board/p3yt-mnmb-yz7k-px8i-343d>
2. **Select Guest Access**
3. **Select a "Post-it Note" and share your thoughts on the board**



Project Schedule + Engagement Strategies



Steering Committee

Role:

The Steering Committee will attend three (3) meetings where they will provide support, guidance, and oversight for progress for the Plan.

- Committee Meeting #1: Plan Vision, Goals, and Existing Conditions (Today)
- Committee Meeting #2: Draft Recommendations (June/July 2023)
- Committee Meeting #3: Draft Plan Review (August/September 2023)



Community + Stakeholder Involvement

- Community Survey
 - Existing Conditions, Opportunities + Constraints, Destinations (March-May 2023)
- Public Meeting
 - Project Overview, Draft Recommendations, and Priority Projects (May 2023)
- Community + Stakeholder Review of Draft Plan
 - Draft Plan Review (September 2023)



Demographics

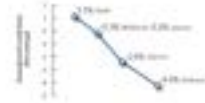
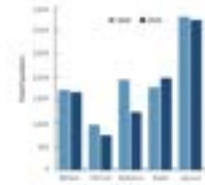
This demographic analysis informs the community engagement approach and helps to ensure recommendations meet the diverse needs of people residing in Weldon.

- Population
- Race + ethnicity
- Age
- Education
- Residents Living Below Poverty Level
- Income
- Commute
- Vehicle Access
- Employment Density

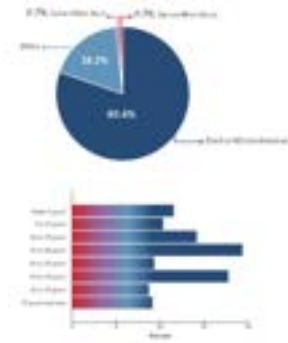
Source: US Census Bureau, 2016-2020 American Community Survey 5-year estimates.



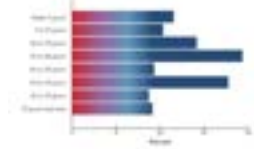
Population



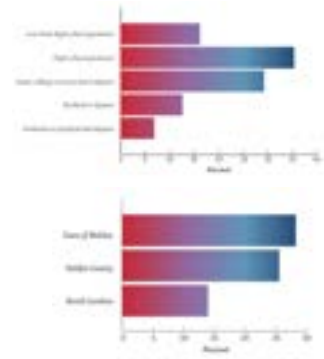
Race + Ethnicity



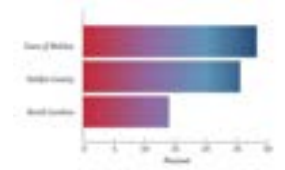
Age



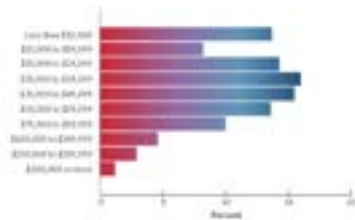
Education



Residents Living Below Poverty Level



Income



Commute Modes



Vehicle Access



Employment Density



Previous Plan + Policy Review

- **Town of Weldon Downtown Façade Improvement Program (2022)**
 - Encourages investment in downtown storefronts, including sidewalks, landscaping, and other improvements to the pedestrian experience.
- **Town of Weldon Land Development Plan (2006)**
 - Envisions an expanded active transportation network with greenways connecting Roanoke Canal Trail to River Falls Park, adding sidewalks to major corridors such as US-158, and improving landscaping and lighting along roadways.
- **Roanoke Rapids Urban Area Comprehensive Transportation Plan (2008)**
 - Recommends bicycle improvements on US-158, US-301, and W 2nd St, and sidewalks along roads including US-158, W C Rivers Dr, E 3rd St, E 5th St, Washington Ave, Country Club Rd, and County Rd. Also recommends improvements and maintenance to existing sidewalks.
- **Town of Weldon Zoning Ordinance (2008)**
 - Requires site plans to include sidewalks, trails, and pedestrian paths, and to reserve a portion of open space for active recreation use.
- **Town of Weldon Subdivision Regulations (2009)**
 - Requires sidewalks be 5ft in width with a 4ft planting strip and on both sides of the street in all zoning districts but R-20 (one side required) and R-40 (not required). Crosswalks are required where considered necessary by the Planning Board.

Study Area



Existing + Proposed Bicycle and Pedestrian Facilities



Existing Conditions: Downtown



Existing Conditions: Existing Trail Facilities



Bicycle Crashes (2007-2021)



Pedestrian Crashes (2007-2021)



Existing Conditions: Safety Concerns



Existing Conditions: Barriers



NCDOT Traffic Volume (AADT)



NCDOT Speed Limits



NCDOT
Right-of-Way



NCDOT
STIP Projects
(2020-2029)



NCDOT
HMIP Projects
(2023-2027)



Floodplains +
Wetlands



Topography



Group
Discussion +
Mapping
Assessment

1. Click the link in the chat:
<https://app.conceptboard.com/board/p3yt-mnmb-yz7k-px8i-343d>
2. Select Guest Access
3. Select a "Post-it Note" and share your thoughts on the board



Next Steps + Questions

Next Steps

- Online Survey Launch (April)
- Survey Analysis (Mid-Late May)
- Draft Recommendations (May/June)


Questions?

Kathryn Zeringue,
Senior Bicycle + Pedestrian Planner
zeringue@mcadamsco.com



Town of Weldon Bicycle + Pedestrian Project Acceleration Plan

Steering Committee Meeting #2
August 10, 2023



Agenda

- Project Schedule Review
- Community Survey Results
- Proposed Plan Goals
- Opportunities + Constraints
- Proposed Bicycle and Pedestrian Network
- Proposed Intersection Treatments
- Proposed Program and Policy Recommendations
- Draft Prioritization Criteria and Methodology
- Next Steps



Project Schedule



Community Survey Results



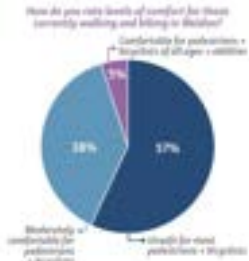
Community Survey Results



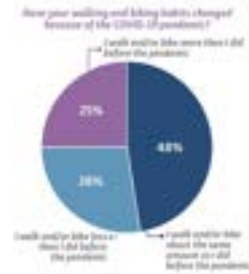
Community Survey Results



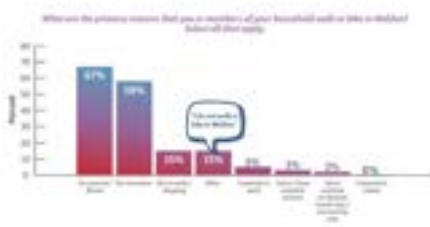
Community Survey Results



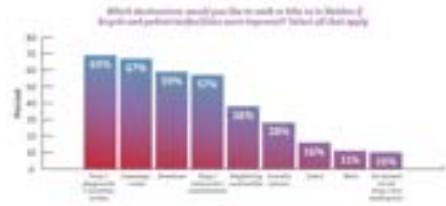
Community Survey Results



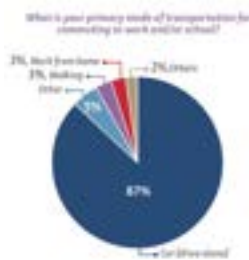
Community Survey Results



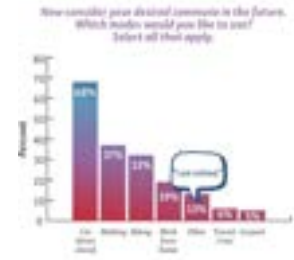
Community Survey Results



Community Survey Results



Community Survey Results



Existing Bicycle and Pedestrian Network's Opportunities and Constraints

1. Connections to Schools
2. Connections to River Falls Park
3. Connections to Halifax Community College
4. Connections to Roanoke Rapids Regional Connections / Great Trails State Corridor
5. Roanoke Canal Trail
6. Roanoke River
7. Downtown Weldon
8. Connections to South Weldon
9. Washington Ave
10. Weldon Historic District
11. Chockoyotte Creek
12. Railroad Corridors
13. Interstate 95 Corridor
14. US-158 Corridor
15. US-301 Corridor
16. Country Club Rd Corridor
17. Sidewalk Maintenance / ADA Accessibility Improvements



Proposed Typical Sections

Sidewalk

A sidewalk is a bi-directional path that provides a dedicated travel area for pedestrians in the transportation network. Sidewalks are physically separated from the roadway by a curb or unpaired buffer space.



Example: 1st St in Weldon

Proposed Typical Sections

Paved Sidepath

A sidepath is a bi-directional shared use path adjacent and parallel to a roadway. Sidepaths offer a low-stress experience for bicyclists and pedestrians along network routes with high-speed or high-volume traffic.



Example: Durham, NC (lane markings optional)

Proposed Typical Sections

Protected Bicycle Lane

A protected bicycle lane is a bike lane physically separated from traffic by parking, a curb, or other barrier. Protected bike lanes offer a low-stress experience for bicyclists along routes with high-speed or high-volume traffic.



Example of protected bicycle lane with flex posts

Proposed Typical Sections

Bicycle Lane

A bicycle lane is a portion of the roadway designated for use by cyclists with pavement markings and/or signage. They offer a dedicated facility for bikes on roads where bicycle connections are desired.



Example of bicycle lane with bike route signage

Proposed Typical Sections

Shared Lane Marking / Bicycle Blvd

A shared lane marking, or bicycle boulevard, designates the roadway for use by both bicyclists and automobiles. Bicycle boulevards provide bicycle connections along local streets with low-speed or low-volume traffic.



Example of signage and pavement markings

Proposed Bicycle + Pedestrian Network



Proposed Intersection Treatments + Crossings

Crosswalk

A crosswalk provides a marked crossing of a roadway and can include pedestrian push buttons in busy locations. Pavement markings are often minimal with two parallel striped lines marking the crosswalk.



Example of conventional crosswalks in downtown Weldon (left) and St. Louis (above, courtesy of NACTO)

Proposed Intersection Treatments + Crossings

High-Visibility Crosswalk

A high-visibility crosswalk provides a marked crossing of a roadway and can include pedestrian push buttons in busy locations. Pavement markings are often more substantial than a conventional crosswalk and may include different paving treatments or signage.



Example of high-visibility crosswalk markings

Proposed Intersection Treatments + Crossings

High Visibility Mid-Block Crossing

A high-visibility mid-block crossing provides a safe pedestrian crossing at locations between intersections, often at schools, parks, and other destinations. Common elements include medians or pedestrian refuge islands, crosswalk signage, and stop bars for vehicle traffic.



Example of high-visibility mid-block crossings

Proposed Intersection Treatments + Crossings

Rectangular Rapid Flashing Beacon

A rectangular rapid flashing beacon (RRFB) provides a high-visibility mid-block crossing where a high volume of bicyclists and pedestrians is expected, such as a greenway crossing. RRFBs offer signage and an active warning beacon on either side of the road to increase driver compliance, and often feature a pedestrian refuge island.



Example of crosswalk markings and push button

Proposed Intersection Treatments + Crossings



Proposed Program + Policy Recommendations

Proposed Program Recommendations

- Educational Programs**
 - Watch for Me NC
 - Let's Go NC
 - Safe Routes to School
 - Eat Smart, Move More NC
 - Environmental Education
- Encouragement Programs**
 - Walking/Bicycling Map and/or Mobile App
 - Walk/Bike to School Day Events
 - Walk/Bike to Work Day Events
 - Walk with a Doc
 - Themed Walking/Biking Events
 - Walk-Friendly Community Designation
 - Bicycle Friendly Community Designation
 - Wayfinding Signage
 - Bicycle Parking
 - Bicycle Repair Stations



Proposed Program + Policy Recommendations

Proposed Program Recommendations

- Project-Based Programs**
 - Pop-Up Demonstration Events/Pilot Projects
 - Open Streets Events
 - Paint the Pavement Program
 - Shared Streets Program
- Evaluation Programs**
 - 311 Service / See-Click-Fix
 - Facility Inventory + Maintenance Program
 - Pedestrian/Bicycle Count Program



Proposed Program + Policy Recommendations

Proposed Policy Recommendations

- Define active transportation facilities in the Subdivision Ordinance
- Create standard design guidelines for active transportation facilities
- Require developer-built sidewalks and shared-use paths
- Establish wayfinding standards
- Adopt a Complete Streets Ordinance
- Establish dedicated funding in the Capital Improvement Plan for bicycle and pedestrian facilities
- Reduce Town-wide speed limit to 25mph
- Implement signal detection for bicyclists at signalized intersections
- Adopt a sidewalk maintenance policy
- Conduct an Accessibility Study/Americans with Disabilities Act (ADA) Transition Plan of existing bicycle and pedestrian facilities

Draft Prioritization Criteria + Methodology

Criteria	Description	Rank	Measurement	Points
Connects to Schools	Bicycle/Pedestrian corridor provides connections to schools or colleges.	High	Corridor provides a direct connection to a school or college.	10
		Medium	Corridor is within ¼ - ½ mile of a school or college.	5
		Low	Corridor does not connect to a school or college.	0
Connects to Parks and/or Recreation Centers	Bicycle/Pedestrian corridor provides connections to parks and/or recreation centers.	High	Corridor provides a direct connection to a park and/or recreation center.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Connects to Activity Centers (such as Retail, Central Business District, and/or General Commercial District)	Bicycle/Pedestrian corridor provides connections to areas proposed as Future Mixed Use, Central Business District, and/or General Commercial District within the Town of Walker Future Land Use Plan.	High	Corridor provides a direct connection to a park and/or recreation center.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Connects to Residential Areas	Bicycle/Pedestrian corridor provides connections to areas proposed as Medium Density Residential, Multi-Family, and/or Transitional Land Uses in the Town of Walker Future Land Use Plan.	High	Corridor provides a direct connection to a park and/or recreation center.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Connects to Existing Bicycle or Pedestrian Facilities	Bicycle/Pedestrian corridor provides connections to existing bicycle or pedestrian facilities, including a major or regional intermodal facility such as the Rowles-Cook Trail.	High	Corridor connects to existing bicycle or pedestrian facilities.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Provides Connections Across a Major Barrier	Bicycle/Pedestrian corridor provides connections across a major bicycle or pedestrian barrier, such as a road, canal, or other transportation corridor or area.	High	Corridor provides connections across a major barrier.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Improves Areas with High Rates of Bicycle or Pedestrian Crashes	Bicycle/Pedestrian corridor provides safety improvements to an area with a history of bicycle and pedestrian crashes.	High	Corridor provides safety improvements to an area with a history of bicycle and pedestrian crashes.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0
Community Input	Bicycle/Pedestrian corridor addresses community needs or concerns.	High	Corridor provides safety improvements to an area with a history of bicycle and pedestrian crashes.	10
		Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
		Low	Corridor does not connect to a park and/or recreation center.	0

Draft Prioritization Criteria + Methodology

1. Connects to Schools

Description

Bicycle/Pedestrian corridor provides connections to schools or colleges.

Rank	Measurement	Points
High	Corridor provides a direct connection to a school or college.	10
Medium	Corridor is within ¼ - ½ mile of a school or college.	5
Low	Corridor does not connect to a school or college.	0

Draft Prioritization Criteria + Methodology

2. Connects to Parks and/or Recreation Centers

Description

Bicycle/Pedestrian corridor provides connections to parks and/or recreation centers.

Rank	Measurement	Points
High	Corridor provides a direct connection to a park and/or recreation center.	10
Medium	Corridor is within ¼ - ½ mile of a park and/or recreation center.	5
Low	Corridor does not connect to a park and/or recreation center.	0

Draft
Prioritization
Criteria +
Methodology

3. Connects to Activity Centers: Future Mixed-Use, Central Business District, and/or General Commercial land uses

Description

Bicycle/Pedestrian corridor provides connections to areas proposed as Future Mixed-Use, Central Business District, and/or General Commercial land uses in the town of Weldon Future Land Use Plan.

Rank	Measurement	Points
High	Corridor is within ¼ mile of Future Mixed-Use, Central Business District, or General Commercial areas.	10
Medium	Corridor is within ½ mile of Future Mixed-Use, Central Business District, or General Commercial areas.	5
Low	Corridor does not connect to an activity center.	0

Draft
Prioritization
Criteria +
Methodology

4. Connects to Residential Areas

Description

Bicycle/Pedestrian corridor provides connections to areas proposed as Medium Density Residential, Multi-Family, and/or Transitional land uses in the Town of Weldon Future Land Use Plan.

Rank	Measurement	Points
High	Corridor is within ¼ mile of Medium Density, Multi-Family, or Transitional residential areas.	10
Medium	Corridor is within ½ mile of Medium Density, Multi-Family, or Transitional residential areas.	5
Low	Corridor does not connect to a residential area.	0

Draft
Prioritization
Criteria +
Methodology

5. Connects to Existing Bicycle or Pedestrian Facilities

Description

Bicycle/Pedestrian corridor provides connections to existing bicycle or pedestrian facilities, including those of regional or statewide significance such as the Roanoke Canal Trail.

Rank	Measurement	Points
High	Corridor closes a gap between existing regional/statewide trails, such as the Roanoke Canal Trail.	10
Medium	Corridor connects to existing bicycle or pedestrian facilities.	5
Low	Corridor does not connect to existing bicycle or pedestrian facilities.	0

Draft
Prioritization
Criteria +
Methodology

6. Provides Connections Across a Major Barrier

Description

Bicycle/Pedestrian corridor provides connections across a major bicycle or pedestrian barrier, such as a railroad, major highway/interstate corridor, or river.

Rank	Measurement	Points
High	Corridor provides connections across a major barrier.	10
Medium	Corridor connects to existing bicycle or pedestrian facilities that provide access across a major barrier.	5
Low	Corridor does not provide access across a major barrier.	0

Draft
Prioritization
Criteria +
Methodology

7. Improves Areas with High Rates of Bicycle or Pedestrian Crashes

Description

Bicycle/Pedestrian corridor provides safety improvements in an area with a history of bicycle and pedestrian crashes.

Rank	Measurement	Points
High	Corridor has seen 4+ bicycle or pedestrian crashes.	10
Medium	Corridor has seen 1-3 bicycle or pedestrian crashes.	5
Low	Corridor has seen 0 bicycle or pedestrian crashes.	0

Draft
Prioritization
Criteria +
Methodology

8. Community Input

Description

Bicycle/Pedestrian corridor addresses community needs or concerns.

Rank	Measurement	Points
High	Town received 4+ public comments identifying needs or concerns along the corridor.	10
Medium	Town received 1-3 public comments identifying needs or concerns along the corridor	5
Low	Town received 0 public comments identifying needs or concerns along the corridor.	0

Mapping Exercise

1. **Click the link in the chat:**
<https://app.conceptboard.com/board/6us1-kbb5-ueu1-hkzg-a5dx>
2. **Select Guest Access**
3. **Select a “Post-it Note” and share your thoughts on the board**



Next Steps + Questions

Next Steps

- Public Meeting (August 22nd, 4-6pm, Halifax Community College - Room 401)
- Network Refinements + Prioritization (August/September)
- Implementation Recommendations (October)
- Steering Committee Meeting #3 (November 2023, Draft Plan Review)

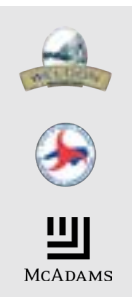
Questions?

Kathryn Zeringue,
Senior Bicycle + Pedestrian Planner
zeringue@mcadamsco.com



Town of Weldon
Bicycle + Pedestrian
Project Acceleration Plan

Steering Committee Meeting #3
November 30, 2023



Agenda

- Public Meeting Input
- Priority Projects, Cut Sheets, + Cost Estimates
- Implementation Strategies
- Plan Tour
- Next Steps + Questions



Project Schedule



Public Meeting Input

- Meeting Purpose:** To share the proposed network recommendations, intersection and crossing treatment recommendations, policy and program recommendations, and implementation plan.
- Key takeaways include the following:
 - Attendees expressed strong support for proposed greenways and trails in Weldon, including a greenway along Chockoyotte Creek and an extension of the Roanoke Canal Trail to River Falls Park along the Roanoke River.
 - There is a great need for safe walking and bicycling facilities in Weldon, particularly along US-158.
 - Students and employees at Halifax Community College would benefit from safe walking and bicycling facilities connecting to the campus.
 - A network of improved walking and bicycling facilities in Weldon could encourage recreational and environmental tourism within the community.
 - Existing sidewalks in the downtown area require maintenance and improvements.

Priority Projects, Cut Sheets, + Cost Estimates

- Priority Projects**
- US-158 Sidepath and Sidewalks
 - Chockoyotte Creek Greenway
 - Roanoke Canal Trail
 - 2nd St Protected Bike Lane
 - Elm St Sidewalk Gaps
 - Country Club Blvd Sidepath
 - Washington Ave Bike Lane
 - Sycamore St Sidewalk Gaps



Priority Project #1



Priority Project #2

Chockygrove Creek Greenway

This project aims to create a greenway along Chockygrove Creek, providing a safe and scenic route for walking and biking. The project includes the installation of concrete curbs, drainage, and landscaping to enhance the natural environment. The map shows the greenway route following the creek through the neighborhood.

Priority Project #3

Rossmore Canal Trail

This project focuses on developing a trail along the Rossmore Canal, offering a recreational space for the community. The plan includes paving the trail, adding lighting, and installing benches. The map illustrates the trail's path along the canal and through the surrounding area.

Priority Project #4

2nd St Protected Bike Lane

The goal of this project is to establish a protected bike lane on 2nd Street to improve cyclist safety. The project involves installing bollards and signage to create a dedicated space for bicycles. The map shows the bike lane running along 2nd Street.

Priority Project #5

Ellis St Sidewalk Laps

This project seeks to improve the sidewalk conditions on Ellis Street, making it more accessible and safer for pedestrians. The work includes repairing damaged sections and adding tactile paving. The map highlights the specific areas on Ellis Street that will be addressed.

Priority Project #6

Country Club Rd Sidewalks

The objective of this project is to upgrade the sidewalk infrastructure on Country Club Road. This includes widening sidewalks, adding crosswalks, and ensuring ADA compliance. The map shows the project area along Country Club Road.

Priority Project #7

Washington Ave Bike Lane

This project is designed to create a new bike lane on Washington Avenue to encourage more people to cycle. The plan includes installing bollards, signage, and pavement markings. The map shows the bike lane running along Washington Avenue.



Implementation Strategies

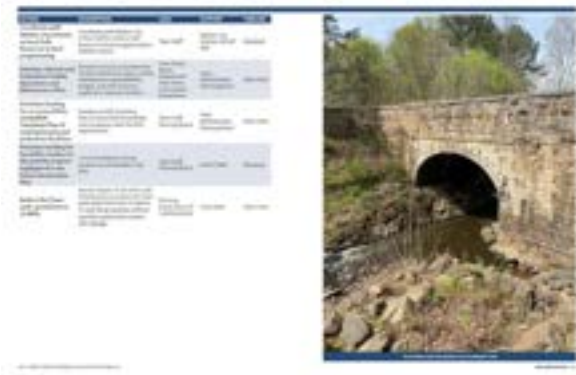
Key Partners

- Town of Weldon
- Regional + County Stakeholders
- North Carolina Department of Transportation (NCDOT)
- Peanut Belt RPO
- Residents, Businesses, and Community Groups
- NC Wildlife Resources Commission
- Private Developers
- CSX

Action Plan

Maintenance Guidance

Project Development Process



Project Development Process



Plan Tour



Table of Contents



Introduction



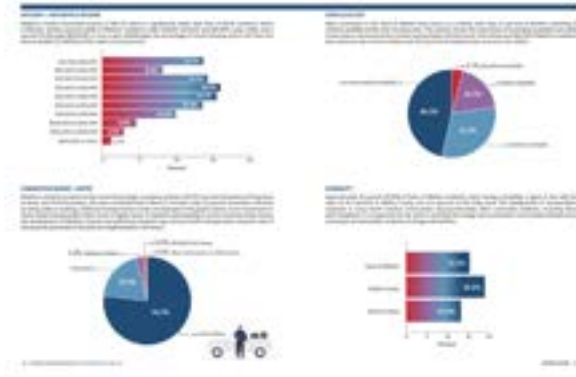
Process + Schedule / Vision + Goals



Study Area



Demographics



Benefits

The page is titled "ACTIVE TRANSPORTATION BENEFITS". It features a grid of six circular icons representing different activities: cycling, walking, roller skating, swimming, rowing, and a general active transportation icon. To the right, there are two photographs showing people engaged in outdoor activities like walking and roller skating. Text blocks provide details about the benefits of these activities.

Existing Conditions



Existing Conditions

The page is titled "EXISTING FACILITIES". It includes a map of the town area on the right side, showing various facilities and their locations. On the left, there is a list of facilities and a photograph of a paved path with people walking. The page also contains several paragraphs of text.

Previous Plan Review

This page features a list of items on the left side, likely a table of contents or a list of findings from a previous plan review. On the right side, there is a map showing a network of roads and possibly other infrastructure.

Policy Review

The page contains several paragraphs of text on the left side, which appear to be a summary or analysis of policy reviews. On the right side, there is a photograph of a street scene showing a car, a person, and a utility pole.

Opportunities + Constraints



Opportunities + Constraints



Community Engagement



Steering Committee

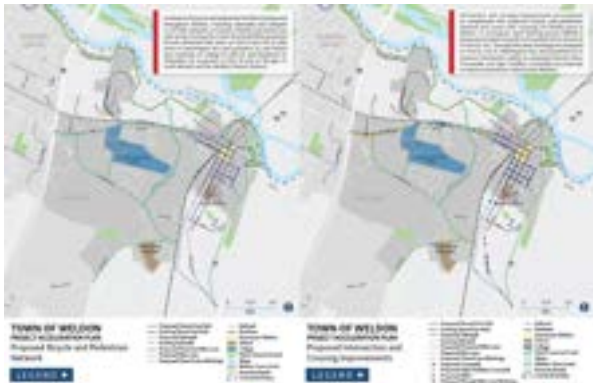


Public Meeting



Community Survey





BICYCLE + PEDESTRIAN NETWORK

Public Transit - Bicycles + Pedestrian Recommendations

Route	Mode	Start	End	Notes
1	Bicycle
2	Bicycle
3	Bicycle
4	Bicycle
5	Bicycle
6	Bicycle
7	Bicycle
8	Bicycle
9	Bicycle
10	Bicycle
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92	Bicycle
93	Bicycle
94	Bicycle
95	Bicycle
96	Bicycle
97	Bicycle
98	Bicycle
99	Bicycle
100	Bicycle

Prioritization

The image displays two tables side-by-side, likely representing project prioritization data. The tables have multiple columns, including project names, locations, and priority levels. The text is small and difficult to read, but the structure suggests a list of projects being ranked or categorized.

Prioritization



Cut Sheets



Recommended Policies + Programs



Recommended Policies



Recommended Programs





Design Resources



Cost Estimates



Next Steps +
Questions

Next Steps

- Steering Committee - Draft Plan Review
- Final Plan
- Plan Adoption

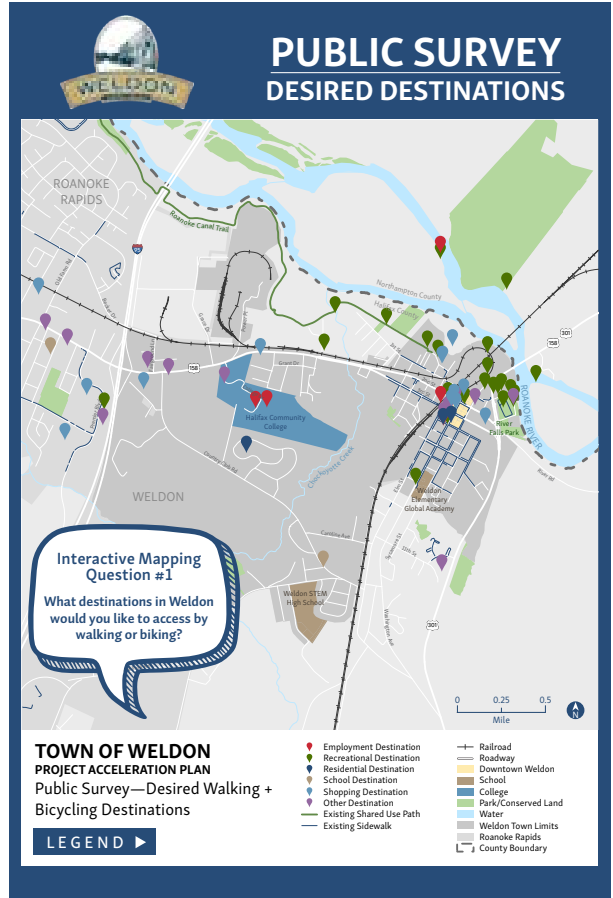
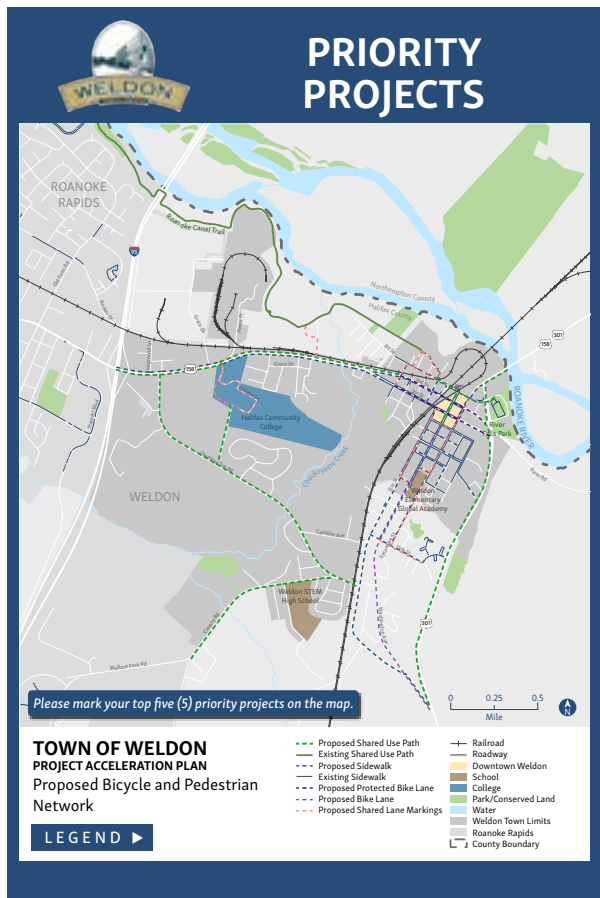
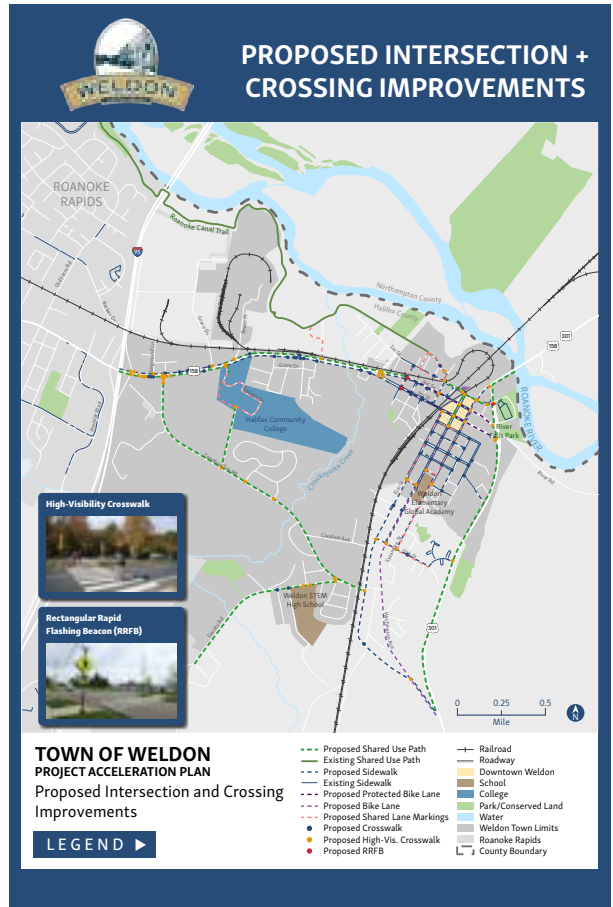
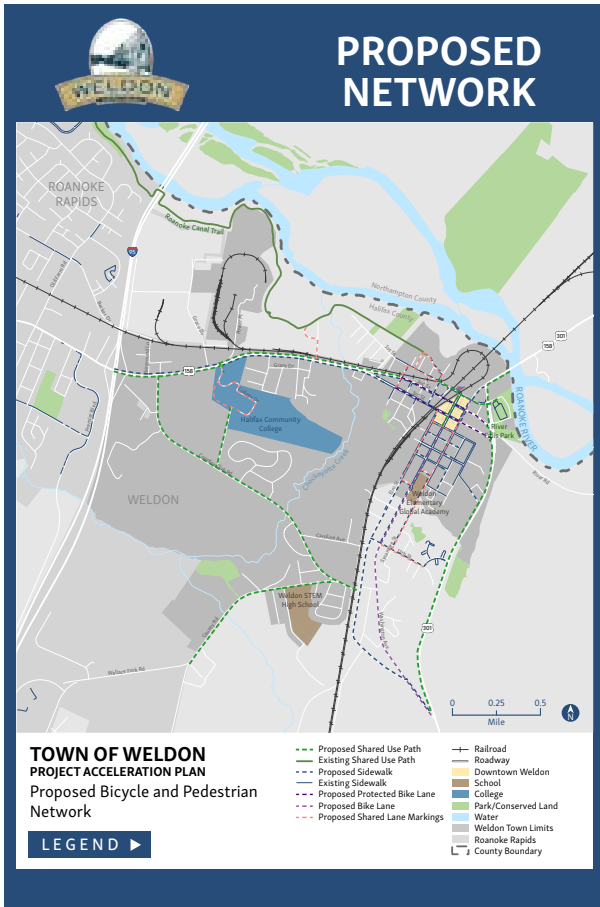
Thank you for serving on the Steering Committee! We appreciate your time and support!

Questions?

Kathryn Zeringue,
Senior Bicycle + Pedestrian Planner
zeringue@mcadamsco.com







PUBLIC SURVEY INFRASTRUCTURE IMPROVEMENTS

Interactive Mapping Question #2
Which locations (streets, intersections, neighborhoods, etc.) need the most bicycle and pedestrian infrastructure improvements in Weldon?

**TOWN OF WELDON
PROJECT ACCELERATION PLAN
Public Survey—Bicycle + Pedestrian
Infrastructure Improvements**

LEGEND

- Improvement Location
- Existing Shared Use Path
- Existing Sidewalk
- Railroad
- Roadway
- Downtown Weldon
- School
- College
- Park/Conserved Land
- Water
- Weldon Town Limits
- Roanoke Rapids
- County Boundary

RECOMMENDATIONS PROGRAMS | POLICIES | MAINTENANCE

RECOMMENDED PROGRAMS

- Educational Programs**
 - Watch For Me NC
 - Let's Go NC
 - Safe Routes to School
 - Eat Smart, Move More NC
 - Environmental Education
- Encouragement Programs**
 - Walking/Bicycling Map and/or Mobile App
 - Walk/Bike to Work Day Events
 - Walk With a Doc
 - Themed Walking/Biking Events
 - Walk-Friendly Community Designation
 - Bicycle Friendly Community Designation
 - Wayfinding Signage
 - Bicycle Parking
 - Bicycle Repair Stations
- Project Based Programs**
 - Pop-up Demonstration Events / Pilot Projects
 - Open Streets Events
 - Paint the Pavement Program
 - Shared Streets Program
- Evaluation Programs**
 - 311 Service / See-Click-Fix
 - Facility Inventory + Maintenance Program
 - Pedestrian/Bicycle Count Program

GUIDING MAINTENANCE PRACTICES

- Common Maintenance Issues with Sidewalks**
 - Uplift, Tilt, or Settlement
 - Sagging
 - Heaving
 - Spalling
 - Tree Root Damage
- Seasonal Maintenance**
 - Vegetation Removal + Control
 - Sweeping
 - Snow and Ice Removal
- ADA Accessibility**

RECOMMENDED POLICIES

- Define Active Transportation Facilities in the Subdivision Ordinance
- Create Standard Design Guidelines for Active Transportation Facilities
- Developer-Built Sidewalks and Shared Use Paths
- Establish Wayfinding Standards
- Establish an Active Transportation Advisory Committee
- Adopt a Complete Streets Ordinance
- Adopt a Flood Damage Prevention Ordinance
- Reduce Town-wide Speed Limit to 25mph
- Promote a Resilient Active Transportation Network Through Green Street Design Standards
- Establish Dedicated Funding in the Capital Improvement Plan for Bicycle and Pedestrian Facilities
- Implement Signal Detection for Bicyclists at Signalized Intersections
- Adopt a Sidewalk Maintenance Policy
- Conduct an Accessibility Study/ADA Transition Plan of Existing Bicycle and Pedestrian Facilities

DESIGN STANDARDS + TYPICAL SECTIONS

SIDEWALK

A sidewalk is a bi-directional path that provides a dedicated travel area for pedestrians in the transportation network. Sidewalks are physically separated from the roadway by a curb or unpaved buffer space.

SHARED LANE MARKING / BICYCLE BOULEVARD

A shared lane marking (or "sharrow") or bicycle boulevard, designates the roadway for use by both bicyclists and automobiles. Bicycle boulevards provide bicycle connections along local streets with low-speed or low-volume traffic.

PAVED SIDEPATH

A sidepath is a bi-directional shared use path adjacent and parallel to a roadway. Sidepaths offer a low-stress experience for bicyclists and pedestrians along network routes with high-speed or high-volume traffic.

BICYCLE LANE

A bicycle lane is a portion of the roadway designated for use by cyclists with pavement markings and/or signage. They offer a dedicated facility for bikes on roads where bicycle connections are desired.

PROTECTED BICYCLE LANE

A protected bicycle lane is a bike lane physically separated from traffic by parking, a curb, or other barrier. Protected bike lanes offer a low-stress experience for bicyclists along routes with high-speed or high-volume traffic.

PUBLIC SURVEY SUMMARY

SURVEY OPEN APR 16, 2023 84 PARTICIPANTS 11 QUESTIONS + 4 BONUS EXERCISES

How do you rate levels of comfort for those currently walking and biking in Weldon?

Very comfortable 38% | Comfortable 57%

What is your primary mode of transportation for commuting to work and/or school?

My Walk (alone) 27% | My Bicycle (alone) 23% | My Car (alone) 23% | My Car (with others) 27%

What is your relationship to Weldon?

Current Resident 50% | Former Resident 42% | Visitor 10% | Other 10% | Other 10% | Other 10%

Have your walking and biking habits changed because of the COVID-19 pandemic?

Increased walking and biking 48% | Decreased walking and biking 28% | Stayed the same 24%

Now consider your desired commute in the future. Which modes would you like to use? (select all that apply)

Walking 43% | Biking 37% | Car 32% | Other 16% | Other 16% | Other 16%

How frequently do you walk in Weldon?

At least once a week 35% | At least once a month 25% | At least once a year 22% | Never 18%

What are the primary reasons that you or members of your household walk or bike in Weldon?

Exercise 50% | Recreation 18% | Commuting 15% | Errands 10% | Other 7% | Other 7%

What factors discourage you from walking and biking in Weldon?

Lack of sidewalks 27% | Lack of bike lanes 23% | Lack of parking 18% | Lack of signage 13% | Lack of lighting 10% | Lack of safety 10%

How frequently do you bike in Weldon?

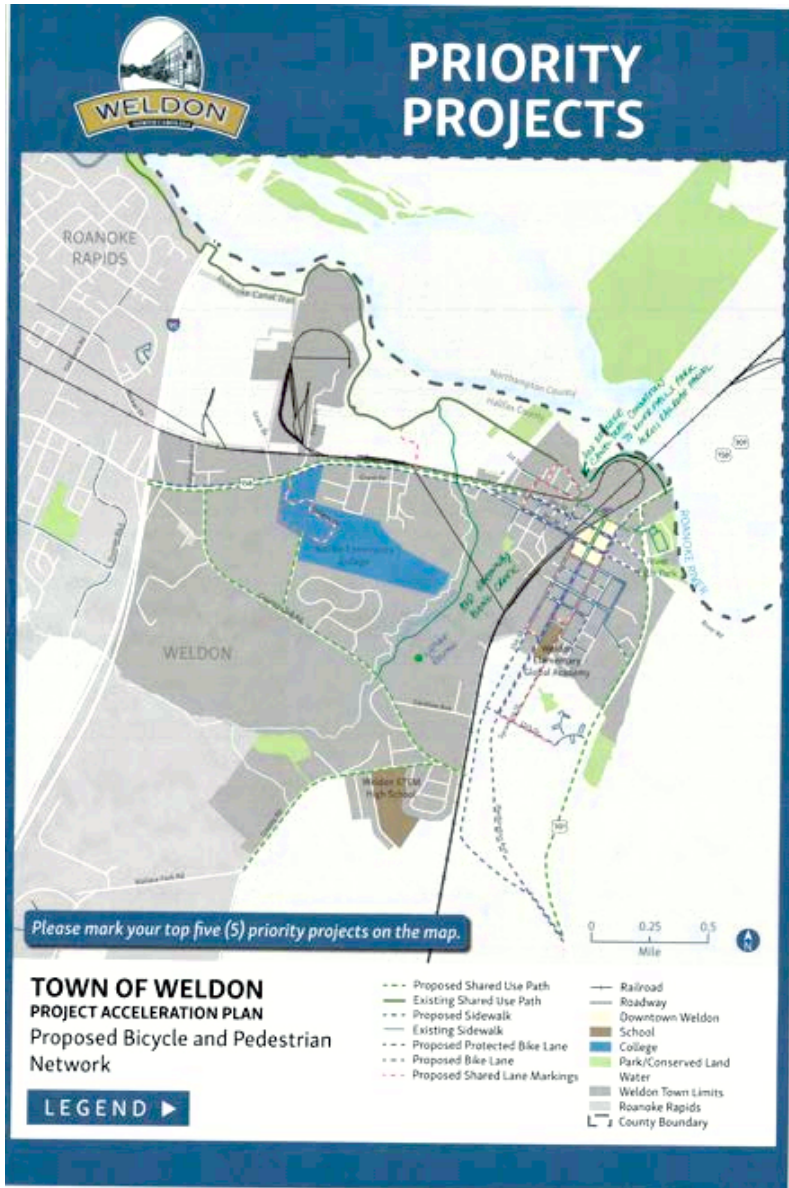
At least once a week 20% | At least once a month 15% | At least once a year 10% | Never 55%

Which destinations would you like to walk or bike to in Weldon if bicycle and pedestrian facilities were improved?

City Center 40% | Downtown 47% | Schools 37% | Parks 30% | Other 15% | Other 11% | Other 10%

Rank which infrastructure or program improvements would encourage you to use bicycle and pedestrian facilities more frequently in Weldon?

Ranked 1st: Sidewalks
Ranked 2nd: Bicycle Lanes
Ranked 3rd: Shared Use Paths
Ranked 4th: Wayfinding Signage
Ranked 5th: Bicycle Parking



Halifax Community College in Room 401A/B | August 22, 2023 | 4:00 PM - 6:00 PM

PUBLIC MEETING SIGN-IN SHEET

Town of Weldon Bicycle + Pedestrian Project Acceleration Plan

Name	Email (optional)	ZIP Code (optional)
Atlan King	kingat@kramcncs.org	
Gray Keeter	pkhuetor@ncdot.gov	27870
Kayla Taylor	kayla.nicole.taylor61@gmail.com	27890
Jennifer Cox	bnspyrd@gmail.com	27890
Nick Morrison	nmorrison@ncdot.gov	27606
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Caroline Long	Countoncaroline@gmail.com	27831
Muzette Kiger	mkiger@historieweldonnc.com	27890
Jo Shadin	jwshadin@yahoo.com	27890
Cesar Neville	cesarnevill@gmail.com	27890
Kelly Barber	kbarber@halifaxcc.edu	27890
Tiffany Hale	thale23@halifaxcc.edu	27890
		thale23@hotmail.com



TOWN OF WELDON BICYCLE + PEDESTRIAN PROJECT ACCELERATION PLAN PUBLIC MEETING COMMENT FORM

1. Please provide your feedback on the Town's draft bicycle and pedestrian network below.

- I am pleased to see the consideration to safety - more crosswalks
- I like the idea of sidewalks along 158 - connecting Weldon to Roanoke Rapids
- Sidewalks and bike paths will allow people to travel safely to & from shopping areas, the college and retail areas. Connecting the 3 areas will be ^{great}.
- Our waterways are our most valuable resource & ecotourism is big. it would be nice if the plan actually maximises the use of our trails & waterways. Our town would benefit from a walkable bikeable path that connects the communities to the natural areas for enjoyment
- We should think big - bigger ^{as} if money was not an issue. We ~~are~~ ^{have} the resources to become a major ecotourism town. I would like to see destinations along the trails so that the trails are interactive; and immersive & possibly multiple entrances and exits to the trail. I would like to see more attention to the Chockosotte Creek Trail.

2. Please list your top five (5) priority projects below.

1. Sidewalks should be standard. No one should have to walk in the ^{Aqua Duct.} Road
2. If ~~our~~ bike lanes & trails were safer we could become a destination for cyclist, hikers and people who nature walk
3. We should clearly define the entrance to the Roanoke Canal trail from the park and work with neighboring towns to connect
4. I like the idea of a maximum speed limit there is no need for speed in such a small community
5. The signage for the bike and ped network should be easily identifiable and the maps should be simple and easy to follow. Government, private, and public cooperation will be needed to accomplish a major dream but

Thank you for attending the meeting. Please return this form to the project team.

other communities have done it so it can be done!

August 22, 2023

Thank you All for your Hard work!!



**TOWN OF WELDON BICYCLE + PEDESTRIAN
PROJECT ACCELERATION PLAN
PUBLIC MEETING COMMENT FORM**

1. Please provide your feedback on the Town's draft bicycle and pedestrian network below.

If there was a greenway and bike path, the back of HCC Campus would have an access to get back to the river and down town. Students could bike/walk safely

2. Please list your top five (5) priority projects below.

1. Greenways for future projects / Safety for Students
2. Agri / Ecotourism Potential
3. Tourism opportunities in downtown Weldon
4. Safe pedestrian sidewalks connecting HCC to downtown Weldon
5. Tourist to walk safely on sidewalks from I95 Hotels to River.

Thank you for attending the meeting. Please return this form to the project team.



**TOWN OF WELDON BICYCLE + PEDESTRIAN
PROJECT ACCELERATION PLAN
PUBLIC MEETING COMMENT FORM**

1. Please provide your feedback on the Town's draft bicycle and pedestrian network below.

The draft bicycle and pedestrian network is missing to connecting corridors, the path along Choeyotte Creek and the loop from River Falls park under the railroad-track to connect behind Riverside Mills

2. Please list your top five (5) priority projects below.

1. The loop under the railroad between the park and mills
2. Development of Choeyotte creek
3. Sidewalks from downtown Weldon to HCC (158)
4. Extended side walk to 11th st down Sycamore
5. Improved downtown sidewalks

Thank you for attending the meeting. Please return this form to the project team.



TOWN OF WELDON BICYCLE + PEDESTRIAN PROJECT ACCELERATION PLAN PUBLIC MEETING COMMENT FORM

1. Please provide your feedback on the Town's draft bicycle and pedestrian network below.

Please add the two lines of greenways (chockyote) and connecting line for the river path which goes under 158. We spoke specifically with both Catherine and Nick Morris. Both - ~~was~~ very informative and helpful - Thank you so much.

2. Please list your top five (5) priority projects below.

1. The loop.
2. The Chock^o Green way
3. side walks along 158 - please
- 4.
- 5.

Thank you for attending the meeting. Please return this form to the project team.

Town of Weldon Bicycle + Pedestrian Plan

Project Engagement

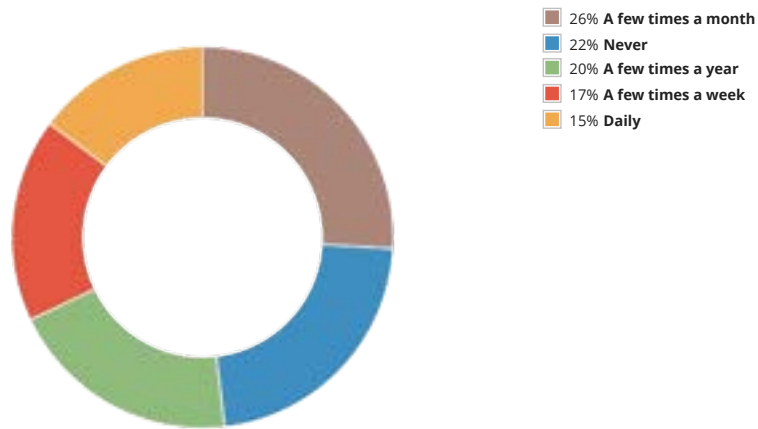
VIEWS	PARTICIPANTS	RESPONSES	COMMENTS
317	84	1,642	186

What is your relationship to Weldon? *Select all that apply.*

50%	I visit/spend time in Weldon	37 ✓
42%	I live in Weldon	31 ✓
30%	I own property in Weldon	22 ✓
26%	I work in Weldon	19 ✓
8%	I own a business in Weldon	6 ✓
8%	None of the above	6 ✓

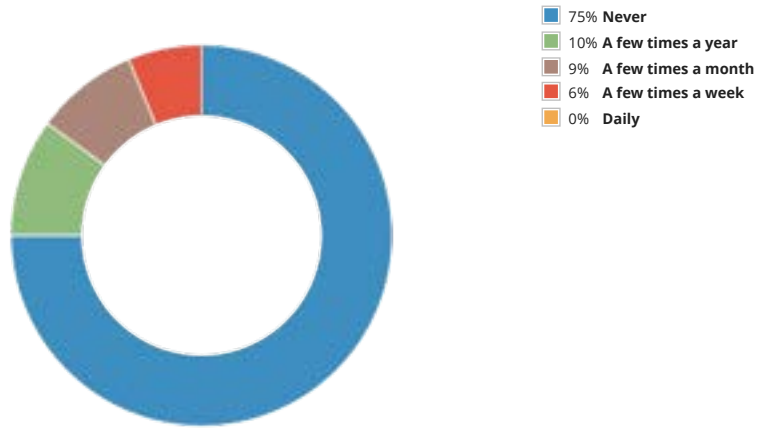
74 Respondents

How frequently do you walk in Weldon?



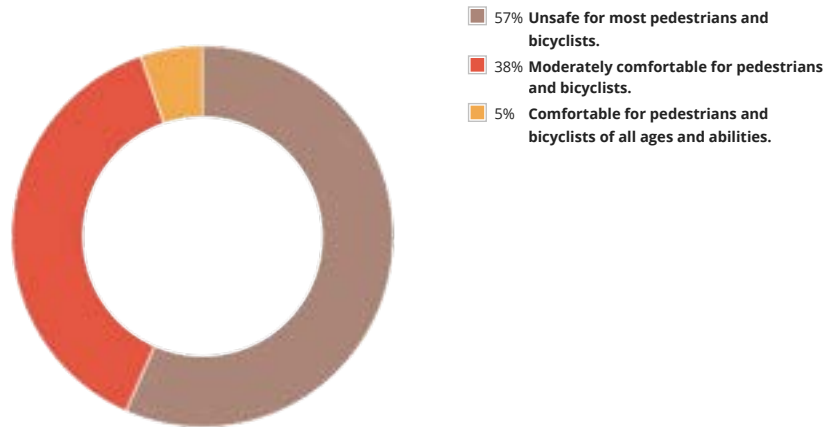
81 respondents

How frequently do you bike in Weldon?



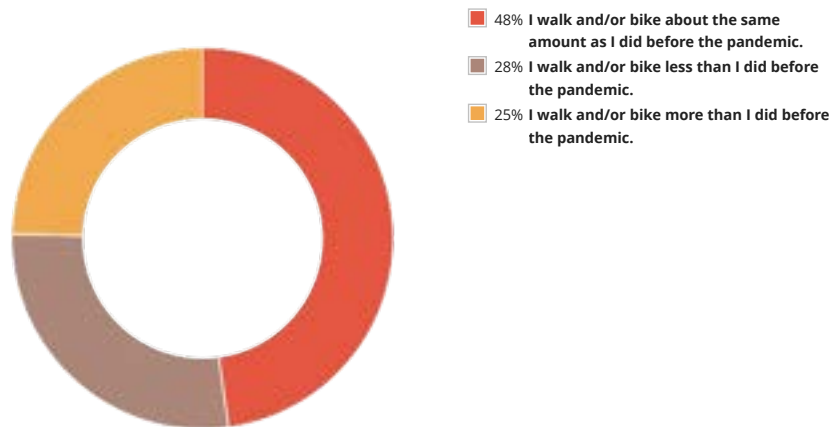
80 respondents

How do you rate levels of comfort for those currently walking and biking in Weldon?



76 respondents

Have your walking and biking habits changed because of the COVID-19 pandemic?



69 respondents

What are the primary reasons that you or members of your household walk or bike in Weldon? *Select all that apply.*

67%	For exercise / fitness	44 ✓
58%	For recreation	38 ✓
15%	Run errands / go shopping	10 ✓
15%	Other	10 ✓
5%	Commute to work	3 ✓
3%	Get to and from essential services (Government centers, medical centers, libraries, etc.)	2 ✓
2%	Get to and from an on-demand transit stop / taxi waiting area	1 ✓
0%	Commute to school	0 ✓

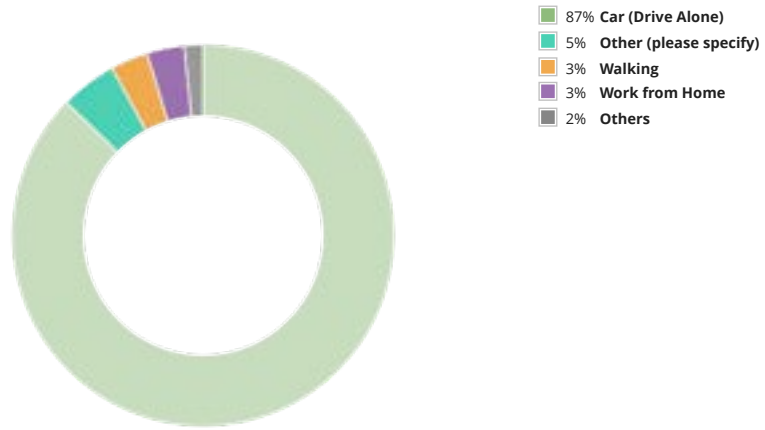
66 Respondents

Which destinations would you like to walk or bike to in Weldon if bicycle and pedestrian facilities were improved? *Select all that apply.*

69%	Parks / playgrounds / recreation centers	42 ✓
67%	Greenways and trails	41 ✓
59%	Downtown	36 ✓
57%	Shops / restaurants / entertainment	35 ✓
38%	Neighboring communities	23 ✓
28%	Essential Services (Government centers, medical centers, libraries, etc.)	17 ✓
16%	School	10 ✓
11%	Work	7 ✓
10%	On-demand transit stops / taxi waiting area	6 ✓

61 Respondents

What is your primary mode of transportation for commuting to work and/or school?



63 respondents

Now consider your desired commute in the future. Which modes would you like to use?
Select all that apply.

68%	Car (Drive Alone)	43 ✓
37%	Walking	23 ✓
32%	Bicycling	20 ✓
19%	Work from Home	12 ✓
13%	Other (please specify)	8 ✓
6%	Transit / Taxi	4 ✓
5%	Carpool	3 ✓

63 Respondents

What factors discourage you from walking and biking in Weldon? *Select all that apply.*

58%	Lack of safe biking and walking connections to and from existing bicycle and pedestrian facilities	36 ✓
56%	Personal safety concerns	35 ✓
55%	Poor maintenance conditions of existing bicycle and pedestrian facilities	34 ✓
44%	Unsafe street crossings	27 ✓
40%	Motor vehicle traffic / speed	25 ✓
37%	Lack of intersection treatments for bicyclists and pedestrians	23 ✓
24%	Lack of nearby destinations	15 ✓
21%	Lack of signage and wayfinding	13 ✓
15%	Lack of parking at trailheads	9 ✓
5%	Lack of interest	3 ✓
5%	Other (please specify)	3 ✓

62 Respondents

Rank which infrastructure or program improvements would encourage you to use bicycle and pedestrian facilities more frequently in Weldon? *Please rank your top 6 from 1 (most important) to 6 (least important).*

69%	Constructing new bicycle and pedestrian facilities	Rank: 1.77	35 ✓
71%	Installing crosswalks, pedestrian signals, and bicycle facilities at intersections	Rank: 2.67	36 ✓
73%	Repairing or maintaining existing bicycle and pedestrian facilities	Rank: 3.00	37 ✓
63%	Installing lighting along existing bicycle and pedestrian facilities	Rank: 3.53	32 ✓
69%	Improving accessibility for disabled or impaired residents (curb ramps, audible pedestrian signals, etc.)	Rank: 3.94	35 ✓
59%	Installing shade trees and benches along existing sidewalks and multi-use paths	Rank: 4.27	30 ✓

51 Respondents

Please provide any additional comments/feedback on Weldon's bicycle and pedestrian network.

1. Provide walking and bike path along river edge. From I-95 to river front at Weldon. 2. Trail lighting and emergency phone; signage and wayfinding; historic indicators/education. 3. Provide accessibility and ADA compliant and benches. Gravel or hard surfaces for biking (proper drainage/ black tops).

6 days ago

We really don't have a viable network. What exists is unsafe and what exists is not accessible for people with impaired walking. Personally, crossing Washington Ave in the 3 blocks of downtown is difficult. I have to have assistance.

6 days ago

Improvement would attract more people to the area

6 days ago

Weldon's community is primarily a pedestrian community with many residents dependent solely on sidewalks, bike paths, and safe crosswalks. Unfortunately, Weldon presently is in great need of all!

6 days ago

A greenway or trail from Halifax to Weldon or widen shoulders on 301 for bikes to travel that route

11 days ago

Your item above only let me rank 1. First is Accessibility. 2. Repairing and Maintaining EXISTING facilities; 3. Installing Lighting; 4. Installing Shade trees, and benches along EXISTING paths-ESPECIALLY FIRST STREET- our official canal trail that does not look like one at all. 5. Installing crosswalks, ped signals, bike items at intersections, 6 Constructing new

11 days ago

The above is not letting me rank from 1-6. Therefore: Accessibility is first SO EVERYONE can get around. 2. Repairing and maintaining EXISTING facilities. Fixing what we have before doing new projects would be a great improvement (Safety first, right?) 3. Installing Lighting along Existing. 4, Installing shade trees, benches along existing (ESPECIALLY FIRST STREET). This is our canal trail and it is not an inviting trail at all, 5, Installing crosswalks, pedestrian signs and bike facilities at intersections, 6. Constructing new.

11 days ago

There are many areas of concern. I will speak to the ones I know best.

Town sidewalks on Washington Avenue are in need of repair for safety and accessibility. There is a grant for the 300 block, but the 100 and 200 blocks also need work. Furniture goes into Red Wagon and there is no great way for it to get there and doing so puts an increased load on the already compromised sidewalks. The area is difficult for those with wheelchairs to navigate. Over on Sycamore Street near the Evangelistic Church of Deliverance, the pavement from the parking lot to the street is crumbling and the sidewalk along the grassy area that the town owns is overgrown with grass in places and there is no good ramp leaving the sidewalk or a crosswalk to get over to the church or other side of the street.

First Street is the "official" Canal Trail going from the parking lot at the river and trailhead through town to Riverside Mill, where the natural trail resumes. This area is bleak and does not look like a trail at all. There were fish painted on the pavement to guide people but they are faded and not noticeable unless an experienced resident points them out. Sidewalks run out. Trail markers are small and once they age, they are hard to see. The tunnel under the road near Weldon Mills was once well-painted, but again, you wouldn't know it is there unless an experienced resident showed it to you. How safe is it? It does not look very inviting or safe, Wheelchair access? Clearly noted signs from the trailhead to Riverside Mill, street markings, clearly marked crossings, a bike path, and also benches and planters along First Street are needed so that 1) people know this is part of the canal trail, 2) they can navigate it safely and easily and 3) it actually looks inviting and pleasant. If you walk the trail from the trailhead back to the natural path after Riverside Mill, you really would think you are lost and not on a trail. While the area may be safe, it doesn't look like it.

Additionally, the canal trail needs to go along the river the entire length of the canal. People naturally follow what appears to be the Canal Trail only to run into a dead end of overgrowth. They get frustrated and turn around. If someone tells them the real trail is through town and along a poorly marked street with questionable sidewalks, old buildings, and a car yard, they leave and don't bother. While this would take time to create and working with the railroad, the continuous path along the river would provide healthy recreation for residents and visitors alike. Weldon needs it's visitors and their business to help turn things around. Residents need safe ways to get where they are going and opportunities for healthy recreation.

12 days ago

NA

12 days ago

As an avid cyclist I bicycle in Weldon many times a month especially at River Falls Park and through the main street area. This has great potential for drawing cyclists to enjoy connecting the good rural roads in the area

13 days ago

Bicycle rentals similar to large cities (Citibike) would be nice for tourists who are traveling without their bicycles.

23 days ago

I would love to see areas that people in the community walk with no sidewalk/greenway have a safe established area for pedestrian travel (i.e. between downtown and edge of Weldon at the I95 bridge along 158)

26 days ago

Construct a new greenway trail from the river boat landing along the river under 301 Hwy bridge to conclude and connect with the existing canal rail at Riverside mill. This would allow a walking biking loop via the river for environmental enjoyment and connect with businesses downtown. It would be about a 2 mile loop that would allow parking on existing parking lot and create an exercise loop for all ages and visitors. I think this should be a number 1 priority of anything from this plan. A lot of citizens and visitors have commented on the need for this what I will call the canal loop trail.

26 days ago

Country Club Road and 158 from Roanoke Rapids could use a dedicated bike lane and a sidewalk.

one month ago

Important improvements for disability and hearing and sight impairment individuals. Adding electronic signage ,sounds for those with disabilities

one month ago

THANK YOU for making Weldon an even better place to live, work, worship and play.

one month ago

It is important to make sure the entire town is connected through this network. The existing Roanoke Canal Trail is a primary focus to loop the trail under the railway back through River Falls park. The sidewalks from downtown should be extended to Halifax Community College and Weldon High School across the railroad tracks

one month ago

Even though I do not ride a bike, I would love to walk more. The sidewalks need to be improved.

one month ago

We get a lot of visitors at my business who like to walk & bike the existing canal trail but it is often hard to navigate. The next most requested is directions to the river. Many of our visitors are in large campers and the ability to walk & bike around town is important to them

one month ago

I sometimes walk the canal trail from the treatment plant behind what used to be Howell Steel into Weldon but don't feel safe on that trail. Need something better that's not so isolated.

one month ago

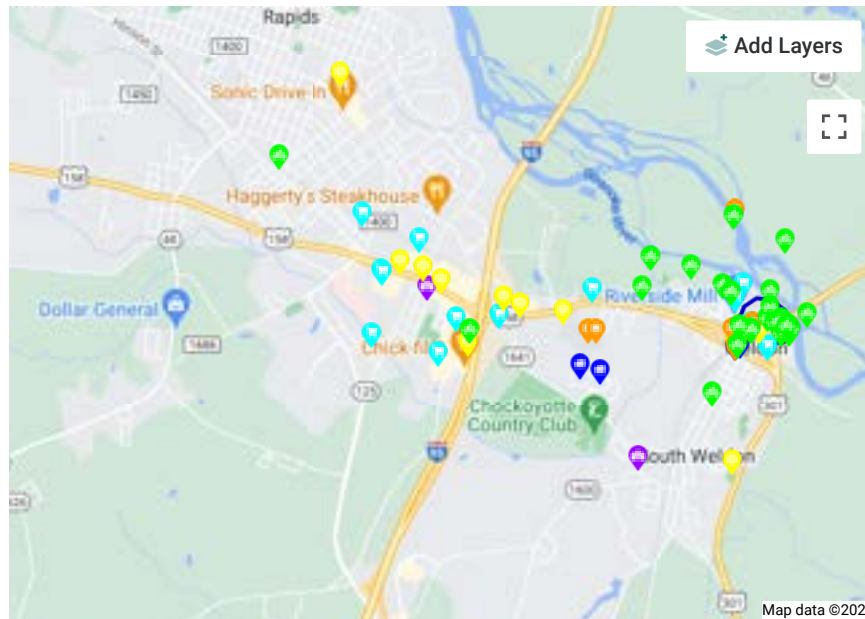
Would like to see bike routes from Weldon to Roanoke Rapids

one month ago

TEST

2 months ago

What destinations in Weldon would you like to access by walking or biking? *Use the pins below to mark locations that you would like to reach via bicycle and pedestrian facilities, such as where you live, work, shop, work out/play, go to school, and other.*



Halifax County Departments/Downtown/Restaurants/Historic Halifax

6 days ago

Downtown and Riverfront and all parks in Roanoke Rapid

6 days ago

Lowe's Shopping Center areas/ restaurants/Walmart/Fitness/banks/hospitals/parks

6 days ago

Home: Lee Lane Weldon (off Country Club Rd)

6 days ago

Rec: Canal Trail other trails can be created (to HCC)

6 days ago

Shopping in downtown, Riverside Mills, River Falls Park, Weldon Mills

6 days ago

Other: Cedarwood cemetery used to be favored for walking -quiet, serene, little or no traffic. Now it is unsafe.

6 days ago

Canal Trail

6 days ago

Will use "nature?" after

6 days ago

Along river and Canal Trail

6 days ago

River Falls Park

6 days ago

Washington Ave

6 days ago

River area needs upgrade, grading on track. Washington Ave (home)

6 days ago

I see that I really answered the second question below in my pin placement above. Please accept these pin placements as answers to your second question. As for where I would go if I could, I think there ought to be a way to walk and bike out to Halifax Community College. That is a long way, but what if that were the only way people could access the Community College without a car?

11 days ago

The sign to the Aquaduct is barely visible coming out of Weldon. Harder to see coming into Weldon. The Aquaduct is well protected by having a lack of signage.

11 days ago

AH HA! Here is the Roanoke Canal Trail. I have not biked (only walked) the trail. Some Weldon folks have told me we need better footing/gravel for bikes.

11 days ago

Now where does a person go to get on the trail? Were there signs, complete sidewalks, a bike path? Bikes can be rented at Riverside, but where is the bike path? The view along this road does not suggest trail at all.

11 days ago

Is this the Canal trail? Who knows?

11 days ago

where do I follow the Canal Trail? Through a tunnel under the road. Is there a VISIBLE sign, is there lighting, is it safe?

11 days ago

Sidewalk overgrown with grass, no wheelchair ramp , no cross walk to get to the church. Church has a number of members that need a better way to cross the street

11 days ago

Sidewalks need improvement.

11 days ago

Sidewalks need repair, to allow furniture delivery and most of all to make it EASY for people in wheelchairs to navigate the shopping areas

11 days ago

Sidewalks unsafe in the 300 block

11 days ago

I want to follow the Canal trail. Where does it go from the Trailhead? Not clear, and not natural and the official trail is not easily found, look safe and has sidewalks that run out.

11 days ago

Community College

13 days ago

Riverside Park

13 days ago

Shopping area.

13 days ago

I live in Roanoke Rapids and ride bicycle all over Roanoke Rapids Weldon and about Halifax and Northampton County. River Falls Park is one of my favorite places.

13 days ago

I know and see people walking to Roanoke Rapids regularly (my pins are where I go), but I would like to see a safe way for them to get there - and me if I decided to walk for what ever reason. I do find walking alone a bit of why I don't do it - for safety's sake.

26 days ago

I would like to feel safe to walk here, but I tend to drive

26 days ago

Visit bar for socialization

26 days ago

Convenient location to eat at

26 days ago

Take dogs to dog run for some exercise and socialization

26 days ago

Groceries

26 days ago

Eat

26 days ago

Shopping for clothes

26 days ago

Eat

26 days ago

Eat

26 days ago

Eat

26 days ago

Clothing

26 days ago

Eat

26 days ago

Groceries

26 days ago

Use this ATM regularly when it appears safe (dark and not the best access)

26 days ago

Get gas semi-regularly for convenience

26 days ago

Attend meetings, vote, and other misc activities at the HCC

26 days ago

Shop for unique items, clothing, and gifts periodically

26 days ago

Own property to turn into a rental unit

26 days ago

Other home marker missed the location - this is more accurate of where I live

26 days ago

Mail letters and conduct postal business at the local post office regularly

26 days ago

Visit store for convenience items

26 days ago

Walking circle for exercise (when I feel safe, I will walk there from home)

26 days ago

Nice natural area to hang out

26 days ago

Work from Home

26 days ago

This green outline shows the proposed canal loop trail I have mentioned above.

26 days ago

shop

26 days ago

bike on canal trail

26 days ago

Bring Weldon back

one month ago

X

one month ago

This is the loop from River Falls Park to the Canal trail that does not connect. This would be an amazing start to the Roanoke Canal sightseeing if it were opened up, needs attention with DOT and railway systems

one month ago

Exercise Equipment located on school property available for community use outside of school hours

one month ago

Visit school often, there is a public community garden here now

one month ago

X

one month ago

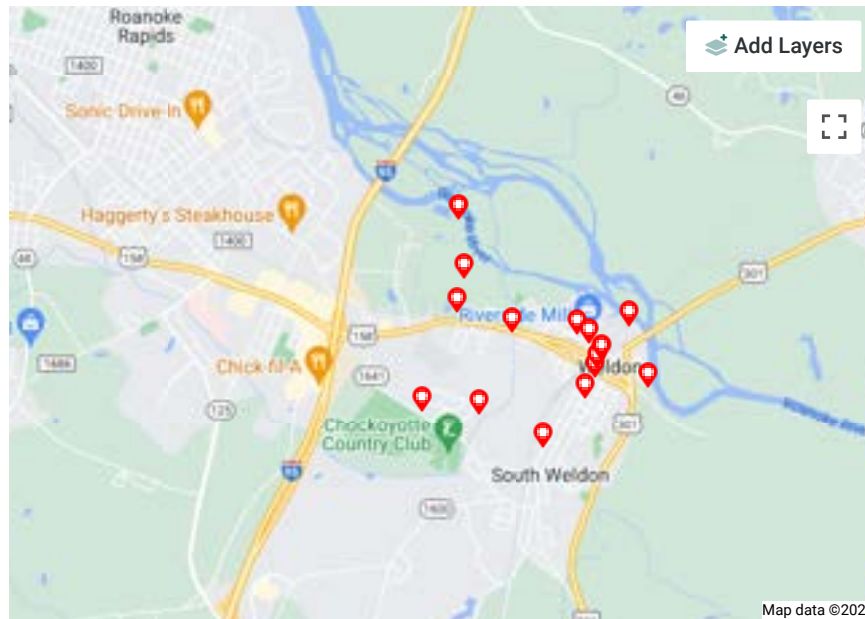
I sometimes walk this canal trail.

one month ago

Grocery Store

2 months ago

Which locations (streets, intersections, neighborhoods, etc.) need the most bicycle and pedestrian infrastructure improvements in Weldon? *Please use the pin option on the map to mark your locations.*



Country Club Rd/Elm St/ Washington Ct/ 301 to Historic Halifax

6 days ago

Along 158 into Downtown Community College access

6 days ago

Walking/ride path along river's edge connect o Roanoke Rapid Trail. Connecting to RR

6 days ago

Connectivity to Roanoke Rapids commercial/retail/restaurant/Walmart area

6 days ago

I can't speak to neighborhood needs. Clearly, the Canal Trail along First St to the river landing. The intersection of downtown streets with Hwy 158. The walkways along Rt 158 from the triangle into town. Sidewalks near and around schools.

6 days ago

HCC

6 days ago

Canal Trail

6 days ago

Canal Trail

6 days ago

Downtown Weldon

6 days ago

Schools

6 days ago

Water

6 days ago

Everywhere

6 days ago

From Weldon to RR via 158 w

6 days ago

Country Club Rd to W 158

6 days ago

158 Weldon to HCC, 158 to RR, Weldon residential paths, and Weldon residential to River

6 days ago

see my comments in the first area.

11 days ago

Sidewalk and Nike trail from 11th St up Washington Ave to Riverside Park. Riverside Park to Hwy 158 traveling west under I-95 to Halifax Community College and shopping .

13 days ago

1. Washington Ave from South Weldon to Downtown Weldon. 2. Bike path from downtown Weldon to Country Club area, behind Halifax Community College, then around to 158 back to downtown. 3. Safe, convenient trails all leading to the River Falls Park. 4. All paths should be clean and maintained in the downtown area, with repairs done to current sidewalks to prevent injuries. 5. Safe trail from downtown to the Canal Trail (including communities that need to be walked/biked through. It seems crazy to drive a mile or two to a walking/biking trail when if in the right conditions walking/biking there makes sense.

26 days ago

X

one month ago

Need sidewalk

one month ago

Need sidewalks down to HCC

one month ago

Need more visibility

one month ago

Historic Area

one month ago

No good signage

one month ago

Swampy area along the trail

one month ago

No access at all to the Canal trail from the park

one month ago

Washington Avenue sidewalks from 1st St to Weldon Elementary

2 months ago

2 months ago

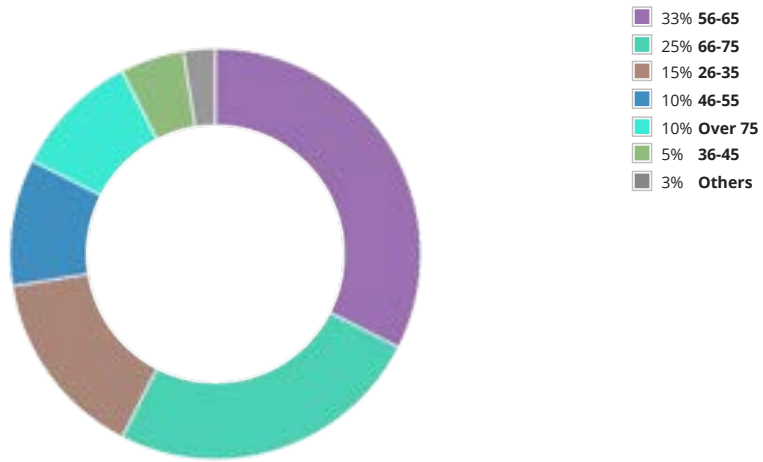
Crossings at Washington Ave and 2nd St (US 158)

2 months ago

1st St between River Falls Park and Riverside Mill

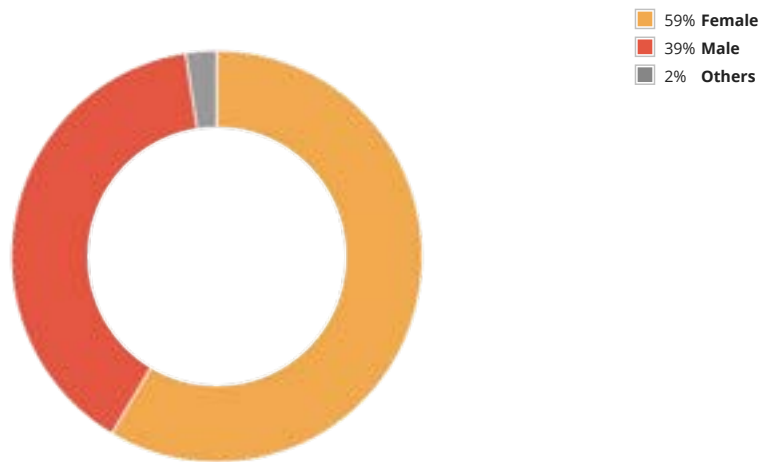
2 months ago

What is your age?

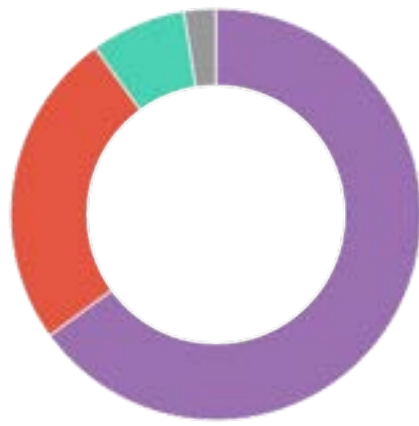


40 respondents

What is your gender?



41 respondents



- 65% **White**
- 25% **Black or African American**
- 8% **Prefer not to answer**
- 3% **Others**

40 respondents

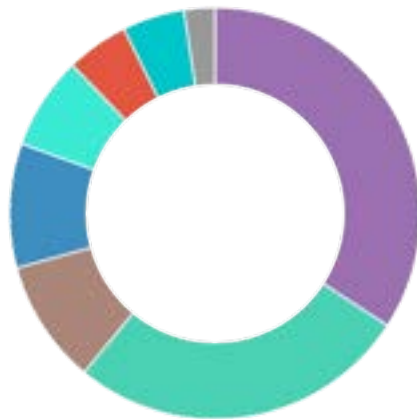
What is your annual household income?



- 26% **Prefer not to answer**
- 21% **\$50,000 - \$74,999**
- 18% **\$75,000 - \$99,999**
- 13% **\$25,000 - \$49,999**
- 11% **\$150,000 or more**
- 8% **\$100,000 - \$149,999**
- 3% **Less than \$25,000**

38 respondents

What is the highest level of education you have completed?



- 34% **Bachelor's degree**
- 27% **Master's degree**
- 10% **Some college, no degree**
- 10% **Associate degree**
- 7% **Doctorate degree**
- 5% **High school diploma or GED**
- 5% **Prefer not to answer**
- 2% **Others**

41 respondents

95% **No**
5% **Others**



39 respondents

How did you hear about the survey? *Please select all that apply.*

30%	Email	12 ✓
30%	Other (please specify)	12 ✓
28%	Word of Mouth (from friends, family, co-workers)	11 ✓
15%	Town Website	6 ✓
15%	Social Media (Facebook, Twitter, Instagram, etc.)	6 ✓
0%	Newspaper	0 ✓

40 Respondents

10. What amenities are most important to you for trails and multi-use paths? *Please rank your top 5 from 1 (most important) to 5 (least important).*

No data to display...

15. How did you hear about the survey? *Please select all that apply.*

No data to display...

15. How did you hear about the survey? *Please select all that apply.*

No data to display...

11. Multi-use paths and trails can be constructed using several different materials. Please select your preferred facilities below. Photos with brief descriptions are provided to the right. *Select all that apply.*

No data to display...

APPENDIX D: COST ESTIMATES

SIT 2: Off-Road/Separated Linear Bicycle Facility Start Over

Project Name: #1 US-158 Sidepath Only

SPOT ID: 0

Project Type: Shared-Use Path, Multi-Use Path, Rail-Trail, or Sidepath

1 Total Project Length: 8,554 feet

2 Proposed Facility Width (Default is 10 feet): 10 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Rural

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 1

10 Percentage of ROW Area Needed: Minimal (15%-25%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected: 4

13 Number of Existing Bridges: 0

Interstate	0	Interstate	0
Freeway	0	Freeway	0
Major Arterial	0	Major Arterial	0
Arterial	0	Arterial	0
Major Collector	0	Major Collector	0
Collector	0	Collector	0
Local Road	4	Local Road	0
Total	4	Small Stream	0
		Medium Stream	0
		Large Stream	0
		Railroad	0
		Total	0

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 3

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	3,865,000
Design	\$	550,000
ROW	\$	5,000
Utilities	\$	150,000
Construction	\$	3,160,000

Print PDF

Enter Any Desired Notes in the Box Below

Construction adjusted for inflation (\$2023): \$4,606,419

Disclaimers

All costs are based on 2019 prices and cost components are rounded to the nearest \$5,000, with a minimum of \$5,000 per component. This tool assumes that 10% of the utilities located within the project area would need to be relocated.

This tool assumes established ecoregion typologies, construction market regions, and average land values specific to North Carolina. They are determined within the tool based on user inputs for project location. This location-based information is used in ROW, construction, and environmental mitigation calculations.

This tool assumes a project impact area for ROW and environmental mitigation calculations based on chosen SIT, project type, project length, and project facility width.

This tool is limited in accuracy by user inputs and the complexity of questions presented for each project. If the inputs are incorrect, the tool's accuracy will be diminished.

This tool does not estimate costs associated with the purchase or taking of buildings within its ROW estimate calculations. It is assumed that projects would require land acquisition only.

Estimates for the construction of new and/or the modification of existing structures (bridges or tunnels) have been simplified to estimate an assumed width of each structure based on the type of feature crossed and other factors. The construction of new and/or modification of existing structures can be exponentially complex based on project specifications. A separate feasibility study is highly recommended to address the high variability associated with structure costs.

SIT 7: Protected Linear Pedestrian Facility Start Over

Project Name: #1 US-158 Sidewalk Only

SPOT ID: 0

Project Type: Sidewalk

1 Total Project Length: 7,572 feet

2 Proposed Facility Width (Default is 5 feet): 5 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Rural

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 1

10 Percentage of ROW Area Needed: Minimal (15%-25%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected: 11

13 Number of Existing Bridges: 0

Interstate	0	Interstate	0
Freeway	0	Freeway	0
Major Arterial	0	Major Arterial	0
Arterial	0	Arterial	0
Major Collector	0	Major Collector	0
Collector	0	Collector	0
Local Road	11	Local Road	0
Total	11	Small Stream	0
		Medium Stream	0
		Large Stream	0
		Railroad	0
		Total	0

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 3

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	1,675,000
Design	\$	310,000
ROW	\$	5,000
Utilities	\$	130,000
Construction	\$	1,230,000

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Enter Any Desired Notes in the Box Below

Construction adjusted for inflation (\$2023): \$1,793,005

Disclaimers

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Estimates for the construction of new and/or the modification of existing structures (bridges or tunnels) have been simplified to estimate an assumed width of each structure based on the type of feature crossed and other factors. The construction of new and/or modification of existing structures can be exponentially complex based on project specifications. A separate feasibility study is highly recommended to address the high variability associated with structure costs.

SIT 2: Off-Road/Separated Linear Bicycle Facility Start Over

Project Name: #2 Chockoyotte Creek Greenway

SPOT ID: 0

Project Type: Shared-Use Path, Multi-Use Path, Rail-Trail, or Sidepath

1 Total Project Length: 7,392 feet

2 Proposed Facility Width (Default is 10 feet): 10 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Forested

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: Large (25%-60%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Total	0

13 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

14 Number of Existing Bridges

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Small Stream	0
Medium Stream	0
Large Stream	0
Railroad	0
Total	0

Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$ 4,225,000
Design	\$ 1,370,000
ROW	\$ 10,000
Utilities	\$ 130,000
Construction	\$ 2,715,000

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Enter Any Desired Notes in the Box Below

Construction adjusted for inflation (\$2023): \$3,957,730

Disclaimers

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SIT 2: Off-Road/Separated Linear Bicycle Facility Start Over

Project Name: #3 Roanoke Canal Trail

SPOT ID: 0

Project Type: Shared-Use Path, Multi-Use Path, Rail-Trail, or Sidepath

1 Total Project Length: 2,640 feet

2 Proposed Facility Width (Default is 10 feet): 10 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Forested

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: Total (80-100%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Total	0

13 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

14 Number of Existing Bridges

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Small Stream	0
Medium Stream	0
Large Stream	0
Railroad	0
Total	0

Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$ 1,535,000
Design	\$ 515,000
ROW	\$ 5,000
Utilities	\$ 45,000
Construction	\$ 970,000

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Construction adjusted for inflation (\$2023): \$1,413,995

Disclaimers

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Estimates for the construction of new and/or the modification of existing structures (bridges or tunnels) have been simplified to estimate an assumed width of each structure based on the type of feature crossed and other factors. The construction of new and/or modification of existing structures can be exponentially complex based on project specifications. A separate feasibility study is highly recommended to address the high variability associated with structure costs.

SIT 2: Off-Road/Separated Linear Bicycle Facility Start Over

Project Name: #4 2nd St Protected Bike Lane

SPOT ID: 0

Project Type: Contra-Flow Bicycle Lane or Separated Bicycle Lane

1 Total Project Length: 2,904 feet

2 Proposed Facility Width (Default is 10 feet): 7 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Rural

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: None (0-15%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	6
Total	6

13 Number of Existing Bridges

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Small Stream	0
Medium Stream	0
Large Stream	0
Railroad	0
Total	0

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 2

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	965,000
Design	\$	370,000
ROW	\$	5,000
Utilities	\$	50,000
Construction	\$	540,000

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Construction adjusted for inflation (\$2023): \$787,172

Disclaimers

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Estimates for the construction of new and/or the modification of existing structures (bridges or tunnels) have been simplified to estimate an assumed width of each structure based on the type of feature crossed and other factors. The construction of new and/or modification of existing structures can be exponentially complex based on project specifications. A separate feasibility study is highly recommended to address the high variability associated with structure costs.

SIT 7: Protected Linear Pedestrian Facility Start Over

Project Name: #5 Elm St Sidewalk Gaps

SPOT ID: 0

Project Type: Sidewalk

1 Total Project Length: 1,108 feet

2 Proposed Facility Width (Default is 5 feet): 5 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Rural

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: Minimal (15%-25%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Total	0

13 Number of Existing Bridges

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Small Stream	0
Medium Stream	0
Large Stream	0
Railroad	0
Total	0

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	260,000
Design	\$	80,000
ROW	\$	5,000
Utilities	\$	20,000
Construction	\$	155,000

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Construction adjusted for inflation (\$2023): \$225,947

Disclaimers

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This tool does not estimate costs associated with the purchase or taking of buildings within its ROW estimate calculations. It is assumed that projects would require land acquisition only.

Estimates for the construction of new and/or the modification of existing structures (bridges or tunnels) have been simplified to estimate an assumed width of each structure based on the type of feature crossed and other factors. The construction of new and/or modification of existing structures can be exponentially complex based on project specifications. A separate feasibility study is highly recommended to address the high variability associated with structure costs.

SIT 2: Off-Road/Separated Linear Bicycle Facility Start Over

Project Name: #6 Country Club Rd Sidepath

SPOT ID: 0

Project Type: Shared-Use Path, Multi-Use Path, Rail-Trail, or Sidepath

1 Total Project Length: 9,240 feet

2 Proposed Facility Width (Default is 10 feet): 10 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Rural

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 1

10 Percentage of ROW Area Needed: Minimal (15%-25%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected: 4

13 Number of Existing Bridges: 1

Interstate	0	Interstate	0
Freeway	0	Freeway	0
Major Arterial	0	Major Arterial	0
Arterial	0	Arterial	0
Major Collector	0	Major Collector	0
Collector	0	Collector	0
Local Road	4	Local Road	0
Total	4	Small Stream	1
		Medium Stream	0
		Large Stream	0
		Railroad	0
		Total	1

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	5,000,000
Design	\$	1,445,000
ROW	\$	5,000
Utilities	\$	160,000
Construction	\$	3,390,000

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Construction adjusted for inflation (\$2023): \$4,941,696

Disclaimers

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SIT 3: On-Road Designated Bicycle Facility Start Over

Project Name: #7 Washington Ave Bike Lane

SPOT ID: 0

Project Type: Bicycle Lane

1 Total Project Length: 1,954 feet

2 Proposed Facility Width (Default is 5 feet): 7 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Downtown

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: None (0-15%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected: 5

13 Number of Existing Bridges: 0

Interstate	0	Interstate	0
Freeway	0	Freeway	0
Major Arterial	0	Major Arterial	0
Arterial	0	Arterial	0
Major Collector	0	Major Collector	0
Collector	0	Collector	0
Local Road	5	Local Road	0
Total	5	Small Stream	0
		Medium Stream	0
		Large Stream	0
		Railroad	0
		Total	0

14 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

15 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$	1,005,000
Design	\$	245,000
ROW	\$	5,000
Utilities	\$	65,000
Construction	\$	690,000

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Adjusted for inflation (\$2023): \$1,005,832

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SIT 7: Protected Linear Pedestrian Facility Start Over 

Project Name: #8 Sycamore St Sidewalk Gaps

SPOT ID: 0

Project Type: Sidewalk

1 Total Project Length: 2,112 feet

2 Proposed Facility Width (Default is 5 feet): 5 feet

3 Project Located on Both Sides of the Road: YES NO

4 County: Halifax

5 City: Weldon

6 Surrounding Development Type: Downtown

7 Registered Historic District: YES NO

8 Existing Curb & Gutter within Project Area: YES NO

9 Number of Stream Crossings: 0

10 Percentage of ROW Area Needed: None (0-15%)

11 Impact to Active Railroad Track or Railroad ROW: YES NO

12 Roadways Intersected

Interstate	0
Freeway	0
Major Arterial	0
Arterial	0
Major Collector	0
Collector	0
Local Road	0
Total	0

13 Signalized Intersections Crossed (Number within Total Roadways Intersected): 0

14 Submitted by: M Date: 10/18/2023

Generate Cost Clear

Cost Estimate Summary Go to Calculation Tab

Total	\$ 510,000
Design	\$ 115,000
ROW	\$ 5,000
Utilities	\$ 35,000
Construction	\$ 355,000

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Construction adjusted for inflation (\$2023): \$517,493

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2023

TOWN OF WELDON
PROJECT ACCELERATION PLAN